



OFFSHORE RULES

2014

2013 OFFSHORE WORLD CHAMPIONS

Class 1



Arif
AL ZAFFAIN
UAE



Mohammed
AL MARRI
UAE

Class 3B



Thomas
WRENKLER
Sweden



Annika
WRENKLER
Sweden

Class 3C



Marcus
JOHANSSON
Finland



Jussi
MYLLYMAKI
Finland

XCAT



Arif
AL ZAFFAIN
UAE



Nadir
BIN HENDI
UAE

2013 OFFSHORE CONTINENTAL CHAMPIONS

European Class 1



Alfredo
AMATO
Italy



Miles
JENNINGS
Great-Britain

European Class 3B



Rasmus
SÖDERHOLM
Sweden



Jonny
INGEMARSSON
Sweden

European Class 3C



Marcus
JOHANSSON
Finland



Jussi
MYLLYMAKI
Finland

Middle East XCAT



Arif
AL ZAFFAIN
UAE



Nadir
BIN HENDI
UAE

European XCAT



Rashid
AL TAYER
UAE



Jay
PRICE
USA

INDEX

CODE OF ETHICS.....	13
ANTI-DOPING RULES	19
ENVIRONMENTAL CODE.....	87

GROUP 100 - UIM ADMINISTRATION

101	UIM EVENT SANCTIONS AND THE SPORTS CALENDAR.....	107
102	WORLD CHAMPIONSHIPS.....	109
103	RACES RECOGNISED BY THE UIM.....	110
104	NON APPROVED RACES.....	110
105	TITLED INTERNATIONAL EVENTS.....	111
120	RACING LICENCES.....	111

GROUP 200 - RACE ORGANISATION

200	RACE ORGANISATION	117
201	RACE CONTROL	119
202	ADVANCE PROGRAMME	125
203	ENTRIES	127
204	PROGRAMME / RACING INSTRUCTIONS.....	129
205	SAFETY RULES.....	131
206	ASSISTANCE IN ACCIDENTS.....	140
207	DISCLAIMER.....	141
208	RESERVED NUMBER.....	142
209	CRANE AREA	142

GROUP 300 - RACING RULES

300	RACING RULES.....	143
301	TIMING	144
303	MARKS OF THE COURSE.....	144
305	PROCEDURE FOR UIM OFFSHORE STARTS.....	144
306	QUALIFICATION	149
309	RACE CANCELLED.....	149
310	RACE POSTPONED	149
311	RACE SHORTENED	150
312	RACE STOPPED.....	150
313	RULES OF THE ROAD.....	151
314	ROUNDING AND FOULING MARKS	153
315	FOULING COMPETING BOATS.....	153
316	RUNNING AGROUND.....	153
317	ANCHORING DURING A RACE.....	154
318	MEANS OF PROPULSION	154
319	OUTSIDE ASSISTANCE	154
320	FINISHING A RACE	155
321	SCORING SYSTEM	156
323	POSTING OF THE RESULTS	157
324	INSPECTION OF HULLS AND ENGINES.....	157
326	AFTER THE EVENT (REPORTS)	158
330	SPRINT POWER SLALOM	159

GROUP 400 - JURISDICTION

400	JURISDICTION.....	162
-----	-------------------	-----

402	JUDICIAL BODIES.....	162
403	PROTEST PROCEDURES.....	164
404	HEARING OF THE PROTEST.....	166
405	THE APPEALS PROCEDURE.....	167
406	PENALTIES.....	170
407	PRIORITY OF THESE RULES.....	173
408	POWERBOAT PROTEST FORM.....	173
409	ARBITRATION.....	175

GROUP 500 - INTERNATIONAL CLASSES

500	TECHNICAL RULES, CLASSES.....	178
501	MEASUREMENT CERTIFICATE.....	179
502	INSPECTIONS.....	186
503	EQUIPMENT.....	192
504	FUEL.....	192
505	ENGINE HOMOLOGATION.....	198
508	CREW SAFETY.....	202

GROUP 600 - RECORDS

600	RECORDS AND PERFORMANCES.....	215
601	CLASSES ELIGIBLE FOR RECORDS.....	215
602	DEFINITION OF UNITS.....	217
604	BASES (THE COURSE).....	218
605	CALCULATION OF SPEED.....	219
606	MARGIN REQUIRED.....	220
607	MOTORS.....	220

608	REQUEST TO ORGANISE	221
609	OFFICIALS	221
610	SPEED RECORDS	222
611	DISTANCE RECORDS	224
612	HOUR RECORDS	225
613	WORLD COMPETITION RECORDS	227
614	PERFORMANCE CERTIFICATES	227
615	HOMOLOGATION OF RECORDS	228
616	LONG DISTANCE OFFSHORE MOTORBOAT WORLD SPEED RECORDS	230

GROUP 700 - INTERNATIONAL OFFSHORE RULES

700	INTERNATIONAL OFFSHORE RULES.....	236
701	GENERAL	236
702	CREW	238
703	HULL	238
704	INBOARD ENGINES	241
705	ENGINES.....	242
706	ENGINE CUT-OUT	243
707	ENGINE AND FUEL COMPARTMENT BULKHEADS	243
708	PROPULSION	244
709	GYROS	245
710	RACE NUMBERS.....	245
711	NATIONAL FLAG	247
712	RACING VESTS	247
713	HELMETS.....	247

714	PROTECTIVE CLOTHING.....	248
715	SAFETY EQUIPMENT	248

CLASS 3

731	MINIMUM/MAXIMUM LENGTHS AND WEIGHTS.....	253
732	HULL DIMENSIONS.....	253
733	MINIMUM FLOTATION	253
	ENGINE ELIGIBILITY	254
734	(NON EPA/CARB) OUTBOARD ENGINES	254
735	MODIFICATIONS PERMITTED (NON EPA/CARB) OUTBOARD ENGINES.....	256

GROUP 800

800	GENERAL RULES FOR OFFSHORE WORLD AND CONTINENTAL CHAMPIONSHIPS (EXCEPT CLASS 1 WORLD CHAMPIONSHIP).....	265
855	ADVISORY REQUIREMENTS FOR UIM CHAMPIONSHIP EVENTS - SECURITY AND LOGISTICS.....	273

GROUP 950 - UIM V24 CLASS

950	U.I.M. V24 CLASS	280
951	GENERAL	280
952	MODIFICATIONS ALLOWED	282
953	FUEL.....	285
954	IDENTIFICATION.....	285
955	SAFETY.....	285
956	EQUIPMENT.....	287

957	CREW	287
958	ENGINE SEALS	287
959	CONTINENTAL/EUROPEAN CHAMPIONSHIPS.....	288
960	RULES FOR UIM PROVEE CLASS.....	288

GROUP 1000 - UIM CLASS 3 - 225 OFFSHORE RULES

1000.1	GENERAL	288
1000.2	CHAMPIONSHIP	288
1000.3	PROMOTIONAL RIGHTS.....	290
1000.4	RACING TEAM AND LICENSE.....	290
1000.5	THE RACE.....	290
1000.6	PROCEDURE FOR CLASS 3 - 225 OFFSHORE STARTS.....	291
1000.7	POLE POSITION / QUALIFICATION	292
1000.8	PIT STOP.....	292
1000.10	PENALTIES	294
1000.11	SEA SAFETY	294
1000.12	TECHNICAL RULES.....	294
1000.13	POWER LIFT	297
1000.14	SPECIFIC RESTRICTIONS.....	297

GROUP 1100 - UIM RIB RACES

1100.1	GENERAL PRESENTATION.....	299
1100.2	COMPETITORS QUALIFICATION	299
1100.3	CLASSES	300
1100.4	ENGINES SPECIFICATIONS AND MODIFICATIONS.....	301
1100.5	SPECIFIC RULES FOR BOATS AND EQUIPMENT	301

1100.6	ASSISTANCE RULES	303
1100.7	WEIGHING OF BOATS	310
1100.8	CRANAGE AND RECOVERY	310
1100.9	UIM FLAG SIGNALS.....	310
1100.10	RIB RACING COURSE RULES	311
1100.11	POINTS SCORING SYSTEM.....	311

GROUP 1200 - MARATHON RACING

1200.1	GENERAL	312
1200.2	COMPETITION RULES.....	313
1200.3	RACE BOATS.....	314
1200.4	ENGINES.....	321
1200.5	SAFETY EQUIPMENT FOR THE BOAT	322
1200.6	SAFETY EQUIPMENT FOR EACH CREW.....	324
1200.7	TURN MARKS AND MISSED MARKS.....	327
1200.8	CLASSIFICATION TABLE.....	329
1200.9	ULTRA MARATHON RACES.....	330

GROUP 1300 - UIM STOCK

INTRODUCTION.....	332
LICENCE TO RACE.....	333
BOAT REGISTRATION AND COMPLIANCE.....	334
RACE RULES - GENERAL	334
BOAT AND EQUIPMENT	335
RACING REGULATIONS	345



CODE OF ETHICS

2014

1. Preamble

The UIM is committed to the highest standards of conduct in sport administration and competition. To meet this commitment, the UIM has developed a Code of Ethics to express the core values of both the organisation and the sport of Powerboating. Such values and ethics underpin the UIM's policies, procedures and rules. Observance of the code is vital to the integrity of Powerboating. The UIM Code of Ethics is inspired to the ethical principles of the Olympic Movement of which the UIM is member.

The UIM Code of Ethics comprises five pillars. It imposes obligations in terms of respect and responsibility to competitors, teams, promoters, officials and all other UIM accredited persons.

The ethical pillars of the UIM

1.1 Equality

Discrimination and harassment against others on grounds of race, disability, marital status, sex, sexuality, age, political or religious conviction are not condoned in Powerboating.

All forms of harassment, be they physical, mental, professional or sexual, are strictly prohibited.

Powerboating promotes the inclusion of men and women equally.

1.2 Fair Play

Fair play is the guiding principle in the sport of Powerboating. It is expected that all persons taking part in Powerboating shall behave with fairness and honesty.

All persons shall operate within and abide by the rules of the sport.

All doping practices at all levels are strictly prohibited. The provisions against doping in Anti-Doping Code shall be scrupulously observed. Powerboating is committed to be a drug free sport.

1.3 Respect

Powerboating shall be characterised by mutual respect and self-responsibility. All persons involved in powerboating shall be treated with dignity.

The contribution that people make to the sport shall be recognised.

In pursuing the sport's goals, the governance of Powerboating shall be mindful of the physical and psychological well-being of its members.

Violence and abusive behaviour are not tolerated.

1.4 Integrity

All persons subject to this Code shall use due care and diligence in fulfilling their roles for, and on behalf of, the UIM or Powerboating in general.

Decisions by the UIM will be made in accordance with established procedures, objectively, fairly and with honesty and integrity.

Conflicts of interest must be avoided.

1.5 Environment

The UIM is committed to raise environmental performance of power boating and make our sport a vector of environmental protection and sustainable development.

UIM looks to youth to breed a future for sportsmanship and safety while nurturing a passion and respect for water and its environment.

The UIM will promote the optimal use of resources and materials, efficient logistics and transport, reduction of polluting discharges to water and emissions to air.

2. Conduct Regulations

This Code applies to the following "Involved Parties": competitors, teams, promoters, officials and all other UIM accredited persons.

a) Basic rules

All involved Parties shall show commitment to an ethical attitude while fulfilling their task. They shall pledge to behave in accordance with the ethical pillars of the UIM.

UIM Officials may not abuse their position as part of their function in any way, especially to take advantage of their function for private aims or gains.

b) Representational duties

UIM Officials shall represent UIM honestly, respectably and with integrity.

c) Conduct towards government and private organizations

In dealings with government institutions, national and international organizations, associations and groupings, "Involved Parties" shall, in addition to observing the

basic rules of art. a, remain politically neutral, in accordance with the principles and objectives of UIM and act in a manner compatible with their function and integrity.

d) Ban on discrimination

“Involved Parties” may not act in a discriminatory manner, especially with regard to ethnicity, race, culture, politics, religion, gender or language.

e) Eligibility and dismissal

Only those persons who demonstrate a high degree of ethics and integrity and pledge to observe the provisions of this Code without reservation are eligible to serve as UIM officials. Any UIM officials who do not comply with these conditions are either no longer eligible or shall be removed from office.

f) Conflicts of interest

“Involved Parties” may not perform their tasks in cases with an existing or potential conflict of interest.

“Involved Parties”, particularly UIM officials, shall avoid any situation that could lead to conflicts of interest. Conflicts of interest arise if there are private or personal interests that detract from the ability to carry out obligations as “Involved Parties”, particularly UIM officials, with integrity in an independent and purposeful manner.

g) Protection of personal rights

During the course of their activities, “Involved Parties” shall ensure that the personal rights of those persons whom they contact and with whom they deal are protected, respected and safeguarded.

h) Loyalty and confidentiality

While performing their duties, officials shall remain loyal to UIM. Depending on their function, any information divulged to officials during the course of their duties shall be treated as confidential. Any information or opinions shall be passed on in accordance with the principles and objectives of UIM.

i) Accepting and giving gifts

“Involved Parties” are not permitted to accept gifts and other benefits that exceed the average relative value of local cultural customs from third parties.

Accepting gifts of cash in any amount or form is prohibited.

While performing their duties, officials may give gifts and other benefits in accordance with the average relative value of local cultural customs to third parties, provided no dishonest advantages are gained and there is no conflict of interest.

j) Bribery

“Involved Parties” may not accept bribes, in other words, any gifts or other advantages that are offered, promised or sent to them to incite breach of duty or dishonest conduct.

“Involved Parties” are forbidden from bribing third parties or from urging or inciting others to do so in order to gain an advantage for them or a third party.

k) Betting

“Involved Parties” are forbidden from taking part, either directly or indirectly, in illegal betting or gambling connected with powerboat races.

3. Procedural regulations

a) Basic principle

Violations of the rules of this Code shall be sanctioned.

b) Judging body

The UIM Executive Committee shall judge cases that are not compliant with the UIM Code of Ethic.

c) Disciplinary measures

The UIM Executive Committee will decide disciplinary measures.

d) Hearings

The hearings of UIM Executive Committee for proceedings related to ethics matters are deemed to be valid if the majority of the UIM Executive Committee members are present.

e) Confidentiality

The members of the judicial body shall ensure that everything disclosed to them during the course of their duty remains confidential (facts of the case, contents of the deliberations and decisions taken). Only the contents of those decisions already notified to the addressees may be made public.

4. Enforcement

The conduct and procedural regulations of this Code are adopted by the UIM General Assembly of October 25th, 2009 and come into force on January 1, 2010.



ANTI-DOPING RULES

2014

TABLE OF CONTENTS

INTRODUCTION.....	23
Preface.....	23
Fundamental Rationale for the Code and UIM's Anti-Doping Rules.....	23
Scope	24
ARTICLE 1 DEFINITION OF DOPING	25
ARTICLE 2 ANTI-DOPING RULE VIOLATIONS.....	25
ARTICLE 3 PROOF OF DOPING.....	30
ARTICLE 4 THE PROHIBITED LIST	32
ARTICLE 5 TESTING	35
ARTICLE 6 ANALYSIS OF SAMPLES.....	39
ARTICLE 7 RESULTS MANAGEMENT	40
ARTICLE 8 RIGHT TO A FAIR HEARING	46
ARTICLE 9 AUTOMATIC DISQUALIFICATION OF INDIVIDUAL RESULTS.....	48
ARTICLE 10 SANCTIONS ON INDIVIDUALS	49
ARTICLE 11 Intentionally left blank	68
ARTICLE 12 SANCTIONS AND COSTS ASSESSED AGAINST NATIONAL ASSOCIATIONS.....	68
ARTICLE 13 APPEALS	69
ARTICLE 14 NATIONAL ASSOCIATIONS INCORPORATION OF UIM RULES, REPORTING AND RECOGNITION	72
ARTICLE 15 RECOGNITION OF DECISIONS BY OTHER ORGANISATIONS..	74
ARTICLE 16 STATUTE OF LIMITATIONS	75
ARTICLE 17 UIM COMPLIANCE REPORTS TO WADA	75
ARTICLE 18 AMENDMENT AND INTERPRETATION OF ANTI-DOPING RULES	75

UIM ANTI-DOPING RULES

INTRODUCTION

Preface

At the UIM General Assembly held on the 2nd November 2008 in the Gold Coast, Australia, the UIM accepted the revised (2009) UIM Anti-Doping Rules based on the World Anti-Doping *Code* (the “*Code*”) approved in Madrid, Spain, in November 2007. These Anti-Doping Rules are adopted and implemented in conformance with the UIM’s responsibilities under the *Code*, and are in furtherance of the UIM’s continuing efforts to eradicate doping in the sport of Powerboating.

Anti-Doping Rules, like *Competition* rules, are sport rules governing the conditions under which sport is played. *Drivers* and other *Persons* accept these rules as a condition of participation and shall be bound by them. These sport-specific rules and procedures, aimed at enforcing anti-doping principles in a global and harmonized manner, are distinct in nature and, therefore, not intended to be subject to, or limited by any national requirements and legal standards applicable to criminal proceedings or employment matters. When reviewing the facts and the law of a given case, all courts, arbitral tribunals and other adjudicating bodies should be aware of and respect the distinct nature of the anti-doping rules in the *Code* and the fact that these rules represent the consensus of a broad spectrum of stakeholders around the world with an interest in fair sport.

Fundamental Rationale for the Code and the UIM’s Anti-Doping Rules

Anti-doping programs seek to preserve what is intrinsically valuable about sport. This intrinsic value is often referred to as “the spirit of sport”; it is the essence of Olympism; it is how we play true. The spirit of sport is the celebration of the human spirit, body and mind, and is characterized by the following values:

- Ethics, fair play and honesty
- Health
- Excellence in performance
- Character and education
- Fun and joy

- Teamwork
- Dedication and commitment
- Respect for rules and laws
- Respect for self and other participants
- Courage
- Community and solidarity

Doping is fundamentally contrary to the spirit of sport.

Scope

These Anti-Doping Rules shall apply to the UIM, each *National Association* of the UIM, and each *Participant* in the activities of the UIM or any of its *National Associations* by virtue of the *Participant's* membership, accreditation, or participation in the UIM, its *National Associations*, or their activities or *Events*. Any *Person* who is not a member of a National Association and who fulfills the requirements to be part of the UIM Registered Testing Pool, must become a member of the *Person's National Association*, and must make himself or herself available for *Testing*, at least six months before participating in *International Events* or events of his/her *National Association*.

To be eligible for participation in UIM events, a Driver must have a Powerboat Racing licence issued by his or her National Association. The Powerboat Racing licence will only be issued to *Drivers* who have personally signed the Appendix 2 consent form, in the actual form approved by the UIM Executive. All forms from under-age applicants must be counter-signed by their legal guardians.

The National Association must guarantee that all *Drivers* registered for a Powerboat Racing Licence accept the Rules of the UIM, concerning these UIM Anti-Doping Rules.

It is the responsibility of each *National Association* to ensure that all national-level *Testing* on the *National Association's Drivers* complies with these Anti-Doping Rules. In some countries, the *National Association* itself will be conducting the *Doping Control* described in these Anti-Doping Rules. In other countries, many of the *Doping Control* responsibilities of the *National Association* have been delegated or assigned by statute or agreement to a *National Anti-Doping Organization*. In those countries, references in these Anti-Doping Rules to the *National Association* shall apply, as appropriate, to the *National Anti-Doping Organization*.

These Anti-Doping Rules shall apply to all *Doping Controls* over which the UIM and its *National Associations* have jurisdiction.

ARTICLE 1 DEFINITION OF DOPING

Doping is defined as the occurrence of one or more of the anti-doping rule violations set forth in Article 2.1 through Article 2.8 of these Anti-Doping Rules.

ARTICLE 2 ANTI-DOPING RULE VIOLATIONS

Drivers and other *Persons* shall be responsible for knowing what constitutes an anti-doping rule violation and the substances and methods which have been included on the *Prohibited List*.

The following constitute anti-doping rule violations:

[Comment to Article 2: The purpose of Article 2 is to specify the circumstances and conduct which constitute violations of anti-doping rules. Hearings in doping cases will proceed based on the assertion that one or more of these specific rules has been violated.]

2.1 The presence of a Prohibited Substance or its Metabolites or Markers in a Driver's Sample

2.1.1 It is each *Drivers* personal duty to ensure that no *Prohibited Substance* enters his or her body. *Drivers* are responsible for any *Prohibited Substance* or its *Metabolites* or *Markers* found to be present in their *Samples*. Accordingly, it is not necessary that intent, fault, negligence or knowing *Use* on the *Driver's* part be demonstrated in order to establish an anti-doping violation under Article 2.1.

[Comment to Article 2.1.1: For purposes of anti-doping violations involving the presence of a Prohibited Substance (or its Metabolites or Markers), UIM's Anti-Doping Rules adopt the rule of strict liability which was found in the Olympic Movement Anti-Doping Code ("OMADC") and the vast majority of pre-Code anti-doping rules. Under the strict liability principle, a Driver is responsible, and an anti-doping rule violation occurs, whenever a Prohibited Substance is found in a Driver's Sample. The violation occurs whether or not the Driver intentionally or unintentionally used a Prohibited Substance or was negligent or otherwise at fault. If the positive Sample came from an In-Competition test, then the results of that Competition are automatically invalidated (Article 9 (Automatic Disqualification of Individual Results)). However, the Driver then has the possibility to avoid or reduce

sanctions if the Driver can demonstrate that he or she was not at fault or significant fault (Article 10.5 (Elimination or Reduction of Period of Ineligibility Based on Exceptional Circumstances)) or in certain circumstances did not intend to enhance his or her sport performance (Article 10.4 (Elimination or Reduction of the Period of Ineligibility for Specified Substances under Specific Circumstances)).

The strict liability rule for the finding of a Prohibited Substance in a Driver's Sample, with a possibility that sanctions may be modified based on specified criteria, provides a reasonable balance between effective anti-doping enforcement for the benefit of all "clean" Drivers and fairness in the exceptional circumstance where a Prohibited Substance entered a Driver's system through No Fault or Negligence or No Significant Fault or Negligence on the Driver's part. It is important to emphasize that while the determination of whether the anti-doping rule has been violated is based on strict liability, the imposition of a fixed period of Ineligibility is not automatic. The strict liability principle set forth in International Federation's Anti-Doping Rules has been consistently upheld in the decisions of CAS.]

2.1.2 Sufficient proof of an anti-doping rule violation under Article 2.1 is established by either of the following: presence of a *Prohibited Substance* or its *Metabolites* or *Markers* in the *Driver's A Sample* where the *Driver* waives analysis of the *B Sample* and the *B Sample* is not analyzed; or, where the *Driver's B Sample* is analyzed and the analysis of the *Driver's B Sample* confirms the presence of the *Prohibited Substance* or its *Metabolites* or *Markers* found in the *Driver's A Sample*.

[Comment to Article 2.1.2: The UIM may in its discretion choose to have the B Sample analyzed even if the Driver does not request the analysis of the B Sample.]

2.1.3 Excepting those substances for which a quantitative threshold is specifically identified in the *Prohibited List*, the presence of any quantity of a *Prohibited Substance* or its *Metabolites* or *Markers* in a *Driver's Sample* shall constitute an anti-doping rule violation.

2.1.4 As an exception to the general rule of Article 2.1, the *Prohibited List* or *International Standards* may establish special criteria for the evaluation of *Prohibited Substances* that can also be produced endogenously.

2.2 Use or Attempted Use by a Driver of a Prohibited Substance or a Prohibited Method

[Comment to Article 2.2: As noted in Article 3 (Proof of Doping), it has always been the case that Use or Attempted Use of a Prohibited Substance or Prohibited Method may be established by any reliable means. Unlike the proof required to establish an anti-doping rule violation under Article 2.1, Use or Attempted Use may also be established by other reliable means such as admissions by the Driver, witness statements, documentary evidence, conclusions drawn from longitudinal profiling, or other analytical information which does not otherwise satisfy all the requirements to establish "Presence" of a Prohibited Substance under Article 2.1. For example, Use may be established based upon reliable analytical data from the analysis of an A Sample (without confirmation from an analysis of a B Sample) or from the analysis of a B Sample alone where the UIM provides a satisfactory explanation for the lack of confirmation in the other Sample.]

2.2.1 It is each *Driver's* personal duty to ensure that no *Prohibited Substance* enters his or her body. Accordingly, it is not necessary that intent, fault, negligence or knowing *Use* on the *Driver's* part be demonstrated in order to establish an anti-doping rule violation for *Use* of a *Prohibited Substance* or a *Prohibited Method*.

2.2.2 The success or failure of the *Use* of a *Prohibited Substance* or *Prohibited Method* is not material. It is sufficient that the *Prohibited Substance* or *Prohibited Method* was *Used* or *Attempted* to be *Used* for an anti-doping rule violation to be committed.

[Comment to Article 2.2.2: Demonstrating the "Attempted Use" of a Prohibited Substance requires proof of intent on the Driver's part. The fact that intent may be required to prove this particular anti-doping rule violation does not undermine the strict liability principle established for violations of Article 2.1 and violations of Article 2.2 in respect of Use of a Prohibited Substance or Prohibited Method.]

A Driver's "Use" of a Prohibited Substance constitutes an anti-doping rule violation unless such substance is not prohibited Out-of-Competition and the Driver's Use takes place Out-of-Competition. (However, the presence of a Prohibited Substance or its Metabolites or Markers in a Sample collected In-Competition will be a violation of Article 2.1 (Presence of a Prohibited Substance or its Metabolites or Markers) regardless of when that substance might have been administered.)]

2.3 Refusing or failing without compelling justification to submit to Sample collection after notification as authorized in these Anti-Doping Rules, or otherwise evading Sample collection.

[Comment to Article 2.3: Failure or refusal to submit to Sample collection after notification was prohibited in almost all pre-Code anti-doping rules. This Article expands the typical pre-Code rule to include “otherwise evading Sample collection” as prohibited conduct. Thus, for example, it would be an anti-doping rule violation if it were established that a Driver was hiding from a Doping Control official to evade notification or Testing. A violation of “refusing or failing to submit to Sample collection” may be based on either intentional or negligent conduct of the Driver, while “evading” Sample collection contemplates intentional conduct by the Driver.]

2.4 Violation of applicable requirements regarding Driver availability for Out-of-Competition Testing set out in the International Standard for Testing, including failure to file whereabouts information in accordance with Article 11.3 of the *International Standard for Testing* (a “**Filing Failure**”) and failure to be available for Testing at the declared whereabouts in accordance with Article 11.4 of the *International Standard for Testing* (a “**Missed Test**”). Any combination of three Missed Tests and/or Filing Failures committed within an eighteen-month period, as declared by the UIM or any other Anti-Doping Organization with jurisdiction over a Driver, shall constitute an anti-doping rule violation.

[Comment to Article 2.4: Separate whereabouts filing failures and missed tests declared under the rules of the UIM or any other Anti-Doping Organization with authority to declare whereabouts filing failures and missed tests in accordance with the International Standard for Testing shall be combined in applying this Article. In appropriate circumstances, missed tests or filing failures may also constitute an anti-doping rule violation under Article 2.3 or Article 2.5.]

2.5 Tampering or Attempted Tampering with any part of Doping Control.

[Comment to Article 2.5: This Article prohibits conduct which subverts the Doping Control process but which would not otherwise be included in the definition of Prohibited Methods. For example, altering identification numbers on a Doping Control form during Testing, breaking the B Bottle at the time of B Sample analysis or providing fraudulent information to the UIM]

2.6 Possession of Prohibited Substances and Methods

2.6.1 *Possession by a Driver In-Competition of any Prohibited Method or any Prohibited Substance, or Possession by a Driver Out-of-Competition of any Prohibited Method or any Prohibited Substance which is prohibited in Out-of-Competition Testing unless the Driver establishes that the Possession is pursuant to a therapeutic use exemption (“TUE”) granted in accordance with Article 4.4 (Therapeutic Use) or other acceptable justification.*

2.6.2 *Possession by Driver Support Personnel In-Competition of any Prohibited Method or any Prohibited Substance, or Possession by Driver Support Personnel Out-of-Competition of any Prohibited Method or any Prohibited Substance which is prohibited Out-of-Competition, in connection with a Driver, Competition or training, unless the Driver Support Personnel establishes that the Possession is pursuant to a TUE granted to a Driver in accordance with Article 4.4 (Therapeutic Use) or other acceptable justification.*

[Comment to Article 2.6.1 and 2.6.2: Acceptable justification would not include, for example, buying or possessing a Prohibited Substance for purposes of giving it to a friend or relative, except under justifiable medical circumstances where that Person had a physician’s prescription, e.g., buying Insulin for a diabetic child.]

[Comment to Article 2.6.2: Acceptable justification would include, for example, a team doctor carrying Prohibited Substances for dealing with acute and emergency situations.]

2.7 Trafficking or Attempted Trafficking in any Prohibited Substance or Prohibited Method.

2.8 Administration or Attempted administration to any Driver In-Competition of any Prohibited Method or Prohibited Substance, or administration or Attempted administration to any Driver Out-of-Competition of any Prohibited Method or any Prohibited Substance that is prohibited Out-of-Competition, or assisting, encouraging, aiding, abetting, covering up or any other type of complicity involving an anti-doping rule violation or any Attempted anti-doping rule violation.

[Comment to Article 2: The Code does not make it an anti-doping rule violation for a Driver or other Person to work or associate with Driver Support Personnel who are serving a period of Ineligibility. However, UIM may adopt its own specific policy which prohibits such conduct.]

ARTICLE 3 PROOF OF DOPING

3.1 Burdens and Standards of Proof

UIM and its *National Associations* shall have the burden of establishing that an anti-doping rule violation has occurred. The standard of proof shall be whether *UIM* or its *National Association* has established an anti-doping rule violation to the comfortable satisfaction of the hearing panel bearing in mind the seriousness of the allegation which is made. This standard of proof in all cases is greater than a mere balance of probability but less than proof beyond a reasonable doubt. Where these Rules place the burden of proof upon the *Driver* or other *Person* alleged to have committed an anti-doping rule violation to rebut a presumption or establish specified facts or circumstances, the standard of proof shall be by a balance of probability, except as provided in Articles 10.4 and 10.6, where the *Driver* must satisfy a higher burden of proof.

[Comment to Article 3.1: This standard of proof required to be met by UIM or its National Association is comparable to the standard which is applied in most countries to cases involving professional misconduct. It has also been widely applied by courts and hearing panels in doping cases. See, for example, the CAS decision in N., J., Y., W. v. FINA, CAS 98/208, 22 December 1998.]

3.2 Methods of Establishing Facts and Presumptions

Facts related to anti-doping rule violations may be established by any reliable means, including admissions. The following rules of proof shall be applicable in doping cases:

[Comment to Article 3.2: For example, UIM or its National Association may establish an anti-doping rule violation under Article 2.2 (Use of a Prohibited Substance or Prohibited Method) based on the Driver's admissions, the credible testimony of third Persons, reliable documentary evidence, reliable analytical data from either an A or B Sample as provided in the Comments to Article 2.2, or conclusions drawn from the profile of a series of the Driver's blood or urine Samples.]

3.2.1 WADA-accredited laboratories are presumed to have conducted *Sample* analysis and custodial procedures in accordance with the *International Standard* for Laboratories. The *Driver* or other *Person* may rebut this presumption by establishing that a departure from the *International Standard* occurred which could reasonably have caused the *Adverse Analytical Finding*.

If the *Driver* or other *Person* rebuts the preceding presumption by showing that a departure from the *International Standard* occurred which could reasonably have caused the *Adverse Analytical Finding*, then *UIM* or its *National Association* shall have the burden to establish that such departure did not cause the *Adverse Analytical Finding*.

[Comment to Article 3.2.1: The burden is on the Driver or other Person to establish, by a balance of probability, a departure from the International Standard that could reasonably have caused the Adverse Analytical Finding. If the Driver or other Person does so, the burden shifts to UIM or its National Association to prove to the comfortable satisfaction of the hearing panel that the departure did not cause the Adverse Analytical Finding.]

3.2.2 Departures from any other *International Standard* or other anti-doping rule or policy which did not cause an *Adverse Analytical Finding* or other anti-doping rule violation shall not invalidate such results. If the *Driver* or other *Person* establishes that a departure from another *International Standard* or other anti-doping rule or policy which could reasonably have caused the *Adverse Analytical Finding* or other anti-doping rule violation occurred, then *UIM* or its *National Association* shall have the burden to establish that such a departure did not cause the *Adverse Analytical Finding* or the factual basis for the anti-doping rule violation.

3.2.3 The facts established by a decision of a court or professional disciplinary tribunal of competent jurisdiction which is not the subject of a pending appeal shall be irrebuttable evidence against the *Driver* or other *Person* to whom the decision pertained of those facts unless the *Driver* or other *Person* establishes that the decision violated principles of natural justice.

3.2.4 The hearing panel in a hearing on an anti-doping rule violation may draw an inference adverse to the *Driver* or other *Person* who is asserted to have committed an anti-doping rule violation based on the *Driver's* or other *Person's* refusal, after a request made in a reasonable time in advance of the hearing, to appear at the hearing (either in person or telephonically as directed by the tribunal) and to answer questions either from the hearing panel or from the *Anti-Doping Organization* asserting the anti-doping rule violation.

[Comment to Article 3.2.4: Drawing an adverse inference under these circumstances has been recognized in numerous CAS decisions.]

ARTICLE 4 THE PROHIBITED LIST

4.1 Incorporation of the Prohibited List

These Anti-Doping Rules incorporate the *Prohibited List* which is published and revised by WADA as described in Article 4.1 of the *Code*. UIM will make the current *Prohibited List* available to each *National Association*, and each *National Association* shall ensure that the current *Prohibited List* is available to its members and constituents.

[Comment to Article 4.1: The Prohibited List will be revised and published on an expedited basis whenever the need arises. However, for the sake of predictability, a new Prohibited List will be published every year whether or not changes have been made. The Prohibited List in force is available on WADA's website at www.wada-ama.org. The Prohibited List is an integral part of the International Convention against Doping in Sport.]

4.2 Prohibited Substances and Prohibited Methods Identified on the Prohibited List

4.2.1 Prohibited Substances and Prohibited Methods

Unless provided otherwise in the *Prohibited List* and/or a revision, the *Prohibited List* and revisions shall go into effect under these Anti-Doping Rules three months after publication of the *Prohibited List* by WADA without requiring any further action by UIM. As described in Article 4.2 of the *Code*, UIM may request that WADA expand the *Prohibited List* for the sport of Powerboating. UIM may also upon the recommendation of its Anti-Doping Commission request that WADA include additional substances or methods, which have the potential for abuse in the sport of Powerboating, in the monitoring program described in Article 4.5 of the *Code*. As provided in the *Code*, WADA shall make the final decision on such requests by UIM.

[Comment to Article 4.2.1: There will be one Prohibited List. The substances which are prohibited at all times would include masking agents and those substances which, when used in training, may have long term performance enhancing effects such as anabolics. All substances and methods on the Prohibited List are prohibited In-Competition. Out-of-Competition Use (Article 2.2) of a substance which is only prohibited In-Competition is not an anti-doping rule violation unless an Adverse Analytical Finding for the substance or its Metabolites is reported for a Sample collected In-Competition (Article 2.1).]

There will be only one document called the "Prohibited List." WADA may add additional substances or methods to the Prohibited List for particular sports (e.g. the inclusion of beta-blockers for shooting) but this will also be reflected on the

single Prohibited List. A particular sport is not permitted to seek exemption from the basic list of Prohibited Substances (e.g. eliminating anabolics from the Prohibited List for “mind sports”). The premise of this decision is that there are certain basic doping agents which anyone who chooses to call himself or herself a Driver should not take.]

4.2.2 Specified Substances

For purposes of the application of Article 10 (Sanctions on Individuals), all *Prohibited Substances* shall be “Specified Substances” except (a) substances in the classes of anabolic agents and hormones; and (b) those stimulants and hormone antagonists and modulators so identified on the *Prohibited List*. *Prohibited Methods* shall not be Specified Substances.

4.2.3 New Classes of Prohibited Substances

In the event WADA expands the *Prohibited List* by adding a new class of *Prohibited Substances* in accordance with Article 4.1 of the *Code*, WADA’s Executive Committee shall determine whether any or all *Prohibited Substances* within the new class of *Prohibited Substances* shall be considered Specified Substances under Article 4.2.2.

4.3 Criteria for Including Substances and Methods on the Prohibited List

As provided in Article 4.3.3 of the *Code*, WADA’s determination of the *Prohibited Substances* and *Prohibited Methods* that will be included on the *Prohibited List* and the classification of substances into categories on the *Prohibited List* is final and shall not be subject to challenge by a Driver or other *Person* based on an argument that the substance or method was not a masking agent or did not have the potential to enhance performance, represent a health risk or violate the spirit of sport.

[Comment to Article 4.3: The question of whether a substance meets the criteria in Article 4.3 (Criteria for Including Substances and Methods on the Prohibited List) in a particular case cannot be raised as a defense to an anti-doping rule violation. For example, it cannot be argued that the Prohibited Substance detected would not have been performance enhancing in that particular sport. Rather, doping occurs when a substance on the Prohibited List is found in a Driver’s Sample. Similarly, it cannot be argued that a substance listed in the class of anabolic agents does not belong in that class.]

4.4 Therapeutic Use

4.4.1 *Drivers* with a documented medical condition requiring the use of a *Prohibited Substance* or a *Prohibited Method* must first obtain a TUE. The

presence of a *Prohibited Substance* or its *Metabolites* or *Markers* (Article 2.1), *Use* or *Attempted Use* of a *Prohibited Substance* or a *Prohibited Method* (Article 2.2), *Possession* of *Prohibited Substances* or *Prohibited Methods* (Article 2.6) or administration of a *Prohibited Substance* or *Prohibited Method* (Article 2.8) consistent with the provisions of an applicable TUE issued pursuant to the *International Standard for Therapeutic Use Exemptions* shall not be considered an anti-doping rule violation.

4.4.2 Subject to Article 4.4.3, *Drivers* included by UIM in its *Registered Testing Pool* and other *Drivers* participating in any *International Event* must obtain a TUE from UIM (regardless of whether the *Driver* previously has received a TUE at the national level). The application for a TUE must be made as soon as possible (in the case of a *Driver* in the *Registered Testing Pool*, this would be when he/she is first notified of his/her inclusion in the pool) and in any event (save in emergency situations) no later than 21 days before the *Driver's* participation in the *Event*.

4.4.3 The only exception to Article 4.4.2 is that, in accordance with Article 7.13 of the *International Standard for Therapeutic Use Exemptions*, *Drivers* not in UIM's *Registered Testing Pool* who inhale Glucocorticosteroids and/or formoterol, salbutamol, salmeterol or terbutaline to treat asthma or one of its clinical variants do not need a TUE in advance of participating in an *International Event* unless so specified by UIM. Instead, if necessary, any such *Driver* may apply for a Retroactive TUE after the *Event* in accordance with Article 7.13 of the *International Standard for Therapeutic Use Exemptions* and Article 7.1.3 of these Anti-Doping Rules.

4.4.4 TUE's granted by UIM shall be reported to the *Driver's National Association* and to WADA. Other *Drivers* subject to *Testing* who need to use a *Prohibited Substance* or a *Prohibited Method* for therapeutic reasons must obtain a TUE from their *National Anti-Doping Organization* or other body designated by their *National Association*, as required under the rules of the *National Anti-Doping Organization/other body*. *National Associations* shall promptly report any such TUE's to UIM and WADA.

4.4.5 The UIM Executive shall appoint a panel of physicians to consider requests for TUE's (the "**TUE Committee**"). Upon UIM's receipt of a TUE request, the Chair of the TUE Panel shall appoint one or more members of the TUE Panel (which may include the Chair) to consider such request. The TUE Committee member(s) so designated shall promptly evaluate such request in accordance with the *International Standard for*

Therapeutic Use Exemptions and render a decision on such request, which shall be the final decision of UIM.

4.4.6 *WADA*, at the request of a Driver or on its own initiation, may review the granting or denial of any TUE by *UIM* if *WADA* determines that the granting or denial of a TUE did not comply with the *International Standard* for Therapeutic Use Exemptions in force at the time then *WADA* may reverse that decision. Decisions on TUE's are subject to further appeal as provided in Article 13.

ARTICLE 5 TESTING

5.1 Authority to Test

All *Drivers* under the jurisdiction of a *National Association* shall be subject to *In-Competition Testing* by *UIM*, the *Driver's National Association*, and any other *Anti-Doping Organization* responsible for *Testing* at a *Competition* or *Event* in which they participate. All *Drivers* under the jurisdiction of a *National Association*, including *Drivers* serving a period of ineligibility or a *Provisional Suspension*, shall also be subject to *Out-of-Competition Testing* at any time or place, with or without advance notice, by *UIM*, *WADA*, the *Driver's National Association*, the *National Anti-Doping Organization* of any country where the Driver is present. *Target Testing* will be made a priority.

[Comment to Article 5.1: Target Testing is specified because random Testing, or even weighted random Testing, does not ensure that all of the appropriate Drivers will be tested (e.g., world-class Drivers, Drivers whose performances have dramatically improved over a short period of time, Drivers whose coaches have had other Drivers test positive, etc.). Obviously, Target Testing must not be used for any purposes other than legitimate Doping Control. The Code makes it clear that Drivers have no right to expect that they will be tested only on a random basis. Similarly, it does not impose any reasonable suspicion or probable cause requirement for Target Testing]

5.2 Responsibility for UIM Testing

The *UIM* shall be responsible for drawing up a test distribution plan for the sport of Powerboating in accordance with Article 4 of the *International Standard for Testing*, and for the implementation of that plan, including overseeing all *Testing* conducted by or on behalf of *UIM Testing* may be conducted by members of the *UIM* or by other qualified persons so authorized by *UIM*.

5.3 Testing Standards

Testing conducted by *UIM* and its *National Associations* shall be in substantial conformity with the *International Standard for Testing* in force at the time of *Testing*.

5.3.1 Blood or other non-urine *Samples* may be used to detect *Prohibited Substances* or *Prohibited Methods*, for screening procedure purposes, or for longitudinal biological profiling. If the *Sample* is collected for screening only, it will have no consequences for the *Driver* other than to identify him/her for a urine test under these anti-doping rules. In these circumstances, the *UIM* may decide at its own discretion which parameters are to be measured in the screening *Sample* and what levels of those parameters will be used to indicate that a *Driver* should be selected for a urine test. If however, the *Sample* is collected for longitudinal biological profiling, it may be used for anti-doping purposes in accordance with Article 2.2 of the *Code*.

5.4 Coordination of Testing

UIM and *National Associations* shall promptly report completed tests through the *WADA* clearinghouse to avoid unnecessary duplication in *Testing*.

5.5 Driver Whereabouts Requirements

5.5.1 *UIM* shall identify a *Registered Testing Pool* of those *Drivers* who are required to comply with the whereabouts requirements of the *International Standard for Testing*, and shall publish the criteria for *Drivers* to be included in this *Registered Testing Pool* as well as a list of the *Drivers* meeting those criteria for the period in question. *UIM* shall review and update as necessary its criteria for including *Drivers* in its *Registered Testing Pool*, and shall revise the membership of its *Registered Testing Pool* from time to time as appropriate in accordance with the set criteria. Each *Driver* in the *Registered Testing Pool* (a) shall advise *UIM* of his/her whereabouts on a quarterly basis, in the manner set out in Article 11.3 of the *International Standard for Testing*; (b) shall update that information as necessary, in accordance with Article 11.4.2 of the *International Standard for Testing*, so that it remains accurate and complete at all times; and (c) shall make him/herself available for *Testing* at such whereabouts, in accordance with Article 11.4 of the *International Standard for Testing*.

[*Comment to Article 5.5.1: The purpose of the UIM Registered Testing Pool is to identify top-level International Drivers who the UIM requires to provide whereabouts information to facilitate Out-of-Competition Testing by UIM and*

other Anti-Doping Organizations with jurisdiction over the Drivers. UIM will identify such Drivers in accordance with the requirements of Articles 4 and 11.2 of the International Standard for Testing.]

Examples for the criteria which could be used separately or in combination include:

- All medalists / team members of (Team) medalists from the previous (one, two,...) world championships / continental championships / top 10 Drivers in each discipline or in World Ranking or World Cup.*
- Any Driver whose performance is better than or equal to the fiftieth best performances in the preceding competition year.*
- All Drivers who are members of the top 8 teams in the Team Ranking*

Every National Association shall report to UIM the performances, names and addresses of all Drivers whose performances fall within the Registered Testing Pool criteria established by UIM.

5.5.2 A Driver's failure to advise UIM of his/her whereabouts shall be deemed a Filing Failure for purposes of Article 2.4 where the conditions of Article 11.3.5 of the *International Standard for Testing* are met.

5.5.3 A Driver's failure to be available for Testing at his/her declared whereabouts shall be deemed a Missed Test for purposes of Article 2.4 where the conditions of Article 11.4.3 of the *International Standard for Testing* are met.

5.5.4 Each *National Association* shall also assist its *National Anti-Doping Organization* in establishing a national level *Registered Testing Pool* of top level national *Drivers* to whom the whereabouts requirements of the *International Standard for Testing* shall also apply. Where those *Drivers* are also in the UIM's *Registered Testing Pool*, the UIM and the *National Anti-Doping Organization* will agree on which of them will take responsibility for receiving whereabouts filings from the *Driver* and sharing it with the other (and with other *Anti-Doping Organizations*) in accordance with Article 5.5.5.

5.5.5 Whereabouts information provided pursuant to Articles 5.5.1 and 5.5.4 shall be shared with *WADA* and other *Anti-Doping Organizations* having jurisdiction to test a *Driver* in accordance with Articles 11.7.1(d) and 11.7.3(d) of the *International Standard for Testing*, including the strict condition that it be used only for *Doping Control* purposes.

5.6 Retirement and Return to Competition

5.6.1 A Driver who has been identified by UIM for inclusion in UIM's *Registered Testing Pool* shall continue to be subject to these Anti-Doping Rules, including the obligation to comply with the whereabouts requirements of the *International Standard for Testing* unless and until the *Driver* gives written notice to UIM that he or she has retired or until he or she no longer satisfies the criteria for inclusion in UIM's *Registered Testing Pool* and has been so informed by UIM.

5.6.2 A Driver who has given notice of retirement to UIM may not resume competing unless he or she notifies UIM at least six months before he or she expects to return to competition and makes him/herself available for unannounced *Out-of-Competition Testing*, including complying with the whereabouts requirements of the *International Standard for Testing*, at any time during the period before actual return to competition.

5.6.3 *National Associations/National Anti-Doping Organizations* may establish similar requirements for retirement and returning to competition for *Drivers* in the national *Registered Testing Pool*.

5.7 Selection of Drivers to be Tested

5.7.1 At *International Events*, the UIM shall determine the number of finishing placement tests, random tests and target tests to be performed.

The following *Drivers* shall be tested for each *Competition* at an *International Event*.

Each *Driver* finishing in one of the top three placements in random disciplines in the *Competition*, plus one other *Driver* in the *Competition* selected at random.

5.7.2 At *National Events*, each *National Association* shall determine the number of *Drivers* selected for *Testing* in each *Competition* and the procedures for selecting the *Drivers* for *Testing*.

5.7.3 In addition to the selection procedures set forth in Articles 5.7.1 and 5.7.2 above, the UIM at *International Events*, and the *National Association* at *National Events*, may also select *Drivers* or teams for *Target Testing* so long as such *Target Testing* is not used for any purpose other than legitimate *Doping Control* purposes.

5.7.4 *Drivers* shall be selected for *Out-of-Competition Testing* by the UIM and by *National Associations* through a process that substantially

complies with the *International Standard for Testing* in force at the time of selection.

5.8 National Associations and the organizing committees for *National Association Events* shall provide access to *Independent Observers at Events* as directed by UIM.

ARTICLE 6 ANALYSIS OF SAMPLES

Doping Control Samples collected under these Anti-Doping Rules shall be analyzed in accordance with the following principles:

6.1 Use of Approved Laboratories

UIM shall send *Doping Control Samples* for analysis only to WADA-accredited laboratories or as otherwise approved by WADA. The choice of the WADA-accredited laboratory (or other laboratory or method approved by WADA) used for the *Sample* analysis shall be determined exclusively by UIM.

[Comment to Article 6.1: Violations of Article 2.1 (Presence of a Prohibited Substance or its Metabolites or Markers) may be established only by Sample analysis performed by a WADA-approved laboratory or another laboratory specifically authorized by WADA. Violations of other Articles may be established using analytical results from other laboratories so long as the results are reliable.]

6.2 Purpose of Collection and Analysis of Samples

Samples shall be analyzed to detect *Prohibited Substances* and *Prohibited Methods* identified on the *Prohibited List* and other substances as may be directed by WADA pursuant to the Monitoring Program described in Article 4.5 of the *Code* or to assist UIM in profiling relevant parameters in a Driver's urine, blood or other matrix, including DNA or genomic profiling, for anti-doping purposes.

[Comment to Article 6.2: For example, relevant profile information could be used to direct Target Testing or to support an anti-doping rule violation proceeding under Article 2.2 (Use of a Prohibited Substance), or both.]

6.3 Research on Samples

No *Sample* may be used for any purpose other than as described in Article 6.2 without the *Driver's* written consent. *Samples* used (with the Driver's consent)

for purposes other than Article 6.2 shall have any means of identification removed such that they cannot be traced back to a particular *Driver*.

6.4 Standards for Sample Analysis and Reporting

Laboratories shall analyze *Doping Control Samples* and report results in conformity with the *International Standard* for Laboratories.

6.5 Retesting Samples

A *Sample* may be reanalyzed for the purposes described in Article 6.2 at any time exclusively at the direction of UIM or WADA. The circumstances and conditions for retesting *Samples* shall conform with the requirements of the *International Standard* for Laboratories.

[Comment to Article 6.5: Although this Article is new, Anti-Doping Organizations have always had the authority to reanalyze Samples. The International Standard for Laboratories or a new technical document which is made a part of the International Standard will harmonize the protocol for such retesting.]

ARTICLE 7 RESULTS MANAGEMENT

7.1 Results Management for Tests Initiated by UIM

Results management for tests initiated by UIM (including tests performed by WADA pursuant to agreement with UIM) shall proceed as set forth below:

7.1.1 The results from all analyses must be sent to UIM in encoded form, in a report signed by an authorised representative of the laboratory. All communication must be conducted in confidentiality and in conformity with ADAMS, a database management tool developed by WADA. ADAMS is consistent with data privacy statutes and norms applicable to WADA and other organizations using it.

7.1.2 Upon receipt of an *A Sample Adverse Analytical Finding*, the UIM Independent Doping Review Panel shall conduct a review to determine whether: (a) the *Adverse Analytical Finding* is consistent with an applicable TUE, or (b) there is any apparent departure from the *International Standard* for Testing or *International Standard* for Laboratories that caused the *Adverse Analytical Finding*.

[7.1.2.1 The UIM Executive shall appoint an Independent Doping Review Panel consisting of a Chair and 2 other members with experience in anti-doping. All members of the panel shall be otherwise

independent from UIM Each panel member shall serve a term of four years. In each case the Chair of the panel shall appoint 1 or more members of the panel (which may include the Chair) to conduct the review discussed in Articles 7.1.2 and 7.1.8 and to review any other potential violations of these Anti-Doping Rules as may be requested by UIM.

7.1.3 In the following circumstances:

- (a) The *Adverse Analytical Finding* is for a Glucocorticosteroid, formoterol, salbutamol, salmeterol or terbutaline; and
- (b) The *Sample* in question was provided by a Driver who is not in UIM's *Registered Testing Pool*, during his/her participation in an *International Event* for which (in accordance with Article 7.13 of the *International Standard* for Therapeutic Use Exemptions and Article 4.4.3 of these Anti-Doping Rules UIM does not require a TUE for asthma medication in advance;

then, before the matter is referred to UIM Independent Review Panel under Article 7.1, the *Driver* shall be given an opportunity to apply to the TUE Committee for a Retroactive TUE in accordance with Article 7.13 of the *International Standard* for Therapeutic Use Exemptions. The result of that application shall be forwarded to the UIM Independent Review Panel for consideration in its review of the *Adverse Analytical Finding* under Article 7.1.2.

7.1.4 If the initial review of an *Adverse Analytical Finding* under Article 7.1.2 does not reveal an applicable TUE, or departure from the *International Standard* for Testing or the *International Standard* for Laboratories that caused the *Adverse Analytical Finding*, UIM shall promptly notify the *Driver* of: (a) the *Adverse Analytical Finding*; (b) the anti-doping rule violated; (c) the *Driver's* right to promptly request the analysis of the B *Sample* or, failing such request, that the B *Sample* analysis may be deemed waived; (d) the scheduled date, time and place for the B *Sample* analysis (which shall be within the time period specified in the *International Standard* for Laboratories) if the *Driver* or UIM chooses to request an analysis of the B *Sample*; (e) the opportunity for the *Driver* and/or the *Driver's* representative to attend the B *Sample* opening and analysis at the scheduled date, time and place if such analysis is requested; and (f) the *Driver's* right to request copies of the A and B *Sample* laboratory documentation package which includes information as required by the *International Standard* for Laboratories. UIM shall also notify the *Driver's National Anti-Doping Organization* and WADA. If UIM decides not to bring forward the *Adverse Analytical Finding* as an anti-

doping rule violation, it shall so notify the *Driver*, the *Driver's National Anti-Doping Organization* and *WADA*.

7.1.5 Where requested by the *Driver* or UIM, arrangements shall be made for *Testing* the *B Sample* within the time period specified in the *International Standard for Testing*. A *Driver* may accept the *A Sample* analytical results by waiving the requirement for *B Sample* analysis. UIM may nonetheless elect to proceed with the *B Sample* analysis.

7.1.6 The *Driver* and/or his representative shall be allowed to be present at the analysis of the *B Sample* within the time period specified in the *International Standard for Laboratories*. Also a representative of the *Driver's National Association* as well as a representative of UIM shall be allowed to be present.

7.1.7 If the *B Sample* proves negative, then (unless UIM takes the case forward as an anti-doping rule violation under Article 2.2) the entire test shall be considered negative and the *Driver*, his *National Association*, and UIM shall be so informed.

7.1.8 If a *Prohibited Substance* or the *Use of a Prohibited Method* is identified, the findings shall be reported to the *Driver*, his *National Association*, UIM, and to *WADA*.

7.1.9 For apparent anti-doping rule violations that do not involve *Adverse Analytical Findings*, UIM shall conduct any necessary follow-up investigation and, at such time as it is satisfied that an anti-doping rule violation has occurred, it shall then promptly notify the *Driver* of the anti-doping rule which appears to have been violated, and the basis of the violation.

7.2 Results Management for Atypical Findings

7.2.1 As provided in the *International Standards*, in certain circumstances laboratories are directed to report the presence of *Prohibited Substances* that may also be produced endogenously as *Atypical Findings* that should be investigated further.

7.2.2 If a laboratory reports an *Atypical Finding* in respect of a *Sample* collected from a *Driver* by or on behalf of UIM, the UIM Independent Doping Review Panel shall conduct a review to determine whether: (a) the *Atypical Finding* is consistent with an applicable TUE that has been granted as provided in the *International Standard for Therapeutic Use Exemptions*, or (b) there is any apparent departure from the *International*

Standard for Testing or *International Standard* for Laboratories that caused the *Atypical Analytical Finding*.

7.2.3 If the initial review of an *Atypical Finding* under Article 7.2.2 reveals an applicable TUE or departure from the *International Standard for Testing* or the *International Standard* for Laboratories that caused the *Atypical Finding*, the entire test shall be considered negative and the *Driver*, his *National Association*, and UIM shall be so informed.

7.2.4 If the initial review of an *Atypical Finding* under Article 7.2.2 does not reveal an applicable TUE or departure from the *International Standard for Testing* or the *International Standard* for Laboratories that caused the *Atypical Finding*, UIM shall conduct the follow-up investigation required by the *International Standards*. If, once that investigation is completed, it is concluded that the *Atypical Finding* should be considered an *Adverse Analytical Finding*, UIM shall pursue the matter in accordance with Article 7.1.3.

7.2.5 UIM will not provide notice of an *Atypical Finding* until it has completed its investigation and has decided whether it will bring the *Atypical Finding* forward as an *Adverse Analytical Finding* unless one of the following circumstances exists:

If UIM determines the B *Sample* should be analyzed prior to the conclusion of its follow-up investigation, it may conduct the B *Sample* analysis after notifying the *Driver*, with such notice to include a description of the *Atypical Finding* and the information described in Article 7.1.3(c) to (f).

7.3 Results Management for Tests Initiated During Other International Events

Results management and the conduct of hearings from a test by a Major Event Organization, shall be managed, as far as sanctions beyond Disqualification from the Event or the results of the Event, by UIM.

7.4 Results Management for Tests initiated by National Associations

Results management conducted by *National Associations* shall be consistent with the general principles for effective and fair results management which underlie the detailed provisions set forth in Article 7. Results of all *Doping Controls* shall be reported to UIM and to WADA within 14 days of the conclusion of the *National Association's* results management process. Any apparent anti-doping rule violation by a *Driver* who is a member of that *National Association* shall be promptly referred to an appropriate hearing panel established pursuant

to the rules of the *National Association* or national law. Apparent anti-doping rule violations by *Drivers* who are members of another *National Association* shall be referred to the *Driver's National Association* for hearing.

7.5 Results Management for Whereabouts Violations

7.5.1 Results management in respect of an apparent *Filing Failure* by a *Driver* in UIM's *Registered Testing Pool* shall be conducted by UIM in accordance with Article 11.6.2 of the *International Standard for Testing* (unless it has been agreed in accordance with Article 5.5.4 that the *National Association* or *National Anti-Doping Organization* shall take such responsibility).

7.5.2 Results management in respect of an apparent *Missed Test* by a *Driver* in UIM's *Registered Testing Pool* as a result of an attempt to test the *Driver* by or on behalf of UIM shall be conducted by UIM in accordance with Article 11.6.3 of the *International Standard for Testing*. Results management in respect of an apparent *Missed Test* by such *Driver* as a result of an attempt to test the *Driver* by or on behalf of another *Anti-Doping Organization* shall be conducted by that other *Anti-Doping Organization* in accordance with Article 11.7.6(c) of the *International Standard for Testing*.

7.5.3 Where, in any eighteen-month period, a *Driver* in UIM's *Registered Testing Pool* is declared to have three *Filing Failures*, or three *Missed Tests*, or any combination of *Filing Failures* or *Missed Tests* adding up to three in total, whether under these *Anti-Doping Rules* or under the rules of any other *Anti-Doping Organization*, UIM shall bring them forward as an apparent anti-doping rule violation.

7.6 Provisional Suspensions

7.6.1 If analysis of an *A Sample* has resulted in an *Adverse Analytical Finding* for a *Prohibited Substance* that is not a *Specified Substance*, and a review in accordance with Article 7.1.2 does not reveal an applicable *TUE* or departure from the *International Standard for Testing* or the *International Standard for Laboratories* that caused the *Adverse Analytical Finding*, UIM shall *Provisionally Suspend* the *Driver* pending the hearing panel's determination of whether he/she has committed an anti-doping rule violation.

7.6.2 In any case not covered by Article 7.6.1 where UIM decides to take the matter forward as an apparent anti-doping rule violation in accordance with the foregoing provisions of this Article 7, the UIM Executive, after

consultation with the UIM Independent Review Panel, may Provisionally Suspend the *Driver* pending the hearing panel's determination of whether he/she has committed an anti-doping rule violation.

7.6.3 Where a *Provisional Suspension* is imposed, whether pursuant to Article 7.6.1 or Article 7.6.2, the *Driver* shall be given either (a) an opportunity for a *Provisional Hearing* before imposition of the *Provisional Suspension* or on a timely basis after imposition of the *Provisional Suspension*; or (b) an opportunity for an expedited hearing in accordance with Article 8 (Right to a Fair Hearing) on a timely basis after imposition of a *Provisional Suspension*. *National Associations* shall impose *Provisional Suspensions* in accordance with the principles set forth in this Article 7.6.

7.6.4 If a *Provisional Suspension* is imposed based on an *Adverse Analytical Finding* in respect of an *A Sample*, and any subsequent analysis of the *B Sample* analysis does not confirm the *A Sample* analysis, then the *Driver* shall not be subject to any further *Provisional Suspension* on account of a violation of Article 2.1 of the *Code* (Presence of a *Prohibited Substance* or its *Metabolites* or *Markers*). In circumstances where the *Driver* or the *Driver's* team as may be provided in the rules of the UIM has been removed from a *Competition* based on a violation of Article 2.1 and the subsequent *B Sample* analysis does not confirm the *A Sample* finding, UIM, without otherwise affecting the *Competition*, it is still possible for the *Driver* or team to be reinserted, the *Driver* or team may continue to take part in the *Competition*.

[Comment to Article 7.6: Before a Provisional Suspension can be unilaterally imposed by an Anti-Doping Organization, the internal review specified in the Code must first be completed. In addition, a Signatory imposing a Provisional Suspension is required to give the Driver an opportunity for a Provisional Hearing either before or promptly after the imposition of the Provisional Suspension, or an expedited final hearing under Article 8 promptly after imposition of the Provisional Suspension. The Driver has a right to appeal under Article 13.2.]

In the rare circumstance where the B Sample analysis does not confirm the A Sample finding, the Driver who had been provisionally suspended will be allowed, where circumstances permit, to participate in subsequent Competitions during the Event..

Drivers shall receive credit for a Provisional Suspension against any period of Ineligibility which is ultimately imposed as provided in Article 10.9.3.]

7.7 Retirement from Sport

If a Driver or other *Person* retires while a results management process is underway, UIM retains jurisdiction to complete its results management process. If a Driver or other *Person* retires before any results management process has begun and UIM would have had results management jurisdiction over the *Driver* or other *Person* at the time the *Driver* or other *Person* committed an anti-doping rule violation, UIM has jurisdiction to conduct results management.

[Comment to Article 7.7: Conduct by a Driver or other Person before the Driver or other Person was subject to the jurisdiction of any Anti-Doping Organization would not constitute an anti-doping rule violation but could be a legitimate basis for denying the Driver or other Person membership in a sports organization.]

ARTICLE 8 RIGHT TO A FAIR HEARING

8.1 Hearings arising out of UIM Testing or Tests at International Events

8.1.1 The UIM Executive shall appoint a standing panel consisting of a Chair and four other experts with experience in anti-doping (“UIM Doping Hearing Panel”). The Chair shall be a lawyer. Each panel member shall be otherwise independent of UIM. Each panel member shall serve a term of four years.

8.1.2 When it appears, following the Results Management process described in Article 7, that these Anti-Doping Rules have been violated in connection with UIM *Testing* or *Testing* at an *International Event* then the case shall be assigned to the UIM Doping Hearing Panel for adjudication.

8.1.3 The Chair of the UIM Doping Hearing Panel shall appoint three members from the panel (which may include the Chair) to hear each case. At least one appointed member shall be a lawyer. The appointed members shall have had no prior involvement with the case and shall not have the same nationality as the *Driver* or other *Person* alleged to have violated these Anti-Doping Rules.

8.1.4 Hearings pursuant to this Article shall be completed expeditiously following the completion of the results management process described in Article 7. Hearings held in connection with *Events* may be conducted on an expedited basis.

8.1.5 The *National Association* of the *Driver* or other *Person* alleged to have violated these Anti-Doping Rules may attend the hearing as an observer.

8.1.6 UIM shall keep WADA fully apprised as to the status of pending cases and the result of all hearings.

8.1.7 A Driver or other *Person* may forego a hearing by acknowledging the Anti-Doping Rule violation and accepting *Consequences* consistent with Articles 9 and 10 as proposed by UIM. The right to a hearing may be waived either expressly or by the *Driver's* or other *Person's* failure to challenge UIM's assertion that an anti-doping rule violation has occurred within 28 days. Where no hearing occurs, UIM shall submit to the persons described in Article 13.2.3 a reasoned decision explaining the action taken.

8.1.8 Decisions of the UIM Doping Hearing Panel may be appealed to Court of Arbitration for Sport as provided in Article 13.

8.2 Hearings Arising Out of National Testing

8.2.1 When it appears, following the Results Management process described in Article 7, that these Anti-Doping Rules have been violated in connection with *Testing* other than in connection with UIM *Testing* or *Testing* at an *International Event*, the *Driver* or other *Person* involved shall be brought before a disciplinary panel of the *Driver* or other *Person's National Association* for a hearing to adjudicate whether a violation of these Anti-Doping Rules occurred and if so what *Consequences* should be imposed.

8.2.2 Hearings pursuant to this Article 8.2 shall be completed expeditiously and in all cases within three months of the completion of the Results Management process described in Article 7. Hearings held in connection with *Events* may be conducted by an expedited process. If the completion of the hearing is delayed beyond three months, UIM may elect to bring the case directly before the UIM Doping Hearing Panel at the responsibility and at the expense of the *National Association*.

8.2.3 *National Associations* shall keep UIM and WADA fully apprised as to the status of pending cases and the results of all hearings.

8.2.4 UIM and WADA shall have the right to attend hearings as an observer.

8.2.5 The *Driver* or other *Person* may forego a hearing by acknowledging the violation of these Anti-Doping Rules and accepting *Consequences* consistent with Articles 9 and 10 as proposed by the *National Association*. The right to a hearing may be waived either expressly or by the *Driver's* or other *Person's* failure to challenge the *National Association's* assertion

that an anti-doping rule violation has occurred within 28 days. Where no hearing occurs, the *National Association* shall submit to the persons described in Article 13.2.3 a reasoned decision explaining the action taken.

8.2.6 Decisions by *National Associations*, whether as the result of a hearing or the *Driver* or other *Person's* acceptance of *Consequences*, may be appealed as provided in Article 13.

8.2.7 Hearing decisions by the *National Association* shall not be subject to further administrative review at the national level except as provided in Article 13 or required by applicable national law.

8.3 Principles for a Fair Hearing All hearings pursuant to either Article 8.1 or 8.2 shall respect the following principles:

- a timely hearing;
- fair and impartial hearing panel;
- the right to be represented by counsel at the *Person's* own expense;
- the right to be informed in a fair and timely manner of the asserted anti-doping rule violation;
- the right to respond to the asserted anti-doping rule violation and resulting *Consequences*;
- the right of each party to present evidence, including the right to call and question witnesses (subject to the hearing panel's discretion to accept testimony by telephone or written submission);
- the *Person's* right to an interpreter at the hearing, with the hearing panel to determine the identity, and responsibility for the cost of the interpreter; and
- a timely, written, reasoned decision, specifically including an explanation of the reason(s) for any period of *Ineligibility*.]

ARTICLE 9 AUTOMATIC DISQUALIFICATION OF INDIVIDUAL RESULTS

A violation of these Anti-Doping Rules in *Individual Sports* in connection with an *In-Competition* test automatically leads to *Disqualification* of the result obtained in that *Competition* with all resulting consequences, including forfeiture of any medals, points and prizes.

[Comment to Article 9: When a Driver wins a gold medal with a Prohibited Substance in his or her system, that is unfair to the other Drivers in that Competition regardless of whether the gold medalist was at fault in any way. Only a “clean” Driver should be allowed to benefit from his or her competitive results.

In sports which are not Team Sports but where awards are given to teams, Disqualification or other disciplinary action against the team when one or more team members have committed an anti-doping rule violation shall be as provided in the applicable rules of UIM]

ARTICLE 10 SANCTIONS ON INDIVIDUALS

10.1 Disqualification of Results in Event During which an Anti-Doping Rule Violation Occurs

An *Anti-Doping Rule* violation occurring during or in connection with an *Event* may lead to *Disqualification* of all of the *Driver's* individual results obtained in that *Event* with all consequences, including forfeiture of all medals, points and prizes, except as provided in Article 10.1.1.

[Comment to Article 10.1: Whereas Article 9 (Automatic Disqualification of Individual Results) disqualifies the result in a single Competition in which the Driver tested positive, this Article may lead to Disqualification of all results in all races during the Event. Factors to be included in considering whether to disqualify other results in an Event might include, for example, the severity of the Driver's anti-doping rule violation and whether the Driver tested negative in the other Competitions.]

10.1.1 If the *Driver* establishes that he or she bears *No Fault or Negligence* for the violation, the *Driver's* individual results in the other *Competition* shall not be *Disqualified* unless the *Driver's* results in *Competition* other than the *Competition* in which the anti-doping rule violation occurred were likely to have been affected by the *Driver's* anti-doping rule violation.

10.2 Ineligibility for Presence, Use or Attempted Use, or Possession of Prohibited Substances and Prohibited Methods

The period of *Ineligibility* imposed for a violation of Article 2.1 (Presence of *Prohibited Substance* or its *Metabolites* or *Markers*), Article 2.2 (*Use or Attempted Use of Prohibited Substance or Prohibited Method*) or Article 2.6 (*Possession of Prohibited Substances and Methods*) shall be as follows, unless the conditions for eliminating or reducing the period of *Ineligibility*, as

provided in Articles 10.4 and 10.5, or the conditions for increasing the period of *Ineligibility*, as provided in Article 10.6, are met:

First violation: Two (2) years' *Ineligibility*.

[Comment to Article 10.2: Harmonization of sanctions has been one of the most discussed and debated areas of anti-doping. Harmonization means that the same rules and criteria are applied to assess the unique facts of each case. Arguments against requiring harmonization of sanctions are based on differences between sports including, for example, the following: in some sports the Drivers are professionals making a sizable income from the sport and in others the Drivers are true amateurs; in those sports where a Driver's career is short (e.g., artistic gymnastics) a two year disqualification has a much more significant effect on the Driver than in sports where careers are traditionally much longer (e.g., equestrian and shooting); in Individual Sports, the Driver is better able to maintain competitive skills through solitary practice during disqualification than in other sports where practice as part of a team is more important. A primary argument in favor of harmonization is that it is simply not right that two Drivers from the same country who test positive for the same Prohibited Substance under similar circumstances should receive different sanctions only because they participate in different sports. In addition, flexibility in sanctioning has often been viewed as an unacceptable opportunity for some sporting bodies to be more lenient with dopers. The lack of harmonization of sanctions has also frequently been the source of jurisdictional conflicts between IFs and National Anti-Doping Organizations.]

10.3 Ineligibility for Other Anti-Doping Rule Violations

The period of *Ineligibility* for violations of these Anti-Doping Rules other than as provided in Article 10.2 shall be as follows:

10.3.1 For violations of Article 2.3 (refusing or failing to submit to *Sample* collection) or Article 2.5 (*Tampering with Doping Control*), the *Ineligibility* period shall be two (2) years unless the conditions provided in Article 10.5, or the conditions provided in Article 10.6, are met.

10.3.2 For violations of Article 2.7 (*Trafficking*) or Article 2.8 (Administration of *Prohibited Substance* or *Prohibited Method*), the period of *Ineligibility* imposed shall be a minimum of four (4) years up to lifetime *Ineligibility* unless the conditions provided in Article 10.5 are met. An anti-doping rule violation involving a *Minor* shall be considered a particularly serious violation, and, if committed by *Driver Support Personnel* for violations other than specified Substances shall result in lifetime *Ineligibility* for such *Driver Support Personnel*. In addition, significant violations of such Articles which also violate non-sporting laws and

regulations, shall be reported to the competent administrative, professional or judicial authorities.

[Comment to Article 10.3.2: Those who are involved in doping Drivers or covering up doping should be subject to sanctions which are more severe than the Drivers who test positive. Since the authority of sport organizations is generally limited to Ineligibility for credentials, membership and other sport benefits, reporting Driver Support Personnel to competent authorities is an important step in the deterrence of doping.]

10.3.3 For violations of Article 2.4 (Filing Failures and/ or Missed Tests), the period of *Ineligibility* shall be at a minimum one (1) year and at a maximum two (2) years based on the *Driver's* degree of fault.

[Comment to Article 10.3.3: The sanction under Article 10.3.3 shall be two years where all three filing failures or missed tests are inexcusable. Otherwise, the sanction shall be assessed in the range of two years to one year, based on the circumstances of the case.]

10.4 Elimination or Reduction of the Period of Ineligibility for specified Substances under specific Circumstances

Where a *Driver* or other *Person* can establish how a specified Substance entered his or her body or came into his or her possession and that such specified Substance was not intended to enhance the *Driver's* sport performance or mask the use of a performance-enhancing substance, the period of *Ineligibility* found in Article 10.2 shall be replaced with the following:

First violation: At a minimum, a reprimand and no period of *Ineligibility* from future *Events*, and at a maximum, two (2) years of *Ineligibility*.

To justify any elimination or reduction, the *Driver* or other *Person* must produce corroborating evidence in addition to his or her word which establishes to the comfortable satisfaction of the hearing panel the absence of an intent to enhance sport performance or mask the use of a performance enhancing substance. The *Driver* or other *Person's* degree of fault shall be the criterion considered in assessing any reduction of the period of *Ineligibility*.

[Comment to Article 10.4: specified Substances as now defined in Article 4.2.2 are not necessarily less serious agents for purposes of sports doping than other Prohibited Substances (for example, a stimulant that is listed as a specified Substance could be very effective to a Driver in competition); for that reason, a Driver who does not meet the criteria under this Article would receive a two-year period of Ineligibility and could receive up to a four-year period of Ineligibility under Article 10.6. However, there is a greater likelihood that specified Substances, as

opposed to other Prohibited Substances, could be susceptible to a credible, non-doping explanation.

This Article applies only in those cases where the hearing panel is comfortably satisfied by the objective circumstances of the case that the Driver in taking a Prohibited Substance did not intend to enhance his or her sport performance. Examples of the type of objective circumstances which in combination might lead a hearing panel to be comfortably satisfied of no performance-enhancing intent would include: the fact that the nature of the specified Substance or the timing of its ingestion would not have been beneficial to the Driver; the Driver's open Use or disclosure of his or her Use of the specified Substance; and a contemporaneous medical records file substantiating the non-sport-related prescription for the specified Substance. Generally, the greater the potential performance-enhancing benefit, the higher the burden on the Driver to prove lack of an intent to enhance sport performance.

While the absence of intent to enhance sport performance must be established to the comfortable satisfaction of the hearing panel, the Driver may establish how the specified Substance entered the body by a balance of probability.

In assessing the Driver's or other Person's degree of fault, the circumstances considered must be specific and relevant to explain the Driver's or other Person's departure from the expected standard of behavior. Thus, for example, the fact that a Driver would lose the opportunity to earn large sums of money during a period of Ineligibility or the fact that the Driver only has a short time left in his or her career or the timing of the sporting calendar would not be relevant factors to be considered in reducing the period of Ineligibility under this Article. It is anticipated that the period of Ineligibility will be eliminated entirely in only the most exceptional cases.]

10.5 Elimination or Reduction of Period of Ineligibility Based on Exceptional Circumstances

10.5.1 No Fault or Negligence

If a Driver establishes in an individual case that he or she bears *No Fault or Negligence*, the otherwise applicable period of *Ineligibility* shall be eliminated. When a *Prohibited Substance* or its *Markers* or *Metabolites* is detected in a *Driver's Sample* in violation of Article 2.1 (presence of *Prohibited Substance*), the *Driver* must also establish how the *Prohibited Substance* entered his or her system in order to have the period of *Ineligibility* eliminated. In the event this Article is applied and the period of *Ineligibility* otherwise applicable is eliminated, the anti-doping rule violation

shall not be considered a violation for the limited purpose of determining the period of *Ineligibility* for multiple violations under Article 10.7.

10.5.2 No Significant Fault or Negligence

If a Driver or other *Person* establishes in an individual case that he or she bears *No significant Fault or Negligence*, then the period of *Ineligibility* may be reduced, but the reduced period of *Ineligibility* may not be less than one-half of the period of *Ineligibility* otherwise applicable. If the otherwise applicable period of *Ineligibility* is a lifetime, the reduced period under this section may be no less than 8 years. When a *Prohibited Substance* or its *Markers* or *Metabolites* is detected in a Driver's *Sample* in violation of Article 2.1 (Presence of *Prohibited Substance* or its *Metabolites* or *Markers*), the *Driver* must also establish how the *Prohibited Substance* entered his or her system in order to have the period of *Ineligibility* reduced.

[Comment to Articles 10.5.1 and 10.5.2: UIM's Anti-Doping Rules provide for the possible reduction or elimination of the period of Ineligibility in the unique circumstance where the Driver can establish that he or she had No Fault or Negligence, or No significant Fault or Negligence, in connection with the violation. This approach is consistent with basic principles of human rights and provides a balance between those Anti-Doping Organizations that argue for a much narrower exception, or none at all, and those that would reduce a two year suspension based on a range of other factors even when the Driver was admittedly at fault. These Articles apply only to the imposition of sanctions; they are not applicable to the determination of whether an anti-doping rule violation has occurred. Article 10.5.2 may be applied to any anti-doping violation even though it will be especially difficult to meet the criteria for a reduction for those anti-doping rule violations where knowledge is an element of the violation.]

Articles 10.5.1 and 10.5.2 are meant to have an impact only in cases where the circumstances are truly exceptional and not in the vast majority of cases.

To illustrate the operation of Article 10.5.1, an example where No Fault or Negligence would result in the total elimination of a sanction is where a Driver could prove that, despite all due care, he or she was sabotaged by a competitor. Conversely, a sanction could not be completely eliminated on the basis of No Fault or Negligence in the following circumstances: (a) a positive test resulting from a mislabeled or contaminated vitamin or nutritional supplement (Drivers are responsible for what they ingest (Article 2.1.1) and have been warned against the possibility of supplement contamination); (b) the administration of a Prohibited Substance by the Driver's personal physician or trainer without disclosure to the Driver (Drivers are responsible for their choice of medical personnel and for advising medical personnel that they cannot be given any Prohibited Substance);

and (c) sabotage of the Driver's food or drink by a spouse, coach or other person within the Driver's circle of associates (Drivers are responsible for what they ingest and for the conduct of those persons to whom they entrust access to their food and drink). However, depending on the unique facts of a particular case, any of the referenced illustrations could result in a reduced sanction based on No significant Fault or Negligence. (For example, reduction may well be appropriate in illustration (a) If the Driver clearly establishes that the cause of the positive test was contamination in a common multiple vitamin purchased from a source with no connection to Prohibited Substances and the Driver exercised care in not taking other nutritional supplements.)

For purposes of assessing the Driver or other Person's fault under Articles 10.5.1 and 10.5.2, the evidence considered must be specific and relevant to explain the Driver or other Person's departure from the expected standard of behavior. Thus, for example the fact that a Driver would lose the opportunity to earn large sums of money during a period of Ineligibility or the fact that the Driver only has a short time left in his or her career or the timing of the sporting calendar would not be relevant factors to be considered in reducing the period of Ineligibility under this Article.

While minors are not given special treatment per se in determining the applicable sanction, certainly youth and lack of experience are relevant factors to be assessed in determining the Driver or other Person's fault under Article 10.5.2, as well as Articles 10.4 and 10.5.1.

Article 10.5.2 should not be applied in cases where Articles 10.3.3 or 10.4 apply, as those Articles already take into consideration the Driver or other Person's degree of fault for purposes of establishing the applicable period of Ineligibility.]

10.5.3 Substantial Assistance in Discovering or Establishing Anti-Doping Rule Violations

The UIM Doping Hearing Panel may, prior to a final appellate decision under Article 13 or the expiration of the time to appeal, suspend a part of the period of *Ineligibility* imposed in an individual case where the *Driver* or other *Person* has provided *Substantial Assistance* to an *Anti-Doping Organization*, criminal authority or professional disciplinary body which results in the *Anti-Doping Organization* discovering or establishing an anti-doping rule violation by another *Person* or which results in a criminal or disciplinary body discovering or establishing a criminal offense or the breach of professional rules by another *Person*. After a final appellate decision under Article 13 or the expiration of time to appeal, UIM may only suspend a part of the applicable period of *Ineligibility* with the approval of WADA. The extent to which the otherwise applicable period of *Ineligibility* may be suspended shall be based on the seriousness of the anti-doping

rule violation committed by the *Driver* or other *Person* and the significance of the *Substantial Assistance* provided by the *Driver* or other *Person* to the effort to eliminate doping in sport. No more than three-quarters of the otherwise applicable period of *Ineligibility* may be suspended. If the otherwise applicable period of *Ineligibility* is a lifetime, the non-suspended period under this Article must be no less than 8 years. If UIM suspends any part of the period of *Ineligibility* under this Article, it shall promptly provide a written justification for its decision to each *Anti-Doping Organization* having a right to appeal the decision. If UIM subsequently reinstates any part of the suspended period of *Ineligibility* because the *Driver* or other *Person* has failed to provide the *Substantial Assistance* which was anticipated, the *Driver* or other *Person* may appeal the reinstatement pursuant to Article 13.2.

[Comment to Article 10.5.3: The cooperation of Driver, Driver Support Personnel and other Persons who acknowledge their mistakes and are willing to bring other anti-doping rule violations to light is important to clean sport.]

Factors to be considered in assessing the importance of the Substantial Assistance would include, for example, the number of individuals implicated, the status of those individuals in the sport, whether a scheme involving Trafficking under Article 2.7 or administration under Article 2.8 is involved and whether the violation involved a substance or method which is not readily detectable in Testing. The maximum suspension of the Ineligibility period shall only be applied in very exceptional cases. An additional factor to be considered in connection with the seriousness of the anti-doping rule violation is any performance-enhancing benefit which the Person providing Substantial Assistance may be likely to still enjoy. As a general matter, the earlier in the results management process the Substantial Assistance is provided, the greater the percentage of the period of Ineligibility may be suspended.

If the Driver or other Person who is asserted to have committed an anti-doping rule violation claims entitlement to a suspended period of Ineligibility under this Article in connection with the Driver or other Person's waiver of a hearing under Article 8.3 (Waiver of Hearing), UIM shall determine whether a suspension of a portion of the period of Ineligibility is appropriate under this Article. If the Driver or other Person claims entitlement to a suspended period of Ineligibility before the conclusion of a hearing under Article 8 on the anti-doping rule violation, the hearing panel shall determine whether a suspension of a portion of the period of Ineligibility is appropriate under this Article at the same time the hearing panel decides whether the Driver or other Person has committed an anti-doping rule violation. If a portion of the period of Ineligibility is suspended, the decision shall explain the basis for concluding the information provided was credible and was important to discovering or proving the anti-doping rule violation or other offense.

If the Driver or other Person claims entitlement to a suspended period of Ineligibility after a final decision finding an anti-doping rule violation has been rendered and is not subject to appeal under Article 13, but the Driver or other Person is still serving the period of Ineligibility, the Driver or other Person may apply to UIM to consider a suspension in the period of Ineligibility under this Article. Any such suspension of the period of Ineligibility shall require the approval of WADA. If any condition upon which the suspension of a period of Ineligibility is based is not fulfilled, UIM shall reinstate the period of Ineligibility which would otherwise be applicable. Decisions rendered by UIM under this Article may be appealed pursuant Article 13.2.

This is the only circumstance under UIM's Anti-Doping Rules where the suspension of an otherwise applicable period of Ineligibility is authorized.]

10.5.4 Admission of an Anti-Doping Rule Violation in the Absence of Other Evidence

Where a Driver or other *Person* voluntarily admits the commission of an anti-doping rule violation before having received notice of a *Sample* collection which could establish an anti-doping rule violation (or, in the case of an anti-doping rule violation other than Article 2.1, before receiving first notice of the admitted violation pursuant to Article 7) and that admission is the only reliable evidence of the violation at the time of admission, then the period of *Ineligibility* may be reduced, but not below one-half of the period of *Ineligibility* otherwise applicable.

[Comment to Article 10.5.4: This Article is intended to apply when a Driver or other Person comes forward and admits to an anti-doping rule violation in circumstances where no Anti-Doping Organization is aware that an anti-doping rule violation might have been committed. It is not intended to apply to circumstances where the admission occurs after the Driver or other Person knows he or she is about to be caught.]

10.5.5 Where a Driver or Other Person Establishes Entitlement to Reduction in Sanction under More than One Provision of this Article

Before applying any reductions under Articles 10.5.2, 10.5.3 or 10.5.4, the otherwise applicable period of *Ineligibility* shall be determined in accordance with Articles 10.2, 10.3, 10.4 and 10.6. If the *Driver* or other *Person* establishes entitlement to a reduction or suspension of the period of *Ineligibility* under two or more of Articles 10.5.2, 10.5.3 or 10.5.4, then the period of *Ineligibility* may be reduced or suspended, but not below one-quarter of the otherwise applicable period of *Ineligibility*.

[Comment to Article 10.5.5: The appropriate sanction is determined in a sequence of four steps. First, the hearing panel determines which of the basic sanctions (Article 10.2, Article 10.3, Article 10.4 or Article 10.6) applies to the particular anti-doping rule violation. In a second step, the hearing panel establishes whether there is a basis for elimination or reduction of the sanction (Articles 10.5.1 through 10.5.4). Note, however, not all grounds for elimination or reduction may be combined with the provisions on basic sanctions. For example, Article 10.5.2 does not apply in cases involving Articles 10.3.3 or 10.4, since the hearing panel, under Articles 10.3.3 and 10.4, will already have determined the period of Ineligibility based on the Driver or other Person's degree of fault. In a third step, the hearing panel determines under Article 10.5.5 whether the Driver or other Person is entitled to a reduction under more than one provision of Article 10.5. Finally, the hearing panel decides on the commencement of the period of Ineligibility under Article 10.9. The following four examples demonstrate the proper sequence of analysis:

Example 1.

Facts: *An Adverse Analytical Finding involves the presence of an anabolic steroid; the Driver promptly admits the anti-doping rule violation as alleged; the Driver establishes No significant Fault (Article 10.5.2); and the Driver provides important Substantial Assistance (Article 10.5.3).*

Application of Article 10:

1. *The basic sanction would be two years under Article 10.2. (Aggravating circumstances (Article 10.6) would not be considered because the Driver promptly admitted the violation. Article 10.4 would not apply because a steroid is not a specified Substance.)*
2. *Based on No significant Fault alone, the sanction could be reduced up to one-half of the two years. Based on Substantial Assistance alone, the sanction could be reduced up to three-quarters of the two years.*
3. *Under Article 10.5.5, in considering the possible reduction for No significant Fault and Substantial Assistance together, the most the sanction could be reduced is up to three-quarters of the two years. Thus, the minimum sanction would be a six-month period of Ineligibility.*
4. *Under Article 10.9.2, because the Driver promptly admitted the anti-doping rule violation, the period of Ineligibility could start as early as the date of Sample collection, but in any event the Driver would have to serve at least one-half of the Ineligibility period (minimum three months) after the date of the hearing decision.*

Example 2.

Facts: An Adverse Analytical Finding involves the presence of an anabolic steroid; aggravating circumstances exist and the Driver is unable to establish that he did not knowingly commit the anti-doping rule violation; the Driver does not promptly admit the anti-doping rule violation as alleged; but the Driver does provide important Substantial Assistance (Article 10.5.3).

Application of Article 10:

1. The basic sanction would be between two and four years Ineligibility as provided in Article 10.6.
2. Based on Substantial Assistance, the sanction could be reduced up to three-quarters of the maximum four years.
3. Article 10.5.5 does not apply.
4. Under Article 10.9.2, the period of Ineligibility would start on the date of the hearing decision.

Example 3.

Facts: An Adverse Analytical Finding involves the presence of a specified Substance; the Driver establishes how the specified Substance entered his body and that he had no intent to enhance his sport performance; the Driver establishes that he had very little fault; and the Driver provides important Substantial Assistance (Article 10.5.3).

Application of Article 10:

1. Because the Adverse Analytical Finding involved a specified Substance and the Driver has satisfied the other conditions of Article 10.4, the basic sanction would fall in the range between a reprimand and two years Ineligibility. The hearing panel would assess the Driver's fault in imposing a sanction within that range. (Assume for illustration in this example that the panel would otherwise impose a period of Ineligibility of eight months.)
2. Based on Substantial Assistance, the sanction could be reduced up to three-quarters of the eight months. (No less than two months.) [No significant Fault (Article 10.2) would not be applicable because the Driver's degree of fault was already taken into consideration in establishing the eight-month period of Ineligibility in step 1.]
3. Article 10.5.5 does not apply.
4. Under Article 9.2, because the Driver promptly admitted the anti-doping rule violation, the period of Ineligibility could start as early as the date of Sample

collection, but in any event, the Driver would have to serve at least half of the Ineligibility period after the date of the hearing decision. (Minimum one month.)

Example 4.

Facts: A Driver who has never had an Adverse Analytical Finding or been confronted with an anti-doping rule violation spontaneously admits that he intentionally used multiple Prohibited Substances to enhance his performance. The Driver also provides important Substantial Assistance (Article 10.5.3).

Application of Article 10:

1. While the intentional Use of multiple Prohibited Substances to enhance performance would normally warrant consideration of aggravating circumstances (Article 10.6), the Driver's spontaneous admission means that Article 10.6 would not apply. The fact that the Driver's Use of Prohibited Substances was intended to enhance performance would also eliminate the application of Article 10.4 regardless of whether the Prohibited Substances Used were specified Substances. Thus, Article 10.2 would be applicable and the basic period of Ineligibility imposed would be two years.
2. Based on the Driver's spontaneous admissions (Article 10.5.4) alone, the period of Ineligibility could be reduced up to one-half of the two years. Based on the Driver's Substantial Assistance (Article 10.5.3) alone, the period of Ineligibility could be reduced up to three-quarters of the two years.
3. Under Article 10.5.5, in considering the spontaneous admission and Substantial Assistance together, the most the sanction could be reduced would be up to three-quarters of the two years. (The minimum period of Ineligibility would be six months.)
4. UIM Article 10.5.4 was considered by the hearing panel in arriving at the minimum six month period of Ineligibility at step 3, the period of Ineligibility would start on the date the hearing panel imposed the sanction. If, however, the hearing panel did not consider the application of Article 10.5.4 in reducing the period of Ineligibility in step 3, then under Article 10.9.2, the commencement of the period of Ineligibility could be started as early as the date the anti-doping rule violation was committed, provided that at least half of that period (minimum of three months) would have to be served after the date of the hearing decision.]

10.6 Aggravating Circumstances Which May Increase the Period of Ineligibility

If UIM establishes in an individual case involving an anti-doping rule violation other than violations under Article 2.7 (Trafficking) and 2.8 (Administration) that aggravating circumstances are present which justify the imposition of a

period of *Ineligibility* greater than the standard sanction, then the period of *Ineligibility* otherwise applicable shall be increased up to a maximum of four years unless the *Driver* or other *Person* can prove to the comfortable satisfaction of the hearing panel that he did not knowingly violate the anti-doping rule.

A *Driver* or other *Person* can avoid the application of this Article by admitting the anti-doping rule violation as asserted promptly after being confronted with the anti-doping rule violation by UIM.

[Comment to Article 10.6: Examples of aggravating circumstances which may justify the imposition of a period of Ineligibility greater than the standard sanction are: the Driver or other Person committed the anti-doping rule violation as part of a doping plan or scheme, either individually or involving a conspiracy or common enterprise to commit anti-doping rule violations; the Driver or other Person used or possessed multiple Prohibited Substances or Prohibited Methods or used or possessed a Prohibited Substance or Prohibited Method on multiple occasions; a normal individual would be likely to enjoy the performance-enhancing effects of the anti-doping rule violation(s) beyond the otherwise applicable period of Ineligibility; the Driver or Person engaged in deceptive or obstructing conduct to avoid the detection or adjudication of an anti-doping rule violation.]

For the avoidance of doubt, the examples of aggravating circumstances described in this Comment to Article 10.6 are not exclusive and other aggravating factors may also justify the imposition of a longer period of Ineligibility. Violations under Article 2.7 (Trafficking or Attempted Trafficking) and 2.8 (Administration or Attempted Administration) are not included in the application of Article 10.6 because the sanctions for these violations (from four years to lifetime Ineligibility) already build in sufficient discretion to allow consideration of any aggravating circumstance.]

10.7 Multiple Violations

10.7.1 Second Anti-Doping Rule Violation

For a *Driver's* or other *Person's* first anti-doping rule violation, the period of *Ineligibility* is set forth in Articles 10.2 and 10.3 (subject to elimination, reduction or suspension under Articles 10.4 or 10.5, or to an increase under Article 10.6). For a second anti-doping rule violation the period of *Ineligibility* shall be within the range set forth in the table below.

Second Violation First Violation	RS	FFMT	NSF	St	AS	TRA
RS	1-4	2-4	2-4	4-6	8-10	10-life
FFMT	1-4	4-8	4-8	6-8	10-life	life
NSF	1-4	4-8	4-8	6-8	10-life	life
St	2-4	6-8	6-8	8-life	life	life
AS	4-5	10-life	10-life	life	life	life
TRA	8-life	life	Life	life	life	life

Definitions for purposes of the second anti-doping rule violation table:

RS (Reduced sanction for specified Substance under Article 10.4): The anti-doping rule violation was or should be sanctioned by a reduced sanction under Article 10.4 because it involved a specified Substance and the other conditions under Article 10.4 were met.

FFMT (Filing Failures and/or Missed Tests): The anti-doping rule violation was or should be sanctioned under Article 10.3.3 (Filing Failures and/or Missed Tests).

NSF (Reduced sanction for *No significant Fault* or *Negligence*): The anti-doping rule violation was or should be sanctioned by a reduced sanction under Article 10.5.2 because *No significant Fault* or *Negligence* under Article 10.5.2 was proved by the *Driver*.

St (Standard sanction under Articles 10.2 or 10.3.1): The anti-doping rule violation was or should be sanctioned by the standard sanction of two years under Article 10.2 or 10.3.1.

AS (Aggravated sanction): The anti-doping rule violation was or should be sanctioned by an aggravated sanction under Article 10.6 because the *Anti-Doping Organization* established the conditions set forth under Article 10.6.

TRA (*Trafficking* or *Attempted Trafficking* and administration or *Attempted* administration): The anti-doping rule violation was or should be sanctioned by a sanction under Article 10.3.2.

[Comment to Article 10.7.1: The table is applied by locating the Driver or other Person's first anti-doping rule violation in the left-hand column and then moving across the table to the right to the column representing the second violation. By way of example, assume a Driver receives the standard period of Ineligibility for a first violation under Article 10.2 and then commits a second violation for which he receives a reduced sanction for a specified Substance under Article 10.4. The

table is used to determine the period of Ineligibility for the second violation. The table is applied to this example by starting in the left-hand column and going down to the fourth row which is "St" for standard sanction, then moving across the table to the first column which is "RS" for reduced sanction for a specified Substance, thus resulting in a 2-4 year range for the period of Ineligibility for the second violation. The Driver or other Person's degree of fault shall be the criterion considered in assessing a period of Ineligibility within the applicable range.]

[Comment to Article 10.7.1 RS Definition: See Article 25.4 with respect to application of Article 10.7.1 to pre-Code anti-doping rule violations.]

10.7.2 Application of Articles 10.5.3 and 10.5.4 to Second Anti-Doping Rule Violation

Where a Driver or other *Person* who commits a second anti-doping rule violation establishes entitlement to suspension or reduction of a portion of the period of *Ineligibility* under Article 10.5.3 or Article 10.5.4, the hearing panel shall first determine the otherwise applicable period of *Ineligibility* within the range established in the table in Article 10.7.1, and then apply the appropriate suspension or reduction of the period of *Ineligibility*. The remaining period of *Ineligibility*, after applying any suspension or reduction under Articles 10.5.3 and 10.5.4, must be at least one-fourth of the otherwise applicable period of *Ineligibility*.

10.7.3 Third Anti-Doping Rule Violation

A third anti-doping rule violation will always result in a lifetime period of *Ineligibility*, except if the third violation fulfills the condition for elimination or reduction of the period of *Ineligibility* under Article 10.4 or involves a violation of Article 2.4 (Filing Failures and/or and Missed Tests). In these particular cases, the period of *Ineligibility* shall be from eight (8) years to lifetime ban.

10.7.4 Additional Rules for Certain Potential Multiple Violations

- For purposes of imposing sanctions under Article 10.7, an anti-doping rule violation will only be considered a second violation if the UIM (or its *National Association*) can establish that the *Driver* or other *Person* committed the second anti-doping rule violation after the *Driver* or other *Person* received notice pursuant to Article 7 (Results Management), or after UIM (or its *National Association*) made reasonable efforts to give notice, of the first anti-doping rule violation; If the UIM (or its *National Association*) cannot establish this, the violations shall be considered together as one single first violation, and the sanction imposed shall be based on the violation that carries the more severe sanction; however, the

occurrence of multiple violations may be considered as a factor in determining Aggravating Circumstances (Article 10.6).

- If, after the resolution of a first anti-doping rule violation, UIM discovers facts involving an anti-doping rule violation by the *Driver* or other *Person* which occurred prior to notification regarding the first violation, then UIM shall impose an additional sanction based on the sanction that could have been imposed if the two violations would have been adjudicated at the same time. Results in all *Competitions* dating back to the earlier anti-doping rule violation will be *disqualified* as provided in Article 10.8. To avoid the possibility of a finding of Aggravating Circumstances (Article 10.6) on account of the earlier-in-time but later-discovered violation, the *Driver* or other *Person* must voluntarily admit the earlier anti-doping rule violation on a timely basis after notice of the violation for which he or she is first charged. The same rule shall also apply when UIM discovers facts involving another prior violation after the resolution of a second anti-doping rule violation.

[Comment to Article 10.7.4: In a hypothetical situation, a Driver commits an anti-doping rule violation on January 1, 2008 which UIM does not discover until December 1, 2008. In the meantime, the Driver commits another anti-doping rule violation on March 1, 2008 and the Driver is notified of this violation by UIM on March 30, 2008 and a hearing panel rules on June 30, 2008 that the Driver committed the March 1, 2008 anti-doping rule violation. The later-discovered violation which occurred on January 1, 2008 will provide the basis for Aggravating Circumstances because the Driver did not voluntarily admit the violation in a timely basis after the Driver received notification of the later violation on March 30, 2008.]

10.7.5 Multiple Anti-Doping Rule Violations during an Eight-Year Period

For purposes of Article 10.7, each anti-doping rule violation must take place within the same eight (8) year period in order to be considered multiple violations.

10.8 Disqualification of Results in Competitions Subsequent to Sample Collection or Commission of an Anti-Doping Rule Violation

In addition to the automatic *disqualification* of the results in the *Competition* which produced the positive *Sample* under Article 9 (Automatic *disqualification* of Individual Results), all other competitive results obtained from the date a positive *Sample* was collected (whether *In-Competition* or *Out-of-Competition*), or other anti-doping rule violation occurred, through the commencement of any *Provisional Suspension* or *Ineligibility* period, shall,

unless fairness requires otherwise, be *disqualified* with all of the resulting consequences including forfeiture of any medals, points and prizes and the awarding of any series title during the relevant season.

10.8.1 As a condition of regaining eligibility after being found to have committed an anti-doping rule violation, the *Driver* must first repay all prize money forfeited under this Article.

10.8.2 Allocation of Forfeited Prize Money.

Forfeited prize money shall be allocated first to reimburse the collection expenses incurred by the *Anti-Doping Organization* in order to perform the necessary steps to collect the prize money back, then to reimburse the expenses incurred by the *Anti-Doping Organization* in order to conduct results management in the case, with the balance, if any, allocated in accordance with UIM's specific rules.

[Comment to Article 10.8.2: Nothing in UIM's Anti-Doping Rules precludes clea Drivers or other Persons who have been damaged by the actions of a Person who has committed an anti-doping rule violation from pursuing any right which they would otherwise have to seek damages from such Person.]

10.9 Commencement of Ineligibility Period

Except as provided below, the period of *Ineligibility* shall start on the date of the hearing decision providing for *Ineligibility* or, if the hearing is waived, on the date *Ineligibility* is accepted or otherwise imposed.

10.9.1 Delays Not Attributable to the *Driver* or other *Person*

Where there have been substantial delays in the hearing process or other aspects of *Doping Control* not attributable to the *Driver* or other *Person*, the *UIM* or *Anti-Doping Organization* imposing the sanction may start the period of *Ineligibility* at an earlier date commencing as early as the date of *Sample* collection or the date on which another anti-doping rule violation last occurred.

10.9.2 Timely Admission

Where the *Driver* promptly (which, in all events, means before the *Driver* competes again) admits the anti-doping rule violation after being confronted with the anti-doping rule violation by UIM, the period of *Ineligibility* may start as early as the date of *Sample* collection or the date on which another anti-doping rule violation last occurred. In each case, however, where this Article is applied, the *Driver* or other *Person* shall serve at least one-half of the period of *Ineligibility* going forward from the

date the *Driver* or other *Person* accepted the imposition of a sanction, the date of a hearing decision imposing a sanction, or the date the sanction is otherwise imposed.

[Comment to Article 10.9.2: This Article shall not apply where the period of Ineligibility already has been reduced under Article 10.5.4 (Admission of an Anti-Doping Rule Violation in the Absence of Other Evidence).]

10.9.3 If a *Provisional Suspension* is imposed and respected by the *Driver*, then the *Driver* shall receive a credit for such period of *Provisional Suspension* against any period of *Ineligibility* which may ultimately be imposed.

10.9.4 If a *Driver* voluntarily accepts a *Provisional Suspension* in writing from UIM and thereafter refrains from competing, the *Driver* shall receive a credit for such period of voluntary *Provisional Suspension* against any period of *Ineligibility* which may ultimately be imposed. A copy of the *Driver's* voluntary acceptance of a *Provisional Suspension* shall be provided promptly to each party entitled to receive notice of a potential anti-doping rule violation under Article 14.1.

[Comment to Article 10.9.4: A Driver's voluntary acceptance of a Provisional Suspension is not an admission by the Driver and shall not be used in any way as to draw an adverse inference against the Driver.]

10.9.5 No credit against a period of *Ineligibility* shall be given for any time period before the effective date of the *Provisional Suspension* or voluntary *Provisional Suspension* regardless of whether the *Driver* elected not to compete or was suspended by his or her team.

[Comment to Article 10.9: The text of Article 10.9 has been revised to make clear that delays not attributable to the Driver, timely admission by the Driver and Provisional Suspension are the only justifications for starting the period of Ineligibility earlier than the date of the hearing decision. This amendment corrects inconsistent interpretation and application of the previous text.]

10.10 Status During Ineligibility

10.10.1 Prohibition against Participation during *Ineligibility*

No *Driver* or other *Person* who has been declared *Ineligible* may, during the period of *Ineligibility*, participate in any capacity in an *Event* or activity (other than authorized anti-doping education or rehabilitation programs) authorized or organized by UIM or any *National Association* or a club or other member organization of UIM or any *National Association*, or in

Competitions authorized or organized by any professional league or any international or national level *Event* organization.

A Driver or other *Person* subject to a period of *Ineligibility* longer than four years may, after completing four years of the period of *Ineligibility*, participate in local sport events in a sport other than sports subject to the jurisdictions of UIM and its *National Associations*, but only so long as the local sport event is not at a level that could otherwise qualify such *Person* directly or indirectly to compete in (or accumulate points toward) a national championship or *International Event*.

A Driver or other *Person* subject to a period of *Ineligibility* shall remain subject to *Testing*.

[Comment to Article 10.10.1: For example, an ineligible Driver cannot participate in a training camp, exhibition or practice organized by his or her National Association or a club which is a member of that National Association. Further, an ineligible Driver may not compete in a non-Signatory professional league (e.g., the National Hockey League, the National Basketball Association, etc.), Events organized by a non-Signatory International Event organization or a non-Signatory national-level event organization without triggering the consequences set forth in Article 10.10.2. Sanctions in one sport will also be recognized by other sports (see Article 15).]

10.10.2 Violation of the Prohibition of Participation during *Ineligibility*

Where a Driver or other *Person* who has been declared *Ineligible* violates the prohibition against participation during *Ineligibility* described in Article 10.10.1, the results of such participation shall be *disqualified* and the period of *Ineligibility* which was originally imposed shall start over again as of the date of the violation. The new period of *Ineligibility* may be reduced under Article 10.5.2 if the *Driver* or other *Person* establishes he or she bears *No significant Fault or Negligence* for violating the prohibition against participation. The determination of whether a Driver or other *Person* has violated the prohibition against participation, and whether a reduction under Article 10.5.2 is appropriate, shall be made by UIM.

[Comment to Article 10.10.2: If a Driver or other Person is alleged to have violated the prohibition against participation during a period of Ineligibility, UIM shall determine whether the Driver violated the prohibition and, if so, whether the Driver or other Person has established grounds for a reduction in the restarted period of Ineligibility under Article 10.5.2. Decisions rendered by UIM under this Article may be appealed pursuant to Article 13.2.]

Where a Driver Support Personnel or other Person substantially assists a Driver in violating the prohibition against participation during Ineligibility, UIM may appropriately impose sanctions under its own disciplinary rules for such assistance.]

10.10.3 Withholding of Financial Support during *Ineligibility*

In addition, for any anti-doping rule violation not involving a reduced sanction for *specified Substances* as described in Article 10.4, some or all sport-related financial support or other sport-related benefits received by such *Person* will be withheld by UIM and its *National Associations*.

10.11 Reinstatement Testing

As a condition to regaining eligibility at the end of a specified period of *Ineligibility*, a Driver must, during any period of *Provisional Suspension* or *Ineligibility*, make him or herself available for *Out-of-Competition Testing* by UIM, the applicable *National Association*, and any other *Anti-Doping Organization* having *Testing* jurisdiction, and must comply with the whereabouts requirements of Article 11 of the *International Standard for Testing*. If a Driver subject to a period of *Ineligibility* retires from sport and is removed from *Out-of-Competition Testing* pools and later seeks reinstatement, the *Driver* shall not be eligible for reinstatement until the *Driver* has notified UIM and the applicable *National Association* and has been subject to *Out-of-Competition Testing* for a period of time equal to the [longer of (a) the period set forth in Article 5.6 and (b)] period of *Ineligibility* remaining as of the date the *Driver* had retired. During such remaining period of *Ineligibility*, a minimum of 2 tests must be conducted on the *Driver* with at least three months between each test. The *National Association* shall be responsible for conducting the necessary tests, but tests by any *Anti-Doping Organization* may be used to satisfy the requirement. The results of such tests shall be reported to UIM. In addition, immediately prior to the end of the period of *Ineligibility*, a Driver must undergo *Testing* by UIM for the *Prohibited Substances* and *Methods* that are prohibited in *Out-of-Competition Testing*. Once the period of a Driver's *Ineligibility* has expired, and the *Driver* has fulfilled the conditions of reinstatement, then the *Driver* will become automatically re-eligible and no application by the *Driver* or by the *Driver's National Association* will then be necessary.

[10.12 Imposition of Financial Sanctions

Anti-Doping Organizations may, in their own rules, provide for financial sanctions on account of anti-doping rule violations. However, no financial

sanction may be considered a basis for reducing the period of *Ineligibility* or other sanction which would otherwise be applicable under the *Code*.]

ARTICLE 11 *Intentionally left blank*

ARTICLE 12 SANCTIONS AND COSTS ASSESSED AGAINST NATIONAL ASSOCIATIONS

12.1 The UIM has the authority to withhold some or all funding or other non financial support to *National Associations* that are not in compliance with these Anti-Doping Rules.

[**12.2** *National Associations* shall be obligated to reimburse UIM for all costs (including but not limited to laboratory fees, hearing expenses and travel) related to a violation of these Anti-Doping Rules committed by a Driver or other *Person* affiliated with that *National Association*.]

12.3 UIM may elect to take additional disciplinary action against National Associations with respect to recognition, the eligibility of its officials and Drivers to participate in International Events and fines based on the following:

12.3.1 Four or more violations of these Anti-Doping Rules (other than violations involving Articles 2.4 and 10.3) are committed by *Drivers* or other *Persons* affiliated with a *National Association* within a 12-month period in testing conducted by UIM or Anti-Doping Organizations other than the *National Association* or its *National Anti-Doping Organization*. [In such event the UIM may in its discretion elect to: (a) ban all officials from that *National Association* for participation in any UIM activities for a period of up to two years and/or (b) fine the *National Association* in an amount up to 10.000 Euro. (For purposes of this Rule, any fine paid pursuant to Rule 12.3.2 shall be credited against any fine assessed.)

12.3.1.1 If four or more violations of these Anti-Doping Rules (other than violations involving Articles 2.4 and 10.3) are committed in addition to the violations described in Article 12.3.1 by *Drivers* or other *Persons* affiliated with a *National Association* within a 12-month period in testing conducted by UIM or Anti-Doping Organizations other than the *National Association* or its *National Anti-Doping Organization*, then the UIM may suspend that *National Association's* membership for a period of up to 4 years.]

12.3.2 More than one *Driver* or other *Person* from a *National Association* commits an *Anti-Doping Rule* violation during an *International Event*. [In such event UIM may fine that *National Association* in an amount up to 10.000 Euro.]

12.3.3 A *National Association* has failed to make diligent efforts to keep UIM informed about a *Driver's* whereabouts after receiving a request for that information from UIM [In such event UIM may fine the *National Association* in an amount up to 10.000 Euro per *Driver* in addition to all of UIM costs incurred in *Testing* that *National Association's Drivers*.]

ARTICLE 13 APPEALS

13.1 Decisions Subject to Appeal

Decisions made under these Anti-Doping Rules may be appealed as set forth below in Article 13.2 through 13.4 or as otherwise provided in these Anti-Doping Rules. Such decisions shall remain in effect while under appeal unless the appellate body orders otherwise. Before an appeal is commenced, any post-decision review authorized in these rules must be exhausted (except as provided in Article 13.1.1).

13.1.1 WADA Not Required to Exhaust Internal Remedies

Where *WADA* has a right to appeal under Article 13 and no other party has appealed a final decision within the UIM or its *National Association's* process, *WADA* may appeal such decision directly to *CAS* without having to exhaust other remedies in the UIM or its *National Association's* process.

[Comment to Article 13.1.1: Where a decision has been rendered before the final stage of UIM's process (for example, a first hearing) and no party elects to appeal that decision to the next level of UIM's process (e.g., the Managing Board), then WADA may bypass the remaining steps in UIM's internal process and appeal directly to CAS.]

13.2 Appeals from Decisions Regarding Anti-Doping Rule Violations, Consequences, and Provisional Suspensions

A decision that an anti-doping rule violation was committed, a decision imposing *Consequences* for an anti-doping rule violation, or a decision that no anti-doping rule violation was committed; a decision that an anti-doping rule violation proceeding cannot go forward for procedural reasons (including, for example, prescription); a decision under Article 10.10.2 (prohibition of

participation during *Ineligibility*); a decision that the UIM or its *National Association* lacks jurisdiction to rule on an alleged anti-doping rule violation or its *Consequences*; a decision by any *National Association* not to bring forward an *Adverse Analytical Finding* or an *Atypical Finding* as an anti-doping rule violation, or a decision not to go forward with an anti-doping rule violation after an investigation under Article 7.4; [and a decision to impose a *Provisional Suspension* as a result of a *Provisional Hearing* or otherwise in violation of Article 7.4] may be appealed exclusively as provided in this Article 13.2. [Notwithstanding any other provision herein, the only *Person* that may appeal from a *Provisional Suspension* is the *Driver* or other *Person* upon whom the *Provisional Suspension* is imposed.]

13.2.1 Appeals Involving *International-Level Drivers*

In cases arising from competition in an *International Event* or in cases involving *International-Level Drivers*, the decision may be appealed exclusively to CAS in accordance with the provisions applicable before such court.

[Comment to Article 13.2.1: CAS decisions are final and binding except for any review required by law applicable to the annulment or enforcement of arbitral awards.]

13.2.2 Appeals Involving National-Level *Drivers*

In cases involving *Drivers* who do not have a right to appeal under Article 13.2.1, each *National Association* shall have in place an appeal procedure that respects the following principles: a timely hearing, a fair and impartial hearing panel; the right to be represented by a counsel at the person's expense; and a timely, written, reasoned decision. UIM's rights of appeal with respect to these cases are set forth in Article 13.2.3 below.

[Comment to Article 13.2.2: UIM may elect to comply with this Article by giving its national-level Drivers the right to appeal directly to CAS.]

13.2.3 *Persons* Entitled to Appeal

In cases under Article 13.2.1, the following parties shall have the right to appeal to CAS: (a) the *Driver* or other *Person* who is the subject of the decision being appealed; (b) the other party to the case in which the decision was rendered; (c) UIM and any other *Anti-Doping Organization* under whose rules a sanction could have been imposed; (d) the International Olympic Committee or International Paralympic Committee, as applicable, where the decision may have an effect in relation to the Olympic Games or Paralympic Games, including decisions affecting eligibility for the Olympic Games or Paralympic Games; and (e) WADA. In

cases under Article 13.2.2, the parties having the right to appeal to the national-level reviewing body shall be as provided in the *National Association's* rules but, at a minimum, shall include the following parties: (a) the *Driver* or other *Person* who is the subject of the decision being appealed; (b) the other party to the case in which the decision was rendered; (c) UIM; and (d) WADA. For cases under Article 13.2.2, WADA and UIM shall also have the right to appeal to CAS with respect to the decision of the national-level reviewing body.

13.3 Failure to Render a Timely Decision by UIM and its National Associations

Where, in a particular case, UIM or its *National Associations* fail to render a decision with respect to whether an anti-doping rule violation was committed within a reasonable deadline set by WADA, WADA may elect to appeal directly to CAS as if UIM or its *National Associations* had rendered a decision finding no anti-doping rule violation. If the CAS panel determines that an anti-doping rule violation was committed and that WADA acted reasonably in electing to appeal directly to CAS, then WADA's costs and attorneys fees in prosecuting the appeal shall be reimbursed to WADA by UIM or its *National Associations*.

[Comment to Article 13.3: Given the different circumstances of each anti-doping rule violation investigation and results management process, it is not feasible to establish a fixed time period for UIM to render a decision before WADA may intervene by appealing directly to CAS. Before taking such action, however, WADA will consult with UIM and give UIM an opportunity to explain why it has not yet rendered a decision. Nothing in this rule prohibits UIM from also having rules which authorize it to assume jurisdiction for matters in which the results management performed by one of its National Associations has been inappropriately delayed.]

13.4 Appeals from Decisions Granting or Denying a Therapeutic Use Exemption

Decisions by WADA reversing the grant or denial of a TUE may be appealed exclusively to CAS by the *Driver*, UIM, or *National Anti-Doping Organization* or other body designated by a *National Association* which granted or denied the exemption. Decisions to deny TUE's, and which are not reversed by WADA, may be appealed by *International-Level Drivers* to CAS and by other *Drivers* to the national level reviewing body described in Article 13.2.2. If the national level reviewing body reverses the decision to deny a TUE, that decision may be appealed to CAS by WADA.

When UIM, *National Anti-Doping Organizations* or other bodies designated by *National Associations* fail to take action on a properly submitted TUE application within a reasonable time, their failure to decide may be considered a denial for purposes of the appeal rights provided in this Article.

13.5 Appeal from Decisions Pursuant to Article 12

Decisions by UIM pursuant to Article 12 may be appealed exclusively to CAS by the *National Association*.

13.6 Time for Filing Appeals

The time to file an appeal to CAS shall be twenty-one (21) days from the date of receipt of the decision by the appealing party. The above notwithstanding, the following shall apply in connection with appeals filed by a party entitled to appeal but which was not a party to the proceedings having lead to the decision subject to appeal:

- a) Within ten (10) days from notice of the decision, such party/ies shall have the right to request from the body having issued the decision a copy of the file on which such body relied;
- b) If such a request is made within the ten-day period, then the party making such request shall have twenty-one (21) days from receipt of the file to file an appeal to CAS.

The above notwithstanding, the filing deadline for an appeal or intervention filed by *WADA* shall be the later of:

- (a) Twenty-one (21) days after the last day on which any other party in the case could have appealed, or
- (b) Twenty-one (21) days after *WADA's* receipt of the complete file relating to the decision.

ARTICLE 14 NATIONAL ASSOCIATIONS' INCORPORATION OF UIM RULES, REPORTING AND RECOGNITION

14.1 Incorporation of UIM Anti-Doping Rules

All *National Associations* shall comply with these Anti-Doping Rules. These Anti-Doping Rules shall also be incorporated either directly or by reference into each *National Associations* Rules. All *National Associations* shall include in their regulations the procedural rules necessary to effectively implement

these Anti-Doping Rules. Each *National Association* shall obtain the written acknowledgement and agreement, in the form attached as Appendix 2, of all *Drivers* subject to *Doping Control* and *Driver Support Personnel* for such *Drivers*. Notwithstanding whether or not the required form has been signed, the Rules of each *National Association* shall specifically provide that all *Drivers*, *Driver Support Personnel* and other *Persons* under the jurisdiction of the *National Association* shall be bound by these Anti-Doping Rules.

14.2 Statistical Reporting

14.2.1 *National Associations* shall report to UIM at the end of every year results of all *Doping Controls* within their jurisdiction sorted by *Driver* and identifying each date on which the *Driver* was tested, the entity conducting the test, and whether the test was *In-Competition* or *Out-of-Competition*. UIM may periodically publish *Testing* data received from *National Associations* as well as comparable data from *Testing* under UIM's jurisdiction.

14.2.2 UIM shall publish annually a general statistical report of its Doping Control activities during the calendar year with a copy provided to WADA.

14.3 Doping Control Information Clearinghouse

When a *National Association* has received an *Adverse Analytical Finding* on one of its *Drivers* it shall report the following information to UIM and WADA within fourteen (14) days of the process described in Article 7.1.2 and 7.1.3: the *Driver's* name, country, sport and discipline within the sport, whether the test was *In-Competition* or *Out-of-Competition*, the date of *Sample* collection and the analytical result reported by the laboratory. The *National Association* shall also regularly update UIM and WADA on the status and findings of any review or proceedings conducted pursuant to Article 7 (*Results Management*), Article 8 (*Right to a Fair Hearing*) or Article 13 (*Appeals*), and comparable information shall be provided to UIM and WADA within 14 days of the notification described in Article 7.1.9, with respect to other violations of these Anti-Doping Rules. In any case in which the period of *Ineligibility* is eliminated under Article 10.5.1 (*No Fault or Negligence*) or reduced under Article 10.5.2 (*No Significant Fault or Negligence*), UIM and WADA shall be provided with a written reasoned decision explaining the basis for the elimination or reduction. Neither UIM nor WADA shall disclose this information beyond those persons within their organisations with a need to know until the *National Association* has made public disclosure or has failed to make public disclosure as required in Article 14.4 below.

14.4 Public Disclosure

14.4.1 Neither UIM nor its *National Association* shall publicly identify *Drivers* whose *Samples* have resulted in *Adverse Analytical Findings*, or who were alleged to have violated other Articles of these Anti-Doping Rules until it has been determined in a hearing in accordance with Article 8 that an anti-doping rule violation has occurred, or such hearing has been waived, or the assertion of an anti-doping rule violation has not been timely challenged or the *Driver* has been *Provisionally Suspended*. Once a violation of these Anti-Doping Rules has been established, it shall be publicly reported within 20 days. UIM or its *National Association* must also report within 20 days appeal decisions on an anti-doping rule violation. UIM or its *National Association* shall also, within the time period for publication, send all hearing and appeal decisions to WADA.

14.4.2 In any case where it is determined, after a hearing or appeal, that the *Driver* or other *Person* did not commit an anti-doping rule violation, the decision may be disclosed publicly only with the consent of the *Driver* or other *Person* who is the subject of the decision. UIM or its *National Association* shall use reasonable efforts to obtain such consent, and UIM consent is obtained, shall publicly disclose the decision in its entirety or in such redacted form as the *Driver* or other *Person* may approve.

14.4.3 Neither UIM nor its *National Association* or WADA accredited laboratory, or official of either, shall publicly comment on the specific facts of a pending case (as opposed to general description of process and science) except in response to public comments attributed to the *Driver*, other *Person* or their representatives.

14.5 Recognition of Decisions by UIM and National Associations

Any decision of UIM or a *National Association* regarding a violation of these Anti-Doping Rules shall be recognized by all *National Associations*, which shall take all necessary action to render such results effective.

ARTICLE 15 RECOGNITION OF DECISIONS BY OTHER ORGANISATIONS

Subject to the right to appeal provided in Article 13, the *Testing*, TUE's and hearing results or other final adjudications of any *Signatory* to the *Code* which are consistent with the *Code* and are within the *Signatory's* authority, shall be recognized and respected by UIM and its *National Associations*. UIM and its *National Associations* may recognize the same actions of other bodies which have

not accepted the *Code* if the rules of those bodies are otherwise consistent with the *Code*.

[Comment to Article 15: Where the decision of a body that has not accepted the Code is in some respects Code compliant and in other respects not Code compliant, UIM or its National Association should attempt to apply the decision in harmony with the principles of the Code. For example, if in a process consistent with the Code a non-Signatory has found a Driver to have committed an anti-doping rule violation on account of the presence of a Prohibited Substance in his body but the period of Ineligibility applied is shorter than the period provided for in the Code, then UIM or its National Association should recognize the finding of an anti-doping rule violation and they should conduct a hearing consistent with Article 8 to determine whether the longer period of Ineligibility provided in the Code should be imposed.]

ARTICLE 16 STATUTE OF LIMITATIONS

No action may be commenced under these Anti-Doping Rules against a *Driver* or other *Person* for a violation of an anti-doping rule contained in these Anti-Doping Rules unless such action is commenced within eight years from the date the violation occurred.

ARTICLE 17 UIM COMPLIANCE REPORTS TO WADA

The UIM will report to *WADA* on the UIM's compliance with the *Code* every second year and shall explain reasons for any noncompliance.

ARTICLE 18 AMENDMENT AND INTERPRETATION OF ANTI-DOPING RULES

18.1 These Anti-Doping Rules may be amended from time to time by the *UIM* Executive.

18.2 Except as provided in Article 18.5, these Anti-Doping Rules shall be interpreted as an independent and autonomous text and not by reference to existing law or statutes.

18.3 The headings used for the various Parts and Articles of these Anti-Doping Rules are for convenience only and shall not be deemed part of the substance of these Anti-Doping Rules or to affect in any way the language of the provisions to which they refer.

18.4 The INTRODUCTION and the APPENDIX I DEFINITIONS shall be considered integral parts of these Anti-Doping Rules.

18.5 These Anti-Doping Rules have been adopted pursuant to the applicable provisions of the *Code* and shall be interpreted in a manner that is consistent with applicable provisions of the *Code*. The comments annotating various provisions of the *Code* may, where applicable, assist in the understanding and interpretation of these Anti-Doping Rules.

18.6 Notice to a Driver or other *Person* who is a member of a *National Association* may be accomplished by delivery of the notice to the *National Association*.

18.7 These Anti-Doping Rules shall come into full force and effect on 1 January 2009 (the “**Effective Date**”). They shall not apply retrospectively to matters pending before the Effective Date; provided, however, that:

18.7.1 Any case pending prior to the Effective Date, or brought after the Effective Date based on an anti-doping rule violation that occurred prior to the Effective Date, shall be governed by the predecessor to these Anti-Doping Rules in force at the time of the anti-doping rule violation, subject to any application of the principle of *lex mitior* by the hearing panel determining the case.

18.7.2 Any Article 2.4 whereabouts violation (whether a filing failure or a missed test) declared by UIM under rules in force prior to the Effective Date that has not expired prior to the Effective Date and that would qualify as a whereabouts violation under Article 11 of the *International Standard for Testing* shall be carried forward and may be relied upon, prior to expiry, as one of the three Filing Failures and/or Missed Tests giving rise to an anti-doping rule violation under Article 2.4 of these Anti-Doping Rules. *[Note: where existing whereabouts violations are carried over to the new regime, any restrictions under the old rules on combining those whereabouts violations with other whereabouts violations must also be carried over. Hence:]* Unless otherwise stated by UIM, however:

- a. a filing failure that is carried forward in this manner may only be combined with (post-Effective Date) Filing Failures;
- b. a missed test that is carried forward in this manner may only be combined with (post-Effective Date) Missed Tests; and
- c. a filing failure or missed test declared by any Anti-Doping Organization other than [UIM and a National Association] prior to the Effective Date may not be combined with any Filing Failure or Missed Test declared under these Anti-Doping Rules.]

18.7.3 Where a period of *Ineligibility* imposed by UIM under rules in force prior to the Effective Date has not yet expired as of the Effective Date, the *Person* who is *Ineligible* may apply to UIM for a reduction in the period of *Ineligibility* in light of the amendments made to the *Code* as from the Effective Date. To be valid, such application must be made before the period of *Ineligibility* has expired.

18.7.4 Subject always to Article 10.7.5, anti-doping rule violations committed under rules in force prior to the Effective Date shall be taken into account as prior offences for purposes of determining sanctions under Article 10.7. Where such pre-Effective Date anti-doping rule violation involved a substance that would be treated as a Specified Substance under these Anti-Doping Rules, for which a period of *Ineligibility* of less than two years was imposed, such violation shall be considered a Reduced Sanction violation for purposes of Article 10.7.1.

APPENDIX 1 - DEFINITIONS

Adverse Analytical Finding. A report from a laboratory or other approved *Testing entity* that identifies in a *Sample* the presence of a *Prohibited Substance* or its *Metabolites* or *Markers* (including elevated quantities of endogenous substances) or evidence of the *Use* of a *Prohibited Method*.

Anti-Doping Organization. A *Signatory* that is responsible for adopting rules for initiating, implementing or enforcing any part of the *Doping Control* process. This includes, for example, the International Olympic Committee, the International Paralympic Committee, other *Major Event Organizations* that conduct *Testing* at their *Events*, WADA, InterNational Associations, and *National Anti-Doping Organizations*.

Driver. Any *Person* who participates in sport at the international level (as defined by each International Association), the national level (as defined by each *National Anti-Doping Organization*, including but not limited to those *Persons* in its *Registered Testing Pool*), and any other competitor in sport who is otherwise subject to the jurisdiction of any *Signatory* or other sports organization accepting the *Code*. All provisions of the *Code*, including, for example, *Testing*, and TUE's must be applied to international and national-level competitors. Some *National Anti-Doping Organizations* may elect to test and apply anti-doping rules to recreational-level or masters competitors who are not current or potential national caliber competitors. *National Anti-Doping Organizations* are not required, however, to apply all aspects of the *Code* to such *Persons*. Specific national rules may be established for Doping Control for non-international-level or national-level competitors without being in conflict with the *Code*. Thus, a country could elect to test recreational-level competitors but not require TUE's or whereabouts information. In the same manner, a *Major Event Organization* holding an *Event* only for masters-level competitors could elect to test the competitors but not require advance TUE or whereabouts information. For purposes of Article 2.8 (Administration or Attempted Administration) and for purposes of anti-doping information and education, any *Person* who participates in sport under the authority of any *Signatory*, government, or other sports organization accepting the *Code* is a Driver.

[Comment to Driver: This definition makes it clear that all international and national-caliber Drivers are subject to the anti-doping rules of the Code, with the precise definitions of international and national level sport to be set forth in the anti-doping rules of the UIM and National Anti-Doping Organizations, respectively. At the national level, anti-doping rules adopted pursuant to the Code shall apply, at a minimum, to all persons on national teams and all persons qualified to compete in any national championship in any sport. That does not mean, however, that all such Drivers must be included in a National Anti-Doping Organization's

Registered Testing Pool. The definition also allows each National Anti-Doping Organization, if it chooses to do so, to expand its anti-doping control program beyond national-caliber Drivers to competitors at lower levels of competition. Competitors at all levels of competition should receive the benefit of anti-doping information and education.]

Driver Support Personnel. Any coach, trainer, manager, agent, team staff, official, medical, paramedical personnel, parent or any other *Person* working with, treating or assisting a Driver participating in or preparing for sports *Competition*.

Attempt. Purposely engaging in conduct that constitutes a substantial step in a course of conduct planned to culminate in the commission of an anti-doping rule violation. Provided, however, there shall be no anti-doping rule violation based solely on an *Attempt* to commit a violation if the *Person* renounces the attempt prior to it being discovered by a third party not involved in the *Attempt*.

Atypical Finding. A report from a laboratory or other WADA-approved entity which requires further investigation as provided by the *International Standard* for Laboratories or related Technical Documents prior to the determination of an *Adverse Analytical Finding*.

CAS. The Court of Arbitration for Sport.

Code. The World Anti-Doping Code.

Competition. A single race, match, game or singular athletic contest. For example, a basketball game or the finals of the Olympic 100-meter dash in athletics. For stage races and other athletic contests where prizes are awarded on a daily or other interim basis the distinction between a *Competition* and an *Event* will be as provided in the rules of the applicable International Association.

Consequences of anti-doping rule violations. A Driver's or other *Person's* violation of an anti-doping rule may result in one or more of the following: (a) *Disqualification* means the *Driver's* results in a particular *Competition* or *Event* are invalidated, with all resulting consequences including forfeiture of any medals, points and prizes; (b) *Ineligibility* means the *Driver* or other *Person* is barred for a specified period of time from participating in any *Competition* or other activity or funding as provided in Article 10.9; and (c) *Provisional Suspension* means the *Driver* or other *Person* is barred temporarily from participating in any *Competition* prior to the final decision at a hearing conducted under Article 8 (Right to a Fair Hearing).

Disqualification. See *Consequences of anti-doping rule violations*, above.

Doping Control. All steps and processes from test distribution planning through to ultimate disposition of any appeal including all steps and processes in between

such as provision of whereabouts information, sample collection and handling, laboratory analysis, TUE's, results management and hearings.

Event. A series of individual *Competitions* conducted together under one ruling body (e.g., the Olympic Games, FINA World Championships, or Pan American Games).

Event Period. The time between the beginning and end of an *Event*, as established by the ruling body of the *Event*.

In-Competition. Unless provided otherwise in the rules of an International Association or other relevant *Anti-Doping Organization*, "*In-Competition*" means the period commencing twelve hours before a *Competition* in which the *Driver* is scheduled to participate through the end of such *Competition* and the *Sample* collection process related to such *Competition*.

Independent Observer Program. A team of observers, under the supervision of WADA, who observe and may provide guidance on the *Doping Control* process at certain *Events* and report on their observations.

Individual Sport. Any sport that is not a *Team Sport*.

Ineligibility. See *Consequences of Anti-Doping Rule Violations* above.

International Event. An *Event* where the International Olympic Committee, the International Paralympic Committee, an International Association, a *Major Event Organization*, or another international sport organization is the ruling body for the *Event* or appoints the technical officials for the *Event*.

International-Level Driver. *Driver* designated by one or more International Associations as being within the *Registered Testing Pool* for an International Association.

International Standard. A standard adopted by WADA in support of the *Code*. Compliance with an *International Standard* (as opposed to another alternative standard, practice or procedure) shall be sufficient to conclude that the procedures addressed by the *International Standard* were performed properly. *International Standards* shall include any Technical Documents issued pursuant to the *International Standard*.

Major Event Organizations. The continental associations of *National Olympic Committees* and other international multi-sport organizations that function as the ruling body for any continental, regional or other *International Event*.

Marker. A compound, group of compounds or biological parameter(s) that indicates the *Use* of a *Prohibited Substance* or *Prohibited Method*.

Metabolite. Any substance produced by a biotransformation process.

Minor. A natural *Person* who has not reached the age of majority as established by the applicable laws of his or her country of residence.

National Anti-Doping Organization. The entity(ies) designated by each country as possessing the primary authority and responsibility to adopt and implement anti-doping rules, direct the collection of *Samples*, the management of test results, and the conduct of hearings, all at the national level. This includes an entity which may be designated by multiple countries to serve as regional *Anti-Doping Organization* for such countries. If this designation has not been made by the competent public authority(ies), the entity shall be the country's *National Olympic Committee* or its designee.

National Event. A sport *Event* involving international or national-level *Drivers* that is not an *International Event*.

National Association. A national or regional entity which is a member of or is recognized by UIM as the entity governing the UIM's sport in that nation or region.

National Olympic Committee. The organization recognized by the International Olympic Committee. The term *National Olympic Committee* shall also include the National Sport Confederation in those countries where the National Sport Confederation assumes typical *National Olympic Committee* responsibilities in the anti-doping area.

No Advance Notice. A *Doping Control* which takes place with no advance warning to the *Driver* and where the *Driver* is continuously chaperoned from the moment of notification through *Sample* provision.

No Fault or Negligence. The *Driver's* establishing that he or she did not know or suspect, and could not reasonably have known or suspected even with the exercise of utmost caution, that he or she had *Used* or been administered the *Prohibited Substance* or *Prohibited Method*.

No Significant Fault or Negligence. The *Driver's* establishing that his or her fault or negligence, when viewed in the totality of the circumstances and taking into account the criteria for *No Fault or Negligence*, was not significant in relationship to the anti-doping rule violation.

Out-of-Competition. Any *Doping Control* which is not *In-Competition*.

Participant. Any *Driver* or *Driver Support Personnel*.

Person. A natural *Person* or an organization or other entity.

Possession. The actual, physical possession, or the constructive possession (which shall be found only if the person has exclusive control over the *Prohibited Substance/Method* or the premises in which a *Prohibited Substance/Method* exists); provided, however, that if the person does not have exclusive control over the *Prohibited Substance/Method* or the premises in which a *Prohibited Substance/Method* exists, constructive possession shall only be found if the person knew about the presence of the *Prohibited Substance/Method* and intended to exercise control over it. Provided, however, there shall be no anti-doping rule violation based solely on possession if, prior to receiving notification of any kind that the *Person* has committed an anti-doping rule violation, the *Person* has taken concrete action demonstrating that the *Person* never intended to have possession and has renounced possession by explicitly declaring it to an *Anti-Doping Organization*. Notwithstanding anything to the contrary in this definition, the purchase (including by any electronic or other means) of a *Prohibited Substance* or *Prohibited Method* constitutes possession by the *Person* who makes the purchase.

[Comment: Under this definition, steroids found in a Driver's car would constitute a violation unless the Driver establishes that someone else used the car; in that event, the Anti-Doping Organization must establish that, even though the Driver did not have exclusive control over the car, the Driver knew about the steroids and intended to have control over the steroids. Similarly, in the example of steroids found in a home medicine cabinet under the joint control of a Driver and spouse, the Anti-Doping Organization must establish that the Driver knew the steroids were in the cabinet and that the Driver intended to exercise control over the steroids.]

Prohibited List. The List identifying the *Prohibited Substances* and *Prohibited Methods*.

Prohibited Method. Any method so described on the *Prohibited List*.

Prohibited Substance. Any substance so described on the *Prohibited List*.

Provisional Hearing. For purposes of Article 7.6, an expedited abbreviated hearing occurring prior to a hearing under Article 8 (Right to a Fair Hearing) that provides the *Driver* with notice and an opportunity to be heard in either written or oral form.

Provisional Suspension. See *Consequences* above.

Publicly Disclose or Publicly Report. To disseminate or distribute information to the general public or persons beyond those persons entitled to earlier notification in accordance with Article 14.

Registered Testing Pool. The pool of top level *Drivers* established separately by each InterNational Association and *National Anti-Doping Organization* who are

subject to both *In-Competition* and *Out-of-Competition Testing* as part of that InterNational Association's or National Anti-Doping Organization's test distribution plan.

Retroactive TUE. As defined in the *International Standard for Therapeutic Use Exemptions*.

Sample. Any biological material collected for the purposes of *Doping Control*.

[Comment to Sample: It has sometimes been claimed that the collection of blood samples violates the tenets of certain religious or cultural groups. It has been determined that there is no basis for any such claim.]

Signatories. Those entities signing the *Code* and agreeing to comply with the *Code*, including the International Olympic Committee, InterNational Associations, International Paralympic Committee, *National Olympic Committees*, National Paralympic Committees, *Major Event Organizations*, *National Anti-Doping Organizations*, and WADA.

Specified Substances. As defined in Article 4.2.2.

Substantial Assistance. For purposes of Article 10.5.3, a *Person* providing *Substantial Assistance* must: (1) fully disclose in a signed written statement all information he or she possesses in relation to anti-doping rule violations, and (2) fully cooperate with the investigation and adjudication of any case related to that information, including, for example, presenting testimony at a hearing if requested to do so by an *Anti-Doping Organization* or hearing panel. Further, the information provided must be credible and must comprise an important part of any case which is initiated or, if no case is initiated, must have provided a sufficient basis on which a case could have been brought.

Tampering. Altering for an improper purpose or in an improper way; bringing improper influence to bear; interfering improperly; obstructing, misleading or engaging in any fraudulent conduct to alter results or prevent normal procedures from occurring; or providing fraudulent information to an *Anti-Doping Organization*.

Target Testing. Selection of *Drivers* for *Testing* where specific *Drivers* or groups of *Drivers* are selected on a non-random basis for *Testing* at a specified time.

Team Sport. A sport in which the substitution of players is permitted during a *Competition*.

Testing. The parts of the *Doping Control* process involving test distribution planning, *Sample* collection, *Sample* handling, and *Sample* transport to the laboratory.

Trafficking. Selling, giving, transporting, sending, delivering or distributing a *Prohibited Substance* or *Prohibited Method* (either physically or by any electronic or other means) by a Driver, *Driver Support Personnel* or any other *Person* subject to the jurisdiction of an *Anti-Doping Organization* to any third party; provided, however, this definition shall not include the actions of bona fide medical personnel involving a *Prohibited Substance* used for genuine and legal therapeutic purposes or other acceptable justification, and shall not include actions involving *Prohibited Substances* which are not prohibited in *Out-of-Competition Testing* unless the circumstances as a whole demonstrate such *Prohibited Substances* are not intended for genuine and legal therapeutic purposes.

TUE. As defined in Article 2.6.1.

TUE Panel. As defined in Article 4.4.3.

UNESCO Convention. The International Convention against Doping in Sport adopted by the 33rd session of the UNESCO General Conference on 19 October 2005 including any and all amendments adopted by the States Parties to the Convention and the Conference of Parties to the International Convention against Doping in Sport.

Use. The utilization, application, ingestion, injection or consumption by any means whatsoever of any *Prohibited Substance* or *Prohibited Method*.

WADA. The World Anti-Doping Agency.

APPENDIX 2 - Acknowledgment and Agreement

I, as a member of [National Association] and/or a participant in a [National Association or UIM] authorized or recognized event, hereby acknowledge and agree as follows:

1. I have received and had an opportunity to review the UIM Anti-Doping Rules.
2. I consent and agree to comply with and be bound by all of the provisions of the UIM Anti-Doping Rules, including but not limited to, all amendments to the Anti-Doping Rules and all International Standards incorporated in the Anti-Doping Rules.
3. I acknowledge and agree that [National Associations and UIM] have jurisdiction to impose sanctions as provided in the UIM Anti-Doping Rules.
4. I also acknowledge and agree that any dispute arising out of a decision made pursuant to the UIM Anti-Doping Rules, after exhaustion of the process expressly provided for in the UIM Anti-Doping Rules, may be appealed exclusively as provided in Article 13 of the UIM Anti-Doping Rules to an appellate body for final and binding arbitration, which in the case of International-Level Drivers is the Court of Arbitration for Sport.

I agree that all decisions of CAS under the rules shall be final and binding and that I will not bring any claim, arbitration, lawsuit or litigation in any other court or tribunal.

5. I have read and understand this Acknowledgement and Agreement.

Date

Print Name (Last Name, First Name)

Date of Birth
(Day/Month/Year)

Signature (or, if a minor, signature of legal guardian)



ENVIRONMENTAL CODE



The UIM Environmental Code
has been acknowledged by RINA and allowed the UIM to be nominated
“2011 GREEN ASSOCIATION”

TABLE OF CONTENTS

1. GENERAL PRINCIPLES	91
2. UIM ENVIRONMENTAL WORKING GROUP (EWG)	93
2.1 Role and responsibilities	93
3. ENVIRONMENTAL STEWARD.....	94
3.1 Role and responsibilities	94
4. UIM ENVIRONMENTAL DELEGATE	95
4.1 Role and responsibilities	95
5. NOISE.....	95
5.1 Introduction	95
5.2 Noise of powerboats	96
5.3 Sound level measurements.....	97
5.4 Public Address System	97
6. FUEL	97
7. PROTECTION OF GROUND AND WATER	98
7.1 Environmental Mat	98
8. CLEANING OF POWERBOATS	98
9. ACTION TO BE TAKEN BY DRIVERS / PARTICIPANTS	99
10. REQUIREMENTS TO ENCOURAGE ENVIRONMENTAL BEHAVIOUR BY THE SPECTATORS.....	99
11. ACTION TO BE TAKEN BY INTERNATIONAL ORGANIZERS	100
11.1 Environmental Provisions.....	101
11.2 Logbook / Check-list.....	102
11.3 Publicity/Advertising	102
11.4 Driver paddock / Service Areas / Time Control Areas	103
11.5 After the event.....	103
11.6 General regulations for the management of venues	104

12. RECOMMENDATIONS TO ALL SEA/LAKE/RIVER USERS	104
12.1 Introduction	104
12.2 Driver conduct	104
13. UIM ENVIRONMENTAL AWARD	105
14. UIM TRIANNUAL WORLD CIRCUMNAVIGATION RACE	106
15. GREEN LOGO	106

1. GENERAL PRINCIPLES

DEFINITION

1.1

Powerboat activities fall into three main categories: competition, recreation and transport. This code includes competitive activities for members running the UIM racing classes included the UIM rulebooks for offshore, circuit, pleasure navigation and aqua bike. The code is a working document until fully adopted by the UIM General Assembly.

1.2

Competitive powerboat racing events are a major constituent of the international and national sporting calendars, attracting an increasing audience and producing significant economic effects. It is a sport which, like most other sports, and human activities in general, creates an impact on the environment.

1.3

Power boating activities in general, as a means of transport or for recreation, continues to increase, and therefore must also be looked at from an environmental point of view. The UIM will do its best to transfer best practices from competitive power boating to these other areas.

1.4

The UIM considers it to be of major importance to develop a coherent environmental policy, taking into account the legislative and regulatory requirements of each country.

1.5

The UIM will seek to establish at all times the highest environmental standards during the organization of powerboat events at all levels and will promote environmental consciousness among all powerboat racers. The UIM will do so in close co-operation with the National Authorities and all involved stakeholders.

1.6

The UIM environment policy, as defined in this Code, is based on mutual respect of the needs of the environment and of reasonable practices of powerboat sports and powerboat driving in general.

1.7

The UIM seeks close co-operation with international authorities and organizations in order to ensure that there are sufficient facilities for powerboat activities in environmentally acceptable conditions and encourages its member nations to do so at national, regional and local level.

1.8

All national federations affiliated to the UIM shall do their best efforts in order to give proper prominence to environmental matters and to the principles of the UIM Environmental Code.

1.9

According to the above general principles, this Code prescribes regulations and recommendations to improve the relationship between Power boating competitions and the environment.

These regulations and recommendations refer in particular to:

- a) Climate emissions in cooperation with guidelines given from the UNEP and minimize to reach zero emissions in the future.
- b) Noise, fuel and protection of the water
- c) Behaviour of the spectators, organizers, officers and race participants in order to maintain the highest possible standards worldwide

1.10

The UIM encourage all national authorities to issue guidelines in line with the above and taking into considerations o regional and national legislations.

1.11

The non respect of a requirement of the UIM Environmental Code by an organizer or a racer or the person responsible for his racing team is liable to a fine, a disqualification from the event or a suspension. Moreover, the participant/driver/pilot may be liable for the damages caused by his non-respect of the environmental provisions.

2. UIM ENVIRONMENTAL WORKING GROUP (EWG)

2.1 ROLE AND RESPONSIBILITIES

Role

The Environmental Group role is to advise the UIM on what policy should be adopted in terms of environmental protection, alternative energies and support for sustainable development and, through its members, support the UIM programs and activities in this field.

Responsibilities

- Raise awareness on environment, alternative energies and sustainable development among UIM National Authorities and UIM stakeholders
- Disseminate knowledge and ensure know-how transfer in the above areas
- Promote environmental responsibility in the planning and staging of UIM events
- Ensure compliance of UIM affiliated members with the Environmental Code
- Promote the creation of an Environmental Working Group within National Federations
- Organize environmental seminars and establish a high profile network of correspondents
- Prepare an annual environmental report
- Presentation of the EWG work and findings at the UIM Council and General Assembly

- Advice the national environmental working groups on how to tackle environmental issues
- Liaise with external institutions such as public authorities, environmental organizations and other sporting bodies

3. ENVIRONMENTAL STEWARD

3.1 ROLE AND RESPONSIBILITIES

Role

At the UIM titled events there should be an official called the Environmental Steward who shall deal only with environmental aspects and who must have successfully completed a seminar organized by the UIM Environmental Working Group (hereafter EWG).

Responsibilities

- Ensure that the UIM Environmental Code is respected.
- Have access to all information concerning the event, and must be able to give prior, during and after the event, recommendations to the OOD on all aspects of the event which may have potential environmental consequences.
- Draw up in an appropriate manner a report on the basis of a check-list and send it to the UIM Secretariat. A copy should also be handed to the OOD and the UIM Environmental Delegate if present at the event. In case of a first non-respect of this point, the Environmental Steward will receive a warning from the UIM or its national federation. In case of a second offence, the UIM may pronounce against the Environmental Steward a suspension of up to 12 months.
- Instruct officials during national seminars for the various disciplines, as well as for racers and organizers.
- In case of non-respect or violation of the provisions mentioned in the present Code, he must immediately first inform the OOD.

4. UIM ENVIRONMENTAL DELEGATE

4.1 ROLE AND RESPONSIBILITIES

Role

The UIM Environmental Delegate is a member of and is nominated by the EWG. There is a need to find the premises for the Environmental Delegate to be able to operate. The UIM Commissioner will handle the environmental observations on title championships and include the observations in their report.

Responsibilities

- Observe and verify the application of the Environmental Code.
- Inform the OOD of any violation of the Environmental Code in order to pronounce sanctions relative to the infraction.
- Identify areas of “good practice” and make recommendations or proposals of modifications of the Environmental Code.
- Perform inspections of the racecourse and its facilities at any time before, during or after the event.
- Prepare a report summing up all important aspects of the event that relate to the environment.
- Any serious failure of the Environmental Steward towards the obligations and due diligence mentioned in article 2 of the present Code noticed by the UIM Environmental Delegate will be examined and, in case of need, sanctioned by the EWG with withdrawal of the Environmental Steward’s license for a period of up to 12 months.

An educational program is mandatory for the UIM environmental delegates.

5. NOISE

5.1 INTRODUCTION

Concerns regarding noise at powerboat events are not limited to the machines themselves. In addition to the expected engine noise, organizers and

environmental stewards must be aware of the magnitude of sound from public address systems, crowds and other sources associated with an event. Minimizing excessive noise associated with powerboat activity and taking public reaction to noise in consideration is the responsibility of all concerned: drivers, clubs, organizers and officials.

5.2 NOISE OF POWERBOATS

Sound is a measurable phenomenon created when a source, such as a powerboat engine, causes the air to vibrate. In contrast, noise is an individual interpretation of the impact of that sound. A sound enjoyed by one may be annoying or even harmful to another. Powerboats with high sound levels are almost always considered noisy. The Environmental Steward must understand the difference between the two and how sound is quantified. The decibel (dB) is the unit used to express sound pressure levels and they are measured on several scales. Powerboat sound is tested on the “A” weighted scale and is expressed as dB (A). Sound pressure levels increase at a logarithmic rate (very quickly) while the human ear interprets that increase more slowly. As a consequence, each time the number of identical sound sources is doubled (as with many powerboats at the starting line), the sound pressure level measurement is increased by only 3 dB(A). Sound levels decrease as the distance from the source increases. A doubling of the distance from the source to the ear causes a loss of 6 dB (A). Temperature, geography, elevation, humidity and the frequency of the sound waves also contribute to the rate of energy loss. Such things as foliage, uneven ground and sea and large obstacles, such as walls, cars or embankments will reflect sound and affect sound levels in the immediate area.

The UIM recommends:

- a) To avoid all unnecessary running of engines.
- b) To reduce as much as possible the sound levels in all disciplines and ensure that all applicable regulations are strictly respected.
- c) The UIM shall always promote research on the question of sound level in relation to powerboat sports.
- d) The UIM will aim to set regulations stipulating the maximum permitted noise levels for each class. Noise levels should be set for the different classes in cooperation with the different commissions. When the limit has been set the regulations shall be monitored and sanctioned strictly.

5.3 SOUND LEVEL MEASUREMENTS

Environmental Stewards and event organizers should be familiar with local ordinances governing both event and powerboat sound levels. For details of specific sound levels for each discipline refer to the relevant rules. The UIM noise control method and sound levels are also recommended for national or club events.

5.4 PUBLIC ADDRESS SYSTEM

- a) Separate public-address systems for drivers' paddock and the public areas/enclosures are preferable and should never produce a higher sound than the hearing damage threshold level 83 dB(A) when measured in a public area and should also not exceed 3 dB(A) above the background and sea sound levels when measured at the nearest dwelling house.
- b) Position of loud-speakers: slanted towards the ground and directed towards the centre of the course.
- c) Maintain sound level as low as possible. The whole system must be ready and tested 30 minutes before the start of the practice. The sound system often causes more annoyance outside the track area than the actual event. Make arrangements with the people in charge of the sound system to reduce the sound volume between races/practice sessions.

6. FUEL

It is recommended that regular unleaded fuel, available at the service stations, or other cleaner fuels provided by the organizers, without additives, except oil for two stroke engines, is used.

The use of alternative energies such as bio fuels, hydrogen or electricity, as long as they are not more noxious for the environment, must be encouraged for all disciplines in conformity with the relevant technical regulations. For the purposes of protecting the environment, provisions regarding fuel storage mentioned in the relevant rules must be respected.

Transition implementation – When a Class has more than 50% of the entries using EPA engines at a World Championship then the non EPA engines are phased out.

For details, refer to the technical rules of the various sporting disciplines.

7. PROTECTION OF GROUND AND WATER

- a) Measures must be taken to prevent leaks of fuel, oil, cleaning, degreasing, cooling and brake fluids, etc. into the ground and water or vapour into the air.
- b) Containers/facilities to recover rubbish, oils, chemicals, detergents, etc. must be provided.
- c) The use of an environmental mat, (or other effective device) protecting the ground and water, is compulsory to be used where servicing of machines is permitted by the organizer, amongst others in the paddock and repair areas. Any infraction of this rule will be reported to the OOD who will fine the driver/pilot responsible a maximum of USD 1000 or any other amount mentioned in the regulation or appendix of the discipline.
- d) Provision must be made for the treatment of spillage and the disposal of contaminated material by the organizers.
- e) It is strictly forbidden to empty onto the ground and water waste fluids from vehicles or boats located in the drivers' paddock and the campsite. Waste water may only be disposed of at the circuit a proper facility to this effect. Any infraction to this rule will be reported to the OOD who will fine the driver/pilot responsible a maximum of USD 500. Other sanctions may be pronounced by the OOD or the UIM Commissioner.
- f) Do not leave on the spot sustainable evidence of any event, club or boating activity.

7.1 ENVIRONMENTAL MAT

The Environmental Mat is compulsory for all disciplines. It must be composed of an absorbent part and an impermeable part. Its use will be compulsory everywhere where work on powerboats is allowed by the organizers.

8. CLEANING OF POWERBOATS

- a) Cleaning of engine and motor parts, where permitted by the regulations, must only be carried out at places with cleaning facilities.
- b) Only certified non toxic solvent, without the addition of chemical products (for example detergent), is permitted when cleaning the boats.

- c) The cleaning area must be built with a non-porous surface and a proper drain with an oil-divider to prevent pollution of the ground and water. Any infraction of this rule will be reported to the Officer of the Day (OOD) who will fine the driver/pilot responsible a maximum of USD 500. – or any other amount mentioned in the regulation or appendix of the discipline.

9. ACTION TO BE TAKEN BY DRIVERS / PARTICIPANTS

- a) Each driver is responsible for the waste generated by his team during the event.
- b) Where organizers provide the necessary containers for waste they must be used as directed.
- c) Waste must be retained by the team until the approved facilities provided by organizers can be used.
- d) Where refuelling or servicing of a powerboat is being undertaken at events, driver/participants must provide and use an environment mat to protect the ground and water. This environmental mat must also be compulsory when an electrical generator or any other device with a thermal engine is used by the driver/pilot or the team. These mats must be removed by the driver/pilots/competitors after use.
- e) Any infringement by the participant or driver/pilot (who is responsible for his team) of the UIM regulations can result in a fine, disqualification from the event or suspension, and may also result in the participant or driver/pilot being liable for any costs of rectification.

10. REQUIREMENTS TO ENCOURAGE ENVIRONMENTAL BEHAVIOUR BY THE SPECTATORS

Visitors to a powerboat circuit, track, event or gathering can play an important role in keeping the environment clean and undamaged. Here are some suggestions:

- a) In co-operation with the local authorities, select the routes to and from circuits, tracks, etc., which will cause as little annoyance as possible for the surrounding areas.
- b) Provide clear signs to circuits, tracks and venues.
- c) Avoid parking on vulnerable places (verges, green lanes).
- d) Avoid parking in long grass.
- e) Promote the use of public transportation in publications and promotional releases i.e. web or other forums.
- f) Avoid too high concentrations of people in order to preserve vulnerable places i.e. wildlife and bird reservation.
- g) Provide sufficient sanitary facilities and safe cleaning and deposit systems.
- h) Inform the spectators about responsible behaviour on the site.
- i) Specify in contracts with catering firms a requirement to sell drinks and food packaged in recyclable, reusable or biodegradable material, and to provide and maintain sufficient waste containers. Promote cooperation with caterers that have high standards on ethical and ecological food.
- j) Manage the events in such a way that only footsteps remain on the soil.

11. ACTION TO BE TAKEN BY INTERNATIONAL ORGANIZERS

Promote environmental behaviour among all persons involved within the organization and promote environmentally friendly work within the secretariat and the administration to save paper and energy.

Venues creating participants more than 50 and/or spectators more 20 000, shall make a more comprehensive written environmental plan including the following points:

1. Use of resources (energy, chemicals and materials)
2. Impact on environment and climate emissions
3. Carbon offsetting all activities related to events from
4. Recycling and reduction of waste

5. Travelling and accommodation for spectators and guests
6. Birds, animals and wildlife
7. Visual environment

Any organizer of an event under the authority of the UIM is subject to its regulation and jurisdiction. Any clearly established infraction or inobservance of the prescriptions and obligations for the organizers are subject to the penalties laid down in the present Environmental Code or in other UIM disciplinary procedures.

Infringement of Article 11 will be reported to the UIM ExCO who can fine the organizer a minimum of USD 1000 or any other amount mentioned in the regulation or appendix of the specific discipline.

11.1 ENVIRONMENTAL PROVISIONS

Make arrangements to prevent pollution of the ground and water in the drivers' dry/wet pits and surrounding areas. Look after the protection of verges along the access roads, parking, and fencing off (tape) vulnerable areas. The facilities for practice, racing, gatherings and the surroundings must be protected as much as possible from pollution and annoyance. Having regard to the expected number of spectators, provide and maintain sufficient and clean sanitation, with proper provisions for waste and water.

Measures must also be taken to prevent:

- a) Leakage of fuel, oil, cleaning, degreasing, cooling and brake fluids, etc. into the ground and water or vapour into the air.
- b) Waste from participants, their teams, spectators and selling points not being collected in waste-bins or containers and being left on the ground and water at the site.
- c) Sanitary facilities being connected improperly to the sewerage.
- d) Waste collected and removed in an illegal way.
- e) UIM sound levels for powerboats during practice or event being exceeded.
- f) Inordinate sound levels at tourist gatherings.
- g) The maximum number of events and/or practicing time being exceeded.

11.2 LOGBOOK / CHECK-LIST

It is important for every motor sport event organizer to maintain a “logbook” in which is written all activities and measures that have been taken, what meetings have taken place and with whom, what permissions have been sought and those that have been granted relative to the use and maintenance of the facilities for powerboat sport or other event. If permission is only given for use of the facilities for a certain period of time, the exact time when it is in use must also be noted. Together with the logbook it is useful to use a check-list for the management of the facilities, to stimulate an environment friendly behaviour by the spectators and an environment friendly use of the facilities during events. This check-list must include at least the following points:

- a) An assessment of the environmental impact of the event on the circuit and its surroundings, having regard to the number of competitors and the expected number of spectators.
- b) Cleaning area with high-pressure cleaners for powerboats.
- c) Distribution of waste bags to participants and their teams.
- d) Containers for used oil, cooling fluid or any other liquid.
- e) Containers for spectators’ general litter.
- f) Sanitary facilities.
- g) Judicious positioning of loud speakers.
- h) Provision of adequate and appropriate direction signs to the event.
- i) Provision of containers or waste bags in the pits of the mechanics’ area and collection of the contents during and after the event.

11.3 PUBLICITY/ADVERTISING

- a) The UIM suggest aggressive use of the Web and media instead of prints/posters on paper.
- b) Do not attach posters to trees, in country areas and/or protected urban-sites nor in places and areas which are not appropriate.
- c) Place billboards only after having obtained permission from the owner of the property.
- d) Take local and governmental regulations into account when placing billboards.

- e) Do not distribute leaflets/pamphlets under windscreen wipers of cars, on powerboats, nor to spectators (do not allow others to do so during your event).

11.4 DRIVER PADDOCK / SERVICE AREAS / TIME CONTROL AREAS

- a) Present every participant on arrival with a rubbish bag and instructions.
- b) Provide sufficient containers for the rubbish bags and set recycling stations accessible to the public and participating teams.
- c) Ensure that there are enough containers with fixed funnels to collect used oil.
- d) Provide recognizable containers for oil filters and cleaning rags; collect separately.
- e) Provide and maintain sufficient and clean sanitation, with proper provisions for waste and water, for both men and women.
- f) If cleaning of powerboats is allowed, a special wash area designed to ensure the ground and water is not polluted must be provided in accordance with the rules of the discipline concerned.
- g) The use of an environmental mat is compulsory when a generator or any other device with a thermal engine is used.

11.5 AFTER THE EVENT

- a) Signposts, billboards and posters must be removed after the event.
- b) Waste left behind on the site and the surroundings must be cleaned up and removed as soon as possible.
- c) Clear away and dispose of any broken branches or shrubs.
- d) Clear away tape, buoys and rope/weights around the track.
- e) Arrange for separate removal of oil-containers, cleaning rags, oil filters and waste-containers.
- f) The infraction or inobservance by the organizer of obligations mentioned in the present Environmental Code are subject to sanctions laid down in this Code and can result in a fine or suspension of approval to organize UIM events. The organizer will also be liable for any costs of rectification.

11.6 GENERAL REGULATIONS FOR THE MANAGEMENT OF VENUES

- a) Keep up the maintenance of the venue and take care that it is kept clean and tidy at all times.
- b) Cordon off sensitive areas.
- c) Appoint a member of the Organizing Board to be in charge of all environmental aspects and maintain the environment logbook.
- d) Ensure proper disposal of waste from sanitary facilities.
- e) Take all necessary care when making changes/adjustments to the site and consult the appropriate authorities.
- f) When locating the starting areas, take acoustic impacts into account.
- g) Never use building waste, rubble, etc. for the construction of noise barriers, etc.
- h) In case of the use of materials from outside, have them checked by the appropriate authorities to ensure that they are not polluted.
- i) Following every event and at regular intervals, make an evaluation of the impact of the event on the environment; make a report of it in the logbook; and make recommendations to correct any shortcomings and errors.

12. RECOMMENDATIONS TO ALL SEA/LAKE/RIVER USERS

12.1 INTRODUCTION

We must voluntarily moderate and add a greater degree of responsibility to our natural desire for individual mobility. We should try to achieve results on the basis of freedom of thought and movement, and exploit every opportunity to combine the pleasure of Powerboating more effectively with ecological and economic needs.

12.2 DRIVER CONDUCT

- a) Individual power boaters should develop driving habits to ensure full integration with other types of sea, lake and river users.

- b) Ride safely and avoid aggressive driving.
- c) Save petrol and reduce pollution by avoiding unnecessary idling of engines.
- d) Limit noise pollution
- e) Noise annoyance. Use exhaust system (towards/into water) which minimize noise and keep audio systems at a low level.
- f) Protect wildlife and its natural habitat
- g) Ensure that your used oil, batteries and other recyclable items are properly recycled or collected.
- h) Remember that our seas/lakes/ivers belong to the overall community.

13. UIM ENVIRONMENTAL AWARD

In order to encourage a greater awareness of environmental concerns, the UIM has created an Environmental Award to reward a significant contribution to the protection of the environment. This Award may be given each year according to the following rules:

- a) By “year”, the EWG understands that the year taken into consideration to award the prize starts on 1 January and ends on 30 September. The candidatures must arrive at the UIM Secretariat by the 5 October at the latest.
- b) This distinction is granted for rewarding individuals, clubs, organizers, manufacturers or other organizations that have made a significant contribution or done something important to enhance environmental awareness in the field of Power boating
- c) Candidatures for the Environmental Award will be submitted to the UIM by October 5th at the latest. The EWG may also propose a candidate.
- d) The candidatures received will be examined by the UIM Environment Group prior to submitting them to the ExCo. The recipient can be invited to the UIM GA to receive his/her (their) distinction.
- e) These nominations should be submitted, along with a description of the occurrence qualifying for the Award, to the UIM Secretariat within the set deadline

14. UIM TRIANNUAL WORLD CIRCUMNAVIGATION RACE

Every three years the UIM may organize a world circumnavigation race. The race shall be performed on alternative energy and sustainability principles.

15. GREEN LOGO

The UIM have approved this Green Logo



101 - UIM EVENT SANCTIONS AND THE SPORTS CALENDAR

All international powerboating events must be sanctioned by the UIM and by the National Authority. An International Sanction is a written authorization which permits an organizing body to conduct an international event under the rules of the UIM.

Written approval by the UIM of the registration of an international event on the UIM annual calendar, subject to payment of a fee determined annually by the General Assembly, constitutes the granting of an International UIM Sanction for that event.

Payment of these fees must be made within three months after the fixing of the International calendar. It is compulsory even if the meeting does not take place.

Permission to organise these meetings is only delivered by the UIM upon payment of these fees. No permission is to be granted to National Authorities in arrears of payment.

International events organised by clubs affiliated to a National Authority and which have not been entered for reserved dates on the Sports Calendar may have to pay a fine up to five times the usual fees.

The registration fee is to be doubled for any meeting requested after the deadline dates set by the rules.

All requests to organise an International championship must be sent to the UIM Secretariat before the time set by the deadline in the rules.

The National Authority where the event starts shall be the main sanctioning body and responsible to the UIM.

All National Authorities through whose waters the event passes must also approve the event.

When compiling the Offshore Calendar due attention is to be paid to the confidential reports of the UIM Commissioners before granting the authorisation to organise titled meetings.

The organisers must name the series and classes which may participate and also give the name and address of the organisers and/or the responsible person (s) who are in a position to supply information about the event. They must also name the type of races: titled, championship, etc., the locality, as well as the date of the inscriptions.

Such information must be provided as soon as possible.

Fees for multi-class events :

For Championship events held for one class or sub-class, there shall be one inscription fee.

For Championship events for two or more classes or sub-classes, the fee shall be doubled.

Championship events for 1.3 and 2 litre classes or Offshore 1.0 and 1.3 litre classes, when raced together shall require only one fee.

These “events” may have up to three races (heats), providing they are all run at the same venue and do not exceed the time interval between races (heats) as allowed for in the relevant Championship series rules.

101.02 - OTHER MEETINGS

It is to be noted that the offshore Calendar also lists the registered ordinary International meetings.

The calendar of national or local events is not to be submitted to the UIM. The interested National Authorities take responsibility for their own calendar.

The organisation of an event cannot be granted if the title, the locality and the date of said event are not given.

The UIM does its utmost to avoid clashes between ordinary meetings. However, whether the interested parties agree or disagree, the UIM lists all requests for dates, leaving the organisers to sort out their responsibilities.

The titled meetings have priority over all other meetings before the closing of the registrations for the Offshore Calendar.

However, when the Offshore Calendar is finalised all the motor boating meetings are on an equal footing.

A titled meeting which has been cancelled does not count any longer for the title. If the organisers decide to run the race in spite of the cancellation, the race must be considered as an ordinary International race.

No national championship race may be run at the same week-end as a UIM World, Continental, or Regional Championship race, in the same continent, for the class concerned.

For a World Championship, no national championship race may be run on the week-end before the World Championship, run in the same continent.

This is providing the championship race has been announced in accordance to UIM dates.

101.03 - DEADLINE DATES

10th September : the deadline date for the UIM Secretariat to receive from the N.A., the dates for the championship races and ordinary International meetings they wish to enter in the Offshore Calendar for the following year. Also at this time, requests may be lodged for any World or Continental Championship not yet allocated or cancelled before this date.

15th November : the deadline date for the UIM Secretariat to send, to all concerned, the final Offshore Calendar for that calendar year.

1st March : the deadline date for the UIM Secretariat to receive from the National Authorities, requests for Continental or World Championships 1 year in advance. It is not necessary to add the locality; the nation is sufficient. These championships will be allocated by Cominoff at the Mid term Cominoff meeting”.

Requests in writing, e-mail or by fax to the UIM Secretariat will be accepted.

Any change of date of a Continental or World Championship, notified after 28th February, in the year that the event is to be run, may be penalised by the payment of a fine up to five times the amount of the registration fee (to be determined by Cominoff).

101.04 - CANCELLATION

If the event is cancelled 30 days or more before the calendar date, 50% of the inscription fee will be refunded. For later cancellations there will be no refunding.

However, if cancellation is due to insufficient participation and is not fault of the organiser, Cominoff may review this.

102 - WORLD CHAMPIONSHIPS

World Championships are open to all UIM classes as well as A.P.B.A. classes. There can be only one World Championship per class, per year. Cominoff will allocate the Championships.

103 - RACES RECOGNISED BY THE UIM

The UIM recognises the following events:

- Titled International meetings
- Ordinary International meetings
- National and local meetings
- Private meetings or club meetings
- International cruises
- Titled offshore races
- Ordinary offshore races

104 - NON APPROVED RACES

All races not organised according to UIM rules are not to be recognised and all officials and drivers who take any part in such races are suspended or excluded if offence recurs.

The same applies to motor boating events not approved by National Authorities.

Any driver having competed in a race not approved by the National Authority of that country or forbidden by the National Authority may be suspended for a period to be fixed by the National Authority.

In the case of an International meeting being organised by a non-affiliated club which has not asked permission to organise such a meeting the National Authority must:

- Notify the organising club that the drivers can be suspended.
- Notify the drivers that they will be suspended in their own country if they take part in International races not approved by the National Authority; the latter may request the UIM to extend this suspension to other countries.

A national race, or a race by "invitation" in which a foreign driver is "invited" to take part must be entered in the UIM Calendar otherwise it becomes an unauthorised event. Invited drivers entering such unauthorised races, may lose their International licences. Remember that drivers having International licences, are only allowed to enter a meeting outside their country provided their National Authority gives them permission to do so.

105 - TITLED INTERNATIONAL EVENTS

The word "International" defines the type of race. It is impossible to know beforehand the number of the countries which will participate.

There are three categories of titled meetings:

- World Championships
- Continental Championships and Hemispheric Championship
- Special events as approved by UIM Cominoff

No meeting is allowed a title unless authorised to do so by the UIM who draw up the annual list.

The UIM has the right to refuse a title if it thinks that the amount of prizes or the organisation are not acceptable.

Titled meetings must recur as far as possible at the same time of the year.

UIM aims to protect titled meetings, the Cominoff may refuse to sanction any meeting which takes place at the same time as a titled meeting. Such refusal is only notified at the specific request of the National Authority lodging a protest.

In order to promote new series and classes, new hulls, motors or engines, new ideas regarding propulsion and new concepts of racing, the Cominoff may, provided the request to organise be accompanied by an amount equal to double the fee of an ordinary meeting, approve the organisation of International contests for such boats, motors or engines, type of propulsion and races of a kind not yet known to the UIM provided such events do not clash with other UIM titled meetings.

During prize presentations and ceremonies at UIM titled events, the UIM medals shall be presented by the highest ranking UIM representative(s) in attendance at the event.

120 - RACING LICENCES

120.01 - COMPULSORY LICENCE

No one can take part in a race, establish or improve on a record, or test drive at any titled event as a driver or crew in International class boats, without being in possession of a licence from his National Authority.

Anyone responsible for controlling a boat in any competition and holding the necessary driver's licence issued by the National Authority is considered to be the driver.

Any driver or crew member that rides in a boat fitted with restraints must partake in a suitable Driver Immersion Test annually prior to being issued with his current licence. Such licensed persons shall have their licenses endorsed as "Driver Immersion Tested : expiry date/...../..... (day/month/year)".

120.01.01

In order to obtain an International licence or Super Licence, following disclaimer must be signed by the licence holder :

"The undersigned ... licensed driver (licence n° ...), participating to UIM sanctioned events, yields the media rights of any coverage by the promoter taken during the event, to the UIM for further use press, radio or television. The right however remain the property of the undersigned.

In addition I herewith agree and acknowledge the following:

UIM is the governing rule making body of UIM racing events which are responsibly organized by clubs, national authorities of UIM, contracted promoters or other organizers. These organizers do not act on behalf of the UIM and UIM is not responsible for any act or omission of such organizer and shall not be liable to the participant accordingly.

The UIM sport and technical rules are intended to minimize risks but they cannot provide the highest possible safety standards at all times. Residual risks might remain.

Enforcement of the rules by UIM or other race officials and in particular but not limited to the technical scrutineering does not guarantee the safety of racing or the safety of the scrutineered boat. Scrutineering is not intended as a construction/condition survey. The racing license issued by the National Authority or a super license issued by the UIM does not guarantee that a driver is physically able to race safely nor that he has sufficient experience or education.

Teams and drivers are solely responsible for their own safety including but not limited to their physical and educational ability to race in the relevant class, the safety of their boats and other gear and the safety of their racing activity. This responsibility includes racing with prudence and taking technical measures which are not mandatory in the rules but deemed necessary.

UIM shall not be liable for any damage, injury or death due to inadequate rules, breach of existing rules by participants or failure to enforce rules by the race officials.”

The signed disclaimer can be requested by the UIM commissioner at any UIM sanctioned event.

120.02 - REGISTRATION

The National Authority establishes each year a list of people to whom international licences have been delivered and this must be sent to the UIM Secretariat.

The list must include: the name and surname, the address, the class(es) for International Licences only and the Anti-Doping Appendix 2 of UIM Anti Doping Rules duly signed per acceptance.

These lists may be sent to others National Authorities upon request.

Licence numbers are given annually by the National Authority to the owners and drivers entered on the list.

Any person wishing to obtain a licence must apply to the National Authority through his club.

The licence is granted by the National Authority and is marked “International” as decided by the National Authority. Licence holders must buy a rule book.

Any National Authority has the right to deliver licences to:

- its nationals;
- nationals of another country affiliated to the UIM but with the agreement of the National Authority of that nation.

With the approval of the UIM a National Authority may also deliver licences to persons belonging to a country which is not yet affiliated to the Union.

Nobody is allowed more than one driver’s licence.

A National Authority may refuse to give a licence without having to state the reason. Notification of this is sent to all affiliated clubs and to UIM.

120.03 - VALIDITY OF LICENCES

Licences are valid from the 1 st. January until the 31 st. December each year, unless local circumstances require other dates, but validity should always be shown on the licence itself.

An International licence is valid in any country affiliated to the Union and entitles the holder to enter or drive his boat in all competitions organised in any affiliated country, provided he complies with what is stated in the UIM general racing rules.

However, for any event held under special rules, the licence holder must abide by special rules.

First and second named drivers competing in the World Offshore Championship for UIM Class 1 must obtain a UIM Offshore Super Licence as described in 860.12.

120.04 - COST OF LICENCE

The cost of the yearly licence is to be fixed by the National Authority.

120.05 - EXAMINATION OF LICENCES

At any meeting, the holder must produce his licence at the request of any official of the meeting.

120.06 - CANCELLATION OF LICENCE

Anybody who takes part in an unauthorised meeting loses his licence.

However, if the unauthorised meeting is held in waters of a country other than the one which has delivered the licence, both National Authorities must agree as to the duration of the suspension. The UIM decides finally in case of dispute.

120.07 - PSEUDONYM

The use of a pseudonym must be requested from the National Authority and the National Authority marks the pseudonym on the licence.

A licence holder, so long as he is listed under a pseudonym, may not take part in any sports event under any other name.

Changing a pseudonym is subject to the same formalities as the adoption of it. Anyone who has chosen a pseudonym may not return to his actual name until the National Authority has agreed and has delivered a new licence.

120.08 - ELECTION OF DRIVERS REPRESENTATIVE

There may be a Driver's Representative from each International racing class.

The election of each representative must be held in the presence of an UIM Official. Candidates must be real drivers. To be eligible, they must have obtained the following minimum experience :

Have competed in International races and be regularly racing. Candidates must be nominated by their National Authority, prior to the mid term meeting and selected by Cominoff.

Driver's Representatives are elected for a 1 year term, beginning January and ending the following December. They should be available to attend Cominoff meetings at least annually to report and advise on the consensus recommendations and requests from the drivers'. Otherwise a full written report must be forwarded to the UIM Cominoff 30 days prior to the General Assembly.

120.09 - MODEL APPLICATION FORM FOR A LICENCE

120.09 - MODEL APPLICATION FORM FOR A LICENCE

The undersigned _____

Date of Birth : / /

begs to apply for
a licence

National
International

Driver's
Owner's

I have read the UIM licence rules and will abide by same.

The pseudonym chosen is _____

Enclosed please find the licence fee _____

_____ the _____

signature _____

address _____

Member of _____

To be sent to the Secretary of the National Authority through the Club.

120.10 - MODEL OF LICENCE

20 _____

The National Authority's
Licence Book)

N° _____

LICENCE
NATIONAL OR
INTERNATIONALOWNER'S
DRIVER'S

Delivered to M _____

Member of _____

Valid until _____

Pseudonym _____

"Driver Immersion Tested : (expiry date .../ .../ ...)"

(Here name of National Authority)

Secretary,

President

Signature of Licence Holder

200 - RACE ORGANISATION

200.01 - GENERALITIES

The following bodies have the right to organise any power boating event: National Authorities and their Affiliated Clubs, Corresponding Members of the UIM, UIM contracted promoters (with the agreement of the National Authority).

All events have to be sanctioned by the National Authority. For events taking place in more than one country, each country's NA must approve the event.

Affiliation to the National Authority implies acceptance of the National Rules and those of the UIM.

A club, a club member, or a driver who is suspended, is not allowed to take part in any power boating event in his country, or in any other country neither as a driver nor as an official, as long as the suspension lasts.

A member or a driver who takes part in any power boat race in a foreign country affiliated to the UIM, undertakes ipso facto to abide by the rules of the National Authority of that country.

All clubs, race organisers, officials and drivers are obliged to know the general rules of the UIM as well as the rules related to races or series (local, National or International). A copy of the current UIM Offshore rule book, including any addendum, must be available for every organisation.

All offshore races organised by National Authorities or their affiliated clubs must be organised in accordance with the UIM Offshore rules. All advance programmes and racing instructions must mention this.

The general racing rules and the rules related to records are applicable to all the International series.

Any member or driver who infringes any of the rules of the UIM or any rules of the responsible National Authority is liable to be penalised.

All organising bodies, members, drivers or owners who take part in a race pledge themselves :

- to know current UIM Offshore rules and National rules of the responsible National Authority, if any, and also any particular event rules imposed by the event organiser or National Authority.

- to bind themselves to submit without restriction to any consequence following the application of these rules.

200.02 - INTERPRETATION OF THE RULES

In all cases not foreseen by the International rules or seemingly inaccurately defined, the National Authority, or Race Committee, or Race Jury or the Appeal Board, is to judge bearing in mind the spirit of the disputed rule.

The rules for series and classes are to be considered as being supplementary to the Offshore general rules.

In case of conflicting rules, those concerning a specific case/occasion (i.e. series and classes) are to prevail over the general rules.

National Authorities may impose special event rules relating to exceptional local requirements for National events or International events including World or Continental Championships. These special event rules prevail over, but must not conflict with the UIM Offshore rules.

English is the working language.

200.03 - MONETARY PENALTIES

At any titled International event, the Race Committee shall be entitled to impose monetary penalties for breaches of the Rules; either the UIM Offshore Rules or special event rules.

The monetary amount shall be scaled according to the severity of the “breach of the rules”.

All monetary fines shall be based on a standard monetary value. For the purpose of these rules the value shall be expressed in Euros and converted at the current rate of exchange to the currency of the country where the event is being held.

Examples of penalties :

Crew or team members smoking in pit area	110 €
Un-authorized transference of fuel	110 €
Late for briefing or drivers meeting	110 €

Other infringements may attract monetary fines based on the above scale.

The fines must be paid before the start of the race ; otherwise the driver is disqualified.

The Race Committee should take care not to substitute monetary fines for infringements of a technical nature or infringements that would normally result in disqualification or suspension.

Any fines collected shall be deposited in the Race Organiser's bank account and may eventually be used to defray organisational expenses.

Absence from Drivers meeting or briefing

Complete absence from briefing or drivers meeting entails disqualification.

201 - RACE CONTROL

201.01 - ORGANISING BODIES

All races are under the control of an Organising Body approved by and affiliated to the National Authority of the country in which the race is run. The Organising Body nominates :

- An **Organising Committee** which organises the general and administrative aspects of the event.

- A **Race Committee** which organises and supervises the actual racing, determines any special event rules, determines the course, decides whether competitors are eligible or not and is responsible for safety. The Race Committee is composed of the following :

- * the Officer of the Day

- * the Safety Officer

- * the Race Secretary

- * the Officials in charge of the Technical Inspections, Course, Wet/Dry Pits, Starting and Time keeping

- * Any other special official appointed by the Organising Body.

- A **Jury**. Foreign nominees must be authorised by their National Authority.

The Race Committee must inspect and verify all entrants documents required prior to racing, i.e., Measurement Certificates, Licences, Insurances, etc.

The Jury decides on any protest submitted by the Race Committee. The names of the members are to be printed in the official programme.

No officer, official, nor member of the Jury nor member of a National or International Committee or Commission, is allowed to intervene in the judgement of a problem in which he is involved himself or is an interested party.

The composition of the Race Jury must be announced at drivers briefing and published in a race bulletin.

The decisions of the Race Committee and the Jury must be based upon the UIM rules as far as they may apply, but as no rules can be devised capable of covering every case, the Race Committee and the Jury should discourage all attempts to win by means other than fair racing, superior speed and skill.

The following are the Race Officials :

- * the Officer of the Day
- * the Safety Officer (deputy to the O.O.D.)
- * the Race Secretary
- * the Technical Officials
- * the Course Officials
- * the Starting Officials
- * the Timekeepers
- * the Pit Officials
- * the Medical and Rescue Officials
- * the Communication Officials.

The UIM Commissioner is a UIM Official.

201.02.1 - THE OFFICER OF THE DAY

The Officer of the Day must direct the organisation of the races on behalf of the Organising Body. He supervises the drivers briefings, maintains order on the course, ascertains that all services work properly, ensures that all correspond to the rules and observe the Racing Rules according to the Programme. He analyses the written reports of the Officers and Timekeepers and any other documents that will enable the results to be compiled. He verifies any decision to stop or curtail a race. He is not allowed to sit on the Jury.

For all the World Continental Championship events, the Officer of the Day must be a UIM approved person. To qualify, he/she must meet the following minimum standards :

Have had previous experience in controlling at least 3 important races.

Have a good knowledge of the UIM Offshore rules.

Have the ability to speak and understand the English language.

Be nominated by his/her National Authority for "UIM International Officer of the Day" approval

A list of approved persons is held by the UIM Secretariat and reviewed each year by Cominoff (considering also any reports from UIM Commissioners). Nominations for International O.O.D. approval shall be sent to the UIM by the nominee's National Authority, accompanied by details of his/her qualifications and previous experience.

National Authorities must keep a list of their approved persons up to date and notify the UIM accordingly of any changes.

National Authorities are strongly encouraged to arrange for their Officers to attend training sessions.

201.02.2 - THE SAFETY OFFICER

The Safety Officer is deputy to the Officer of the Day. He/She co-ordinates (in harmony with the O.O.D. and the medical and rescue officials) the safety services on shore, on the water and in the air.

At the scene of an accident a qualified doctor can take command of emergency services and actions.

201.02.3 - THE RACE SECRETARY

The Race Secretary is responsible for the general organisation of the event. He/She must insure that all the various officials know their respective duties and that they have the necessary documents and equipment to fulfil their duties. He/She must collate all documents following the race.

201.02.4 - THE TECHNICAL OFFICIALS

The Technical Officials are to make sure that all drivers have their Measurement Certificates and Homologation Sheets (or production verification) available for

inspection and shall verify that the hull and engine(s) conform to the measurement certificate, homologation sheets and the rules.

They check control and safety devices for boats and drivers as provided for in the rules. Scrutineers should be specifically delegated for this duty.

Special attention must be paid to engine and fuel shut-off devices (where applicable), driver protection and safety devices, fire protection equipment and that the racing number is clearly visible.

After the race, they must carry out all the necessary checks.

It is not permitted to disqualify any participant for reasons that were visible during pre-race inspection.

201.02.5 - THE COURSE OFFICIALS

The Course Officials must ensure that the drivers and boats conform to the racing rules. At the end of the race, they draw up and sign a statement certifying that all participants conduct during the race and at the turn marks was according to the rules. This statement is handed to the Officer of the Day who will apply the International rules in the case of infringements. The Course Officials monitoring turning marks must have a clear un-obstructed view.

201.02.6 - THE STARTING OFFICIALS

The Starting Officials are responsible for the safe and orderly execution of the race start. They must closely observe all boats during the starting procedure to ensure that the rules are adhered to and report any infringements.

One Starting Official must be on the start boat.

201.02.7 - THE TIMEKEEPERS

The Timekeepers are responsible for recording the start time, all lap times and the finish time and finish order of a race. They must be located at a position where they have clear un-obstructed viewing of the start and finish lines. In some races they may be required to carry out lap timing at turning points other than just the start and finish lines. Timekeepers must work in teams of at least two persons for each timing point. They must use reliable synchronised timing apparatus. They must keep detailed records of the timing of each boat and hand these to the Race Secretary at the completion of the race.

201.02.8 - THE PIT OFFICIALS

The Pit Officials maintain order in the wet and dry pits. They are responsible for the parking of boats, the fuelling of boats, the launching and retrieving of boats and all safety procedures within the pit areas including fire safety.

201.02.9 - THE MEDICAL AND RESCUE OFFICIALS

The Medical and Rescue Officials are responsible for all first aid and rescue procedures. The Medical personnel must be fully trained in emergency accident procedures with emphasis on resuscitation, spinal type injury care and trauma treatment. They must have facilities available for emergency treatment and facilities for rapid transportation of seriously injured persons to the nearest hospital.

The Rescue Officials must ensure that adequate rescue boats and rescue helicopters when required and available with trained personnel at strategic points of the race course and that they are equipped for any rescue eventuality.

201.02.10 - THE COMMUNICATION OFFICIALS

The Communication Officials are responsible for maintaining radio or other alternative means of communication between all key race officials with emphasis on communication between rescue crews and race headquarters.

201.08 - UIM COMMISSIONERS

The presence of a Commissioner of the UIM is necessary at any International titled meeting, except in case of force majeure. The Commissioner is appointed by the UIM Cominoff.

The function of the Commissioner must be taken very seriously.

The UIM Commissioner :

- * ensure that UIM rules and regulations are adhered to
- * assists the local organiser, especially the Officer of the Day
- * is a member of the International Jury with the right to vote
- * signs late alterations of the Advance Programme
- * of the declaration paper by the drivers using a safety cockpit and restraint system

- * takes part in important decisions with the O.O.D. and the Safety Officer during the event
- * approves and advises on safety procedures with the rescue teams and the Safety Officer (especially if a foreign crew is participating)
- * if an emergency decision is required on general organisation, the following committee will be responsible :
 - The Officer of the Day
 - The UIM Commissioner
 - A representative of the National Authority, if present
 - A representative of the Promoter/Organiser
 - A drivers elected representative

The UIM Commissioner in conjunction with the O.O.D. is entitled to take action with the race organisation if safety conditions are not met. In some cases, he will even be entitled to request that an International Championship/event be suspended if safety conditions are not approved.

After the event, he sends his report to the UIM Secretariat using the forms he has received for the purpose within 10 days of the race and these must be accompanied by the official results of the race.

The reports from the Commissioner are to be considered as confidential documents and are not to be used for aims which are contrary to those of the UIM, nor be communicated to the press nor otherwise revealed for any reason whatsoever. Should the Commissioner not have received all necessary data and results for transmission to the UIM Secretariat, he notes the fact in his report and insists that the organisers send every document required, without delay, to the Secretariat of the UIM.

A copy of the Commissioner's report shall be sent by the UIM Secretariat to all Cominoff members. These reports will be reviewed at the next Cominoff meeting.

National Authorities are requested to send to the UIM Cominoff, nominations for Commissioner with the following information :

Full name

Complete address

Telephone - Fax numbers

Languages read and understood

Offshore power boating knowledge and experience.

Cominoff may also nominate candidates for Commissioners. The approval of Commissioners will be made by Cominoff.

A Commissioner cannot be appointed to an event organised by his own National Authority, unless by reason of force majeure.

Following confirmation of appointment, the UIM Secretary writes to the National Authority to announce that such and such Commissioner will come to such event.

201.09 - TRAVELLING EXPENSES AND ACCOMMODATION

The UIM must ensure that the travelling expenses for the Commissioner from his town of residence to the locality where the event is taking place are paid. Air, rail and/or road travel expenses are to be granted at economy rates.

The expenses stated above must be refunded to the Commissioner by the event organiser, or if not, by the UIM within 30 days of receipt of vouchers.

The Commissioner delegate is to be accommodated in a good hotel for the whole duration of the race and as long as is necessary for the accomplishment of his mission, at the expense of the organisers.

The organisers must offer daily meals and local transportation to the Commissioner for the whole duration of the event.

202 - ADVANCE PROGRAMME

202.00 - ADVANCE NOTICE

To promote International titled events, an advance notice should be published at least 6 months ahead of the event. The advance notice should contain the following information :

- The championship title and class(es)
- The date and place of the event
- The name, address, phone, fax, e-mail, website (if any) of the organiser
- Any other information that is available for promoting teams to attend the event.

For Ultra Marathon races, the organisers must provide full details of the race entry fees and conditions.

202.01 - ADVANCE PROGRAMME

45 days before any International race, the organising committee must forward at least one advance programme on the official form to the UIM Secretary, the UIM Cominoff Secretary and the National Authorities. Should an organiser fail to meet this requirement, the UIM Secretary shall deem this International race to be cancelled. The UIM shall notify all National Authorities of the cancellation. The inscription fee shall remain due.

The allocation of future International events to that organiser shall be given lowest priority by the UIM Cominoff.

The above advance programme must be written in the national language and English. It must contain the following information

- 202.02.0 The name, the address and fax number of the organiser.
- 202.02.1 The classes eligible.
- 202.02.2 The race status.
- 202.02.3 The venue and the date of the race(s).
- 202.02.4 The provisional course(s).
- 202.02.5 The races are held under the UIM Offshore rules.
- 202.02.6 The special conditions which in accordance with the UIM Offshore rules may be added or modified.
- 202.02.7 The closing date and place to which entries must be sent, and whether by post or by fax.
An entry form must be included in the advance programme.
- 202.02.8 The amount of the entry fee and details of whom payment is to be made to.
- 202.02.9 All information regarding prizes for each race.
- 202.02.10 Where and when the racing instructions can be obtained from the organising committee.
- 202.02.11 An entry form must be enclosed to the advance programme.

- 202.02.12 Transportation facilities and freight concessions.
- 202.02.13 Details of compulsory insurance for personal and material damage to third parties including other entrants, where required by National Authorities. The amount of the insurance fee to be paid prior to racing to the organising committee.
- 202.02.14 The choice and efficiency of a racing vest is the sole responsibility of the wearer.
- 202.02.15 The wearer is entirely responsible for the choice of his helmet.
- 202.02.16 Should no publicity be allowed during a race, or there be a restriction on any advertising, this is to be mentioned in the advance programme.
- 202.02.17 Dates and times for registration and scrutineering.
- 202.02.18 The protest fee

The maximum amount for a protest fee in an International event will be decided from time to time by the UIM General Assembly.

202.03 - MODIFICATION TO ADVANCE PROGRAMME/ RACING INSTRUCTIONS

No modification is to be made to any particular rule after the advance programme or racing instructions are posted, except if decided by the Race Committee and for cases of force majeure only. Such modification shall be communicated to each and every driver and crew person in writing at an official drivers meeting.

203 - ENTRIES

203.01 - RATIFICATION

The National Authorities must ratify in writing any entry for drivers wishing to race abroad.

Direct correspondence between the Organising Committee and foreign drivers is permitted, but no entry is to be accepted unless approved by the driver's National Authority.

The Organising Committee will keep blank entry forms at the disposal of the drivers.

An entry is not valid unless it reaches the Organising Committee within the time prescribed in the advance programme and the entry fee is paid.

Entry forms must include the names of all drivers, including ratified test drivers.

203.02 - ADMISSION

No National Authority, or Organising Committee, organising an International race has the right to refuse the entry of a foreign driver duly commissioned by his National Authority, provided said driver is not within a period of expulsion and he conforms to the UIM Offshore rules.

The closing date for entries should be at least two weeks before the event. Owners need only to supply such particulars as are applicable to the class for which their boat is entered.

203.03 - ENTRY FEE

Each Organising Committee fixes the amount of the entry fee for a race.

The National Authorities or organiser have the right to establish a rule whereby higher fees are imposed on late entries. No late entry may be accepted after the closing time of registration for an event.

203.04 - MINIMUM AGE

Each National Authority has the right to fix a minimum age for the drivers allowed to race on its waters. Such age restriction is to be shown in the advance programme.

203.05 - OWNERSHIP OF BOATS

All boats entitled to race must be owned by a person or a group of persons who are members of a club affiliated to a National Authority.

The nationality of a boat is the nationality of the owner. The nationality of the driver licence only must be taken into consideration in the classification.

A boat is allowed to race only under its own name. The owner and the driver may only race under their own name or the pseudonym written on the licence.

203.06 - PUBLICITY ON BOATS

Advertisements may be freely applied to the entire surface of the boat, but there must be a free distance of space of no less than 0.5 metre around the race number.

No owner or crew can be compelled to carry any advertising whatsoever on their person, clothes or upon the boat that is being raced and his/their refusal to do so cannot in any way be an obstacle to their participation in any race. If an entrant refuses to display sponsor advertising, this must be noted on his entry application.

Penalties may be applied to any driver or crew member who, in the course of any offshore racing event, has made any statement contrary to the truth or has distorted the facts.

Political publicity or immoral advertisements are not allowed.

Should restricted publicity or no publicity be allowed for an event, this must be announced in the advance programme.

Any National Authority has the right to fix its own rules regarding publicity for its own boats and venues.

203.07 - TEAM REPRESENTATIVE

At any titled event, each boat must designate its representative for use in case of an accident or force majeure.

204 - PROGRAMME / RACING INSTRUCTIONS

204.01

The Organising Committee must clearly state the following points in their programme / racing instructions :

- 204.01.1 The venue and the date(s) of the race(s).
- 204.01.2 The date and times of driver registration.
- 204.01.3 The dates and times for scrutineering.
- 204.01.4 The date(s) and time(s) for drivers briefing.
- 204.01.5 Exact, complete description of the race course(s), including all turning marks, known hazards and any designated spectator areas.

- 204.01.6 Starting area and its marks.
- 204.01.7 Finishing area and its marks.
- 204.01.8 Starting time of the race(s), time limits for late starters and finishers.
- 204.01.9 Time when the control closes for each race (sign off time).
- 204.01.10 Signals of race cancellation, postponement, recommencement of a race, curtailment of a race, or stopping a race.
- 204.01.11 When helmets and racing vests must be worn.
- 204.01.12 The choice and efficiency of a racing vest is the sole responsibility of the wearer.
- 204.01.13 The wearer is entirely responsible for the choice of his helmet.
- 204.01.14 Special harbour conditions and local laws, if any.
- 204.01.15 Full particulars of prizes for each race.
- 204.01.16 Time and place of the prize giving.
- 204.01.17 Time limit and place for the handing in of protests.
- 204.01.18 Composition of the Race Committee and the Jury.

204.2 - DRIVERS MEETING

A drivers meeting must be held before the race.

It is mandatory for drivers to attend the drivers meeting(s).

At least one of the crew, or as described in the Race Instructions, shall attend the briefing(s), otherwise the crew shall not be allowed to start.

It is the drivers' responsibility to read the Race Instructions and have full knowledge of its content before attending the drivers meeting(s).

Any infringement to the rules specified in the Race Instructions may incur a penalty or disqualification.

The drivers briefing shall take place at least two hours (except in cases of force majeure, one hour) before the start of the race and shall be recorded on audio or video.

The briefing must cover :

- * name call or sign in
- * important items not mentioned in, or changes from Race Instructions which must be in writing
- * important local conditions and rules
- * special safety routines
- * weather forecast
- * telephone number of Race Secretary.

The members of the Jury and the Commissioner(s) should attend.

205 - SAFETY RULES

205.01 - NATIONAL AUTHORITY

Each National Authority must submit all drivers under its jurisdiction to an annual medical examination and where applicable, an immersion test annually.

Each National Authority must adopt a form to be used for the medical examination to which every driver who competes Internationally must be submitted every year.

Each National Authority must submit to a special examination any handicapped driver, any driver who has had a serious accident, or any driver who during two or three races has shown odd behaviour when driving his boat. This check is compulsory before participation in an International race and must be certified by a commission appointed by the National Authority of the interested driver. The commission will be composed of a well known medical specialist and two other experienced offshore racing administrators who are members of the National Authority. The commission may give or refuse authorisation as to the eligibility of the driver(s).

Each National Authority which organises an International race must abide by any recommendations of Cominsafe regarding the medical organisation.

205.02 - MEDICAL EXAMINATION

The medical Officer who is to be present at any race has the right to submit any driver entered to a physical or psychiatric examination, or an alcohol and/or drug test. These examinations/tests may be performed at any time during the event.

The result of such examination/test is immediately communicated to the Race Committee, who basing themselves on the report of the medical officer, may suspend any driver from the event.

205.02.01 - CONSUMPTION OF ALCOHOL

Competitors shall not be under the influence of alcohol while competing or practising.

A competitor shall, if requested by an authorised officer, submit to an alcohol test. Written notification of selection for testing will be handed to a competitor by an authorised official. Refusal or failure to do so may be taken as if a positive test had been ordered and dealt with accordingly.

Testing when carried out shall be using an Alcoholmeter.

For International Races, any competitor found to have more than BAC 0,10 0/00 (0,10 g/L blood // AAC 0,020 mg/L air) 1 hour before and during testing or race shall be immediately suspended and disqualified from the whole event. A format report shall be forward to the N.A. of the competitor.

If in an organising country, the legal requirements of the above numbers are lower, then these numbers have to be announced in the advance regulations and prevail.

Any competitor found to have an alcohol level more than 50 % of that permitted by the Road Traffic Act governing domestic motoring regulations of the country which is organising the race shall be immediately suspended and disqualified from the whole event. A formal report shall be forwarded to the National Authority of the competitor.

If no limit is specified by the country organising the race in that country, then the level should be zero +. Note : This final figure of zero + is to be clarified by the UIM Medical Commission which is responsible to the UIM for these "specialist definitions".

205.02.02 - ANTI-DOPING REGULATIONS

Anti-doping regulations based upon the world anti-doping code.

As per separate publication.

205.03 - MEDICAL EVALUATION SHEET

Location of test _____ Date _____

Name _____ Surname _____
 Born in _____ Date _____
 Address _____
 Status _____ Profession _____
 Date of first involvement in the sport _____
 Other sports practised _____

PRESENT STATE OF HEALTH AND FORMER DISEASES

Disease (former or current) _____

With special mention of _____

Cardiopathy Coronary Diabetes Type 1 / 2 (circle the type)

Epilepsy High blood pressure

Operations undergone previously _____

Dates of these operations _____

Accidents (when, where, under which circumstances) _____

Alcohol g/day _____ Tobacco pack/year _____

Allergies _____

Medicines usually/frequently taken _____

Please keep in mind the Anti Doping rules and the WADA – UIM list of banned substances and Therapeutic Use Exemption rules.

205.04 - GENERAL OBJECTIVE EXAMINATION

Height m _____ Weight kg _____

Body Mass Index (Weight (kg)/Height²(m) BMI): _____

No superlicence will be issued to a driver with a BMI over 30 kg/m²

These anthropometric data can be checked during competitions by the UIM Medical and/or AntiDoping Delegate who has the right to withdraw Superlicence from non compliant pilots.

Age _____ Sex _____

Congenital or acquired deformities _____

Respiratory system _____

Spirometry (mandatory) Please attach printed results and graphs _____

FVC (measured/predicted) _____

FEV₁ (measured/predicted) _____

Cardiovascular system : is there any evidence of abnormality of the heart or cardiovascular system ? o Yes o No

If "Yes", give details below.

Blood pressure at rest _____

Heart frequency at rest _____

Electrocardiogram at rest _____

Stress ECG and blood pressure profile (Mandatory, please attach printed results and graphs and final report in English language

Ophthalmologic check-up (mandatory every other year)

Have you undergone this ophthalmologic check-up last year ?

If yes, please specify when : .. / .. /

If not, ask your ophthalmologist to fill the hereafter items

Faculty of vision (uncorrected) R _____ L _____

(with correction) R _____ L _____

Vision with both eyes open (wearing corrective lenses if necessary)

Field of vision _____

Vision of colours: is the applicant's colour vision normal?

o Yes o No

If "No", give details below.

Signature and practice stamp of the ophthalmologist

Hearing testing _____

Urine: Albumine _____ Sugar _____

Is the Urine analysis normal? o Yes o No

If "No", give details below.

Nervous system _____

Others _____

Does the applicant have any physical abnormality or restriction of movements in the arms or legs? o Yes o No

If "Yes", give details below.

Is there any evidence of a physical or mental condition (past or present) which could, in your opinion, prevent the applicant from holding a competition licence for motorsport ?

o Yes o No

If "Yes", give details below.

To the Doctor and the Driver :

Are you the applicant's usual doctor? o Yes o No

Your practice stamp (together with your name and qualifications):

Name in capital letters _____

Degree _____

Address _____

Tel _____

Fax _____

Aptitude assessment _____

Date _____

This is to certify that I have examined the applicant in line with this form.

Signature of the Doctor _____

Signature of the Driver _____

Maximal stress ECG and blood pressure profile: electrocardiogram have to be performed:

* on a cycloergometer starting at 60 or 70 watts and increasing by 30 or 35 watts every two minutes.

The driver must be capable of reaching a minimum power level of 2.5 watts (males) – 2.2 watts (females) per kilogram of body weight.

* or on a treadmill, using a Bruce protocol (preliminary warm-up recommended)

The driver must be capable to complete at least the entire stage 3 (i.e. 3 minutes at 5.5 km/h (3.4 mph) and 14% gradient).

This stress ECG aims to confirm both cardiovascular health and that the driver reaches the minimal fitness level to compete in motonautic sports. Therefore, we strongly encourage drivers to undergo regular physical training and to perform a maximal stress ECG test.

A written report from the physician (English language) must be enclosed.

Exhaustive ophthalmological check-up, tested and certified (English language) by a professional ophthalmologist to be renewed every two years and after skull trauma.

General Recommendation for Class 1 and XCat superlicence holders concerning physical fitness

Cardiovascular training (endurance training) as jogging, cycling, rowing etc. twice a week - minimum 20 minutes without any interruption. Any sports activity which has “stand still” during the activity such as soccer, tennis or any ball sport is not suitable for the endurance training, but recommended as additional training.

Training to build up muscles for neck and back at least twice a week.

205.05 - INJURED DRIVERS

If a driver has an accident needing medical care, he must immediately inform his National Authority.

It is the responsibility of his N.A. to have the driver examined and verified recovered before the driver is allowed to compete in an International race.

If his N.A. does not manage to verify this (lack of time etc.), the N.A. must notify the organiser and the driver must be examined by the Medical Officer of that race.

205.06 - RACING VEST

Racing vests (for boats without canopies). Each person aboard a boat must wear, during the race and practise, an efficient racing vest.

The racing vest shall be of full jacket type, with collar and leg straps. All straps shall be at least 40 mm wide and have a minimum breaking strain of 500 kg.

The vest shall be at least 70 % orange or yellow, have lifting straps at the front or on the shoulders and have impact protection material covering the back.

The disposition of the buoyancy must be such as to ensure that an unconscious person will float face up in the water. The racing vest must have a minimum of 7.5 kg buoyancy.

The choice and efficiency of a racing vest is the sole responsibility of the wearer. Race organisers are required to repeat this important clause in the Advance Programme and the Race instructions.

For boats with canopies, refer to rule 508.25.

205.07 - PROTECTING HELMET

Any person aboard any boat partaking in races must wear an adequate helmet of orange colour throughout the race and during practise runs. Helmets may only be removed when effecting repairs and the boat is "off plane".

Each crew member's helmet must be identified with their boat race number. This number shall be placed on the top surface of the helmet and shall be read from the rear. The numbers shall have a minimum height of 7.5 cm and shall be of the same style as the boat race number. These numbers shall be painted in waterproof black paint or be made of black coloured self adhesive material of adequate strength.

The wearer is entirely responsible for the choice of his helmet. The organisers are requested to repeat this important rule in their particular rules, as well as in their Advance Programmes and Race Instructions.

Each National Authority may require that their nationals wear a helmet of a type laid down in their own National rules.

The use of neck collars is strongly recommended.

205.08 - SAFETY BOATS

Safety Boats in sufficient number must be standing by during official practise and for the duration of all races.

Each boat must carry :

- * a minimum of two persons who can enter the water and carry out rescue procedure
- * necessary signal flags
- * a large capacity fire extinguisher suitable for fuel fires
- * ropes suitable for towing and a boat hook.

All safety boats must have radio communication with race headquarters.

A first aid station with qualified medical attendants and an ambulance must be stationed at a central on-shore location where injured drivers can be easily transferred from a safety boat to shore. It is recommended to have a medical doctor present, preferably qualified in handling trauma type accidents.

It must be clear before the race, who has the medical responsibility during the race.

205.09 - INSURANCE

All drivers must be properly insured, either by themselves, with the policy to be produced before the start, or by any other insurance agency as required by the National Authority, the fee being paid by the driver.

The following insurance requirements are to be included in the Advance Programme :

- * Terms and conditions of an insurance for personal and material damage to third parties, and to other competitors if available.

- * The minimum limit of insurance cover required.
- * The type of risks to be covered by the insurance.
- * Insurances from other National Authorities are acceptable and must be written in the English language as well as in the language of the country where the boat is registered.
- * The competitors must have a racing third party insurance. The maximum amount of cover must not be inferior to that required by the laws of the countries where the races will be held.
- * If a team wish to use their own insurance, a copy of their policy must be sent to the relevant National Authorities at least 28 days before the race, so that the National Authority can consider whether the cover is acceptable. If the cover is acceptable, the local insurance will not be required to be taken out.
- * The amount of insurance fees to be paid at the race site if appropriate.
- * All insurances must cover the whole period of the practise testing and races from beginning to end of the event.

205.10 - TECHNICAL PROTECTION

It is forbidden to start a motor with the propeller rotating in the air.

After launching a boat, it is forbidden to start the motor(s) or the engine(s) with the boat elevated and the propeller(s) rotating.

205.11 - PROHIBITED SMOKING

All smoking of any type is prohibited in pit areas. Adequate signs will be posted to this effect and the rule will be rigorously applied by the pit officials.

205.12 - USE OF SAFETY EQUIPMENT

Whilst on the water at an event, all the relevant safety equipment, i.e., restraints, racing vests, helmets and racing suits must be worn whilst the race boat is on the plane.

205.13 - PROTECTIVE CLOTHING

All crew members whilst racing must wear suitable protective clothing that covers the torso and all limbs to the wrists and ankles.

Protective clothing used must be durable enough to provide bodily protection and it is recommended to be fire retardant.

205.14 - FUELLING

Any team who dispenses fuel, either in or out of the boat, other than in the designated fuelling area, or by prior approval of the Pit Officials, will be subject to a penalty as decided by the Race Committee.

206 - ASSISTANCE IN ACCIDENTS

It is mandatory for competing boats to stop and the drivers to provide assistance at the scene of an accident involving other competing boats, if no other suitably qualified rescue assistance is present.

Placing and championship points shall be awarded to any boat/driver that stops at the accident scene and provides assistance. Such placing shall be related to the last confirmed position it held prior to the time of stopping at the accident scene after deleting all non-finishers from the placing list.

However, except in extreme circumstances as decided by the Race Committee, the boat/driver that gives assistance must, when relieved of their duties by suitable rescue personnel, continue in the race until the completion.

Providing such competitor does finish at the completion of the race, at racing speed, the 70 % rule shall not be applied to that boat/driver.

Should such boat/driver improve its position, it will keep its better place.

Any prize money must be paid for the position awarded.

Penalties imposed by the ODD or the UIM Commissioner for reason of failing to stop in an accident or dangerous driving must be imposed as soon as possible on receipt of the evidence of the offence. In a multi-heat championship this must be before the next heat.

The deadlines in 406.02 do not apply to this rule.

206.02 - HAND SIGNALS

The following hand signals are to be used by drivers and/or crew members at an accident scene to advise rescue personnel of their requirements.

HANDS CLASPED OVER HEAD

OK - No help needed at this time

THUMBS - UP

OK

BOTH HANDS WAVED OVER HEAD

Need immediate medical assistance

ONE HAND WAVED OVER HEAD

Need immediate tow or fire extinguishing assistance

NO SIGNAL

Immediate response by nearest personnel

ONE HAND RAISED WITH CLENCHED FIST WHILE ON PLANE

Caution - Debris or accident ahead !



Slowing or avoidance maneuver to follow.

All drivers, before being issued with their annual licence, must be tested on these signals.

207 - DISCLAIMER

By participating in UIM governed races any participant (driver, team member, spectator, race official or other involved person) acknowledges the following:

UIM is the governing rule making body of UIM racing events which are responsibly organized by clubs, national authorities of UIM, contracted promoters or other organizers. These organizers do not act on behalf of the UIM and UIM is not responsible for any act or omission of such organizer and shall not be liable to the participant accordingly.

The UIM sport and technical rules are intended to minimize risks but they cannot provide the highest possible safety standards at all times. Residual risks might remain.

Enforcement of the rules by UIM or other race officials and in particular but not limited to the technical scrutineering does not guarantee the safety of racing or

the safety of the scrutineered boat. Scrutineering is not intended as a construction/condition survey. The racing license issued by the National Authority or a super license issued by the UIM does not guarantee that a driver is physically able to race safely nor that he has sufficient experience or education.

Teams and drivers are solely responsible for their own safety including but not limited to their physical and educational ability to race in the relevant class, the safety of their boats and other gear and the safety of their racing activity. This responsibility includes racing with prudence and taking technical measures which are not mandatory in the rules but deemed necessary.

UIM shall not be liable for any damage, injury or death due to inadequate rules, breach of existing rules by participants or failure to enforce rules by the race officials.

208 - RESERVED NUMBER

209 - CRANE AREA

When race boats are being craned in and out of the water prior to, during of after a race meeting, the boat shall be craned without any person on board. The arrangements for craning shall be so organised that on no account will the craned boats pass over the head of any person.

This area must be cordoned off and marshalled.

300 - RACING RULES

300.01 - DEFINITIONS

French	English	Italian	German
Course	Race	Gara	Rennen
Manche	Heat	Prove	Lauf
Tour	Lap	Giro	Runde
Circuit	Circuit	Circuito	Rundestrecke
Parcours	Dist.covered	Percorso	Rennstrecke
Base	Base	Base	Messstrecke

Circuit

A circuit is a closed course defined by buoys where races are to be held.

Distance covered

Distance covered is the total distance to be covered per heat or race.

Base

A base is a course expressly measured for record attempts.

Marks of the course

Any objects specially indicated as such in the race circular, are to be considered as marks of the course.

Obstacle

Any non specified object is considered as being an obstacle to free navigation.

Race

A race is a competition in speed between boats held in one or more heats.

301 - TIMING

In all International races and national championships the timing is carried out by officially appointed timekeepers using certified timing devices graduated to one tenth of a second. The devices must have a split second system one part of which must be capable of being stopped and restarted.

303 - MARKS OF THE COURSE

303.01 - DEFINITIONS

The Marks of the Course are any objects (boat, buoys...) that are specifically designated as such in the racing instructions. The objects that are not designated specifically as such are to be considered as obstacles.

An Official is posted near the turning marks. He must observe the turning of the marks and report to the Race Committee.

Should any mark be removed from its proper position either by accident or otherwise, the Race Committee shall, if possible, have it replaced. Should it be impossible to replace the mark in time for the boats to round it, the Race Committee shall decide whether the race is to be restarted or not.

The distance between the starting line and the first turning buoy must be at least one nautical mile.

303.02 - SPECTATOR AREAS

Race Organisers should designate spectator areas.

If a race boat finds itself in a designated spectator area, it must come off the plane immediately and proceed "off plane" with caution until back on the race course to avoid disqualification.

305 - PROCEDURE FOR UIM OFFSHORE STARTS

305.01

A safe area shall be established for milling. In this area, boats must be off plane (bow down, no wake) and must turn in counter clockwise direction.

305.02

Distinct areas in which each starting group can mill (safe area) will be established. See diagram for area and location.

305.03

A control line on the race side of the milling area which is effectively where the start boat turns to pick up each class to start will be established.

305.04

The referee - wearing orange gloves and an orange vest (optional) over his life jacket on the start boat may use hand signals to control the fleet prior to the start.

305.05

The use of a starter boat is mandatory for all classes. There must be a safe interval between each class start with the start boat headed towards the start line to one side or the other of the chute.

305.06

The minimum width of starting chute is 280 metres with a minimum length of 1600 metres. The chute must be straight.

Drivers must keep their lane in the start chute, up to a point presented in Race Instructions. From there on only "The rules of the road", § 313, are valid.

Breaking this rule will result in one or more of the following penalties: warning, one lap penalty, disqualification, yellow cards, depending on the severity.

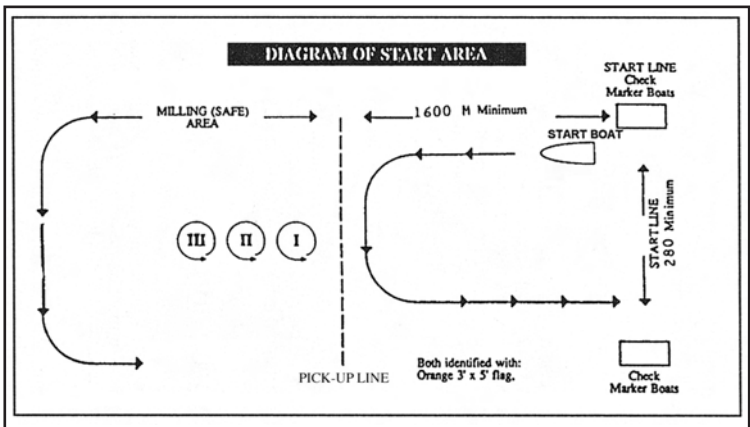
305.07

A single white smoking or rocket flare will be fired 2 minutes before the beginning of the starting run-up for the first start class, unless otherwise stated in the race instructions.

305.08

Two minutes following the firing of the start signal flare, the start boat shall raise and continue to display a yellow flag whilst running across the “pick-up” line. When the start boat reaches the opposite side of the starting shute it shall turn and begin it’s run towards the start line. All race boats may then begin their run towards the start line, following the start boat, at the 30 metre minimum safety distance, until the raising of the green flag.

The minimum dimensions recommended for the yellow flag are 1.5 m x 1.0 m.



305.09

During the start procedure the race boats must run at the safety distance of at least 30 metres behind the start boat and must observe a safe spreading distance between boats.

305.10

The start boat should approach the start line at a speed which allows boats which have difficulty in getting on plane to catch up.

305.11

When the starter is satisfied that all boats are in a satisfactory line, at a satisfactory speed, he raises the green flag and lowers the yellow flag simultaneously. The raising of the green flag determines the actual start.

The minimum dimensions recommended for the green flag are 1.5 m x 1.0 m.

305.12

The race only starts when the green flag is raised. However, for time keepers, the official start is when the leading boat crosses the actual start line. The actual start line shall be announced in the racing instructions.

One, or two start line/check marker boats may be used in conjunction with other fixed or laid marks help define the actual start line.

305.13

If at any time during the start procedure after the white flare has been fired, a red flag is raised, the start has been aborted and all boats must return off plane to the milling area for restart or follow specific instructions of the start boat.

305.14

A disabled boat or one which cannot get on plane with its class, must not interfere with the start of the next class in any manner. The craft must go to one side of the chute and wait for a clear lane to commence their start. Should a boat fail to start after its respective class has started, it may start, in a normal fashion within a half hour (30 minutes) of its respective class time of start.

305.15

It is recommended that the start and/or pace boat personnel be limited to seven persons.

305.16

Penalties for infraction of the starting procedure are:

- a) Failing to respect a safety distance of 30 metres (305.05) : 3 minutes;

- b) Turning wrong direction (305.01) : 3 minutes;
- c) Interference with starting procedure : disqualification;
- d) Excess speed in the milling area, or faster than 10 knots : 10 minutes penalty. If the driver has caused danger to other boats, he may be disqualified;
- e) Should the bow of any boat be in front of the transom of the start boat when the green flag is raised : 10 minute penalty.

An outer-limit pace boat on the other end of the penalty line may be used to help define the actual penalty line from transom to transom.

305.17

Safety takes precedence over racing. Pilots should do nothing to endanger crew, spectators or other participants.

305.18

If, through lack of entries, it is desirable to start two or more starting groups together, this may be done provided it is fully covered in the racing instructions (and/or drivers briefing).

305.19

It is recommended that the same start boat starts each starting group and in which case the start boat, having started the first group should return on the diagonal to its original position at the opposite end on the control line and immediately repeat the starting procedure outlined in 305.08 for the next starting group until all the groups have been started. Alternatively, if separate start boats are used for secondary starting groups they should utilise the procedure described in Rule 305.08 (or 305.18) at time intervals fully described in racing instructions.

305.20

Should sea conditions in the milling area be such that boats ship water when turning while off plane, procedure will be modified as follows:

The start boat will lead race boats to the milling area at which point it will raise yellow flag slowly to allow race boats to get on the plane in safety before accelerating towards the start line. Race boats will follow the start boat at the thirty

metres minimum safety distance, on the side and in the manner described in the racing instructions or at race briefing, until the raising of the green flag.

306 - QUALIFICATION

A timed qualification session may be held the day before the race. The qualification course must be from 10 to 15 miles, with at least one turn. There must be 2 minutes of interval between each timed running start.

The Race Committee or the Officer of the Day can decide to split the start of one class in case of more than 10 participants. Results of the qualification session will determine the starting grid. The second flight shall start no more than one minute behind the first start.

309 - RACE CANCELLED

The Race Committee has the power to cancel any race should unfavourable weather or other serious circumstances render such action necessary. A red flag raised indicates that such race has been cancelled.

310 - RACE POSTPONED

310.01

The Race Committee has the right to postpone a race, in which case the red flag must be raised from the start boat and/or a patrol boat. Race starts thus postponed may re-commence the starting procedure with the raising of the yellow flag as described in 305.08.

The Race Committee may postpone the start, even after the first signals have been given, but only in cases of force majeure or if an error in the starting signals has been made.

310.02

In both cases, signalling is to be recommenced. When the need for postponement has passed, the Race Committee signals the start according to the instructions contained in the programme.

New entries are not to be accepted for postponed races.

Only drivers who duly entered and were present when the race was postponed are admitted.

311 - RACE SHORTENED

A shortened race is a race over a course which has been shortened by the Race Committee before the start.

The decision with instructions about the shortened course must be handed in writing to the drivers and the timekeepers.

312 - RACE STOPPED

A red flag raised indicates the race has been stopped.

A stopped race is a race which has been interrupted by the Race Committee after the start.

Stopping the race is decided by the Race Committee for reason of force majeure of which it is the sole judge.

The Race Committee classes the boats according to their position at the time or place when and where the race was stopped.

312.01 - RACE CURTAILED

A curtailed race is a race shortened by the Race Committee after the start.

The signal to be used to announce it, is the waving of the chequered flag at the finish line.

The O.O.D. or other responsible official can implement a curtailment decision at any time during the event, even in a multi-class race, provided the curtailment signal is broken out on the start/finish line. Once the curtailment signal has been broken out, all boats will stop racing at the end of their current lap.

Raising a retirement flag will not preclude a driver from getting finishing points that would otherwise be awarded if the race is curtailed.

Arriving at a finishing sequence - The first boat in each class, or subdivision, to be stopped by the curtailment signal becomes the "key" boat in that class. Then -

- I) All boats that subsequently take the curtailment signal and have then completed the same number of laps as the “key” boat are listed, in their finishing sequence behind the “key” boat.
- II) All boats that subsequently take the curtailment signal and have completed one less lap than their “key” boat are listed, in their finishing sequence, behind all the boats listed under (I) above.
- III) Boats that break down on their last lap shall be placed in the finishing order, in the sequence which they last crossed the start/finish line and in accordance with the number of laps which they actually completed.
- IV) All boats that subsequently take the curtailment signal and have completed one more lap than their “key” boat are listed, in their final finishing sequence, ahead of all boats listed under (I) and (II) above, and probably ahead of all boats listed under (III).
- V) Boats that break down on their last lap having completed less laps than any other finisher (under the above rules) shall be regarded as non-finishers.

313 - RULES OF THE ROAD

313.01

The International regulations for the prevention of collisions at sea, shall apply at all times.

313.02

Where by any of these rules one of the two boats is to give way, the other must keep her course and speed.

313.03

Every boat, which is directed by these rules to give way to another boat must, if the circumstance of the case admit, avoid crossing ahead of the other.

313.04

Every boat which is directed by these rules to give way to another boat must on approaching her, if necessary, slacken her speed, or stop or reverse.

313.05

Any boat overtaking any other must give way to the overtaken boat.

313.06

In obeying these rules, due regard must be made to all dangers of navigation and collision, and to any special circumstances which may render a departure from the above rules necessary in order to avoid immediate danger.

313.07 - OVERLAP

An overlap may only be established when two boats are on the same course or approximately on the same course and the overtaking boat has no longer a free choice on which side he shall pass.

The inside boat may gain the right of way by establishing an overlap even after the outside boat has altered her helm for the purpose of rounding the mark.

Once an overlap is established during the approach to or the rounding of a mark it cannot be broken even if the inside boat is clear astern.

An overlap exists when one boat is not clear astern of another.

A boat is clear astern of another when her hull is abaft a line projected abeam from the aftermost part of the other.

313.08 - PASSING OBSTACLES

Should an overlap exist between two or more boats when they are about to pass an obstacle, then the outside boats shall give the inside boats room to pass clear of the obstacle.

313.09 - PASSING MARKS

Should an overlap exist between two or more boats when they are about to pass a mark on the required side, then the outside boats shall give the inside boats room to pass clear of the mark, leaving it on the required side.

313.10 - ALTERING COURSE

When one of the boats is obliged to keep clear to avoid risk of fouling, the other shall not alter her course.

A boat must not alter her course so as to hinder another in passing to starboard.

313.11

When an overlap exists but neither boat is an overtaking boat, the one which has the other on her starboard side must give way.

314 - ROUNDING AND FOULING MARKS

314.01

Rounding marker buoys must be in the direction and on the side stipulated in the race instructions.

314.02

A driver who is forced on to a mark by another driver may lodge a protest against that driver.

315 - FOULING COMPETING BOATS

If a boat, in consequence of her neglect of any of these rules fouls another boat, or compels others to foul each other, she is disqualified.

316 - RUNNING AGROUND

Any boat, running aground, fouling a buoy, vessel or other obstruction, may use her own anchors, warps, spars or other gear to clear herself.

317 - ANCHORING DURING A RACE

A boat may anchor during a race but must weigh and recover her anchor again, not slip it.

318 - MEANS OF PROPULSION

All methods of propulsion other than motor, and paddle are prohibited.

The action of the wind and current will not entail the disqualification of the competitor.

319 - OUTSIDE ASSISTANCE

319.01

Unless specified in racing instructions, no outside assistance (including refuelling) is allowed during a race.

For all classes it is permitted to use electronic steering compasses and/or electronic position fixing equipment. It is expressly forbidden to interface any of this equipment to the steering system.

GYROS : All movable surfaces allowed on the boat shall be under the control of the onboard crew without any sensors in the control loop (i.e. no auto pilots, gyros, accelerometers or other attitude or acceleration sensors may be in the control loop).

The Race Committee has the right to decide whether there was outside assistance or not.

No changes to the boats systems may be made from outside races (i.e. no radio command links to adjust onboard parameters to engine management systems, either air/fuel ECUs or ignition ECUs).

320 - FINISHING A RACE

320.01 - THE FINISH

The finish of a race is signalled to the driver of the winning boat by waving a black and white flag at the finish line.

Where two or more classes are running together, a different flag may be used for each class.

The description of the flag used for signalling the finish of each class must be clearly specified in the Racing Instructions.

When the leading boat completes the course and crosses the finishing line, the chequered flag will be waved. It designates the “winner” of the race and the start of the finish procedure of the race.

Any boat crossing the finishing line after the “winning boat” will be classified considering its time and the number of laps completed, provided that it has covered the minimum number of laps announced at the race briefing.

When calculating the number of laps completed by a boat, its last lap completed (after the arrival of the winning boat) must not be slower than 50 % of its fastest average lap speed.

The race may be curtailed or stopped at the finish line at any time by waving the chequered flag. This designates the end of the race. To be designated a “finisher”, a minimum of 70% of the laps completed by the winning boat must be completed, rounded up or down to the nearest full lap unless otherwise stated in the race instructions.

320.02 - STOPPING THE RACE

In case of force majeure or accident, the Race Committee may stop the whole race, waving a red flag. This signal will be given from the turn mark control boats. Additionally this signal may be given by other official safety boats.

320.03

A driver who finishes a race or heat must withdraw from the course without hindering the boats that are still in the race. This constitutes an exception to rule “Right of Way”.

A driver who has finished that race and cuts across the course may be disqualified.

A vessel is timed as completing a race when her bow crosses the finishing line. After finishing the race, she must continue to observe any special regulations prescribed by the Race Committee as to keeping clear of the finishing line and the course.

320.04

The Race Committee has the right to stipulate the maximum duration of each race.

321 - SCORING SYSTEM

321.01

Unless otherwise specified, the following scoring system is to be adopted:

1 - 400 pts	11 - 22 pts
2 - 300 pts	12 - 17 pts
3 - 225 pts	13 - 13 pts
4 - 169 pts	14 - 9 pts
5 - 127 pts	15 - 7 pts
6 - 95 pts	16 - 5 pts
7 - 71 pts	17 - 4 pts
8 - 53 pts	18 - 3 pts
9 - 40 pts	19 - 2 pts
10 - 30 pts	20 - 1 pt

321.03

The official results must be posted on the notice board within an hour of the end of the races.

321.05

National class boats may race at International meetings.

321.06

If boats of different series or classes start together every boat must be classified within its own class.

323 - POSTING OF THE RESULTS

The results of each race must be posted in a clearly visible position, specified during the driver's meeting.

The officer in charge must sign the results and state the time of posting.

324 - INSPECTION OF HULLS AND ENGINES

324.01

Boats, and engines taking part in a race which are subject to checking, must not leave the boat park until one hour after the posting of the results.

324.02

The Technical Officers may postpone the inspection and stipulate the time limit and place where the engines must be available.

324.03

In the meantime, the motor must remain sealed to prevent any alterations being made.

Refusing to produce the motor or the hull entails disqualification.

324.04

It is compulsory for a driver to make arrangements for tools, special tools and mechanics so that his motor can be disassembled if the Technical Officers at a post race inspection want to check any part of the motor. Both the driver and/or the mechanics shall work as efficiently as possible so that inspection duration can be kept to a minimum.

Motors and boats will be placed pending inspection in a well-delimited place, which remains under the care of the “ boat park “ Official. This area is called the “ park fermé “.

324.05

After the pre-race technical inspection, the boats must not leave the boat park without permission except for practising or racing.

324.06

If any repairs can only be effected outside the boat park, they must be carried out in the presence or with the permission of a Technical Officer.

324.07

Officers allowed to attend the inspection:

- The Officer of the Day.
- The delegate of the interested National Authority
- The measurers named by the organising committee
- The driver and a mechanic if necessary
- The UIM Commissioner

The presence of other persons is only allowed if authorised by the Race Committee.

326 - AFTER THE EVENT (REPORTS)

326.01

At the end of the event, the Secretary of the Race Committee must send to the Offshore Commission of his National Authority a report including:

- 326.011 Classification and distribution of prizes (minutes of the jury)
- 326.012 Report of the timekeepers
- 326.013 List of drivers

- 326.014 Penalties
- 326.015 Documents on any records beaten in conformity with the UIM rules
- 326.016 List of protests and decisions relating to these protests.
- 326.017 All necessary documents in order to evaluate the event or the race.
- 326.018 These documents must be sent to the Offshore Commission of the National Authority within 8 days of the end of the event or the race.
- 326.019 After each titled event the organisers must send the results to the Secretariat of the UIM.
- 326.020 If the results are taken into consideration for the granting of a trophy or a prize, the points must be sent to the UIM by telefax within 48 hours.
- 326.021 days after a competition, the organising club must send the final result to the National Authorities of all the participants and to the Secretariat of the Union.

330 - SPRINT POWER SLALOM

The course is a simple slalom course with only two turns.

The race is with two boats against each other on two separated but identical sprint courses.

The race shall be run with a serial produced boat. All pilots shall run with identical boats.

The competition is a elimination competition. If you will, you may continue, if you loose - you are out.

The race could be either one single race, or if time permits you could do it twice, then the drivers should exchange course and/or boat.

The course area is at least about 120 m x 60 m. The length and size of the course is pending on the speed and performance of the boats.

The course is designed so that the boats always turn outward from each other.

330.01 - COMPETITORS AND SAFETY

All competitors must go through a driver education.

All competitors must have approved helmet and an approved model of the racing life jacket.

Kill switch is mandatory, which stops the engine if someone falls out of the boat.

One should always be at least two persons in the boat.

Crew members must have a license issued by the National Authority, with including accident and liability insurance in force during the racing.

330.02 - START

We need a berthed start / finish boat with judges and with flag signals.

The first part of the track has a startbox area of about 20 x 30 meters.

When a yellow flag is shown from the jury boat, the boats shall go into the startbox area in the idle speed.

The start signal is shown by a green flag.

When it's about 10 seconds to start the green flag will be shown in horizontal position. When about 5 seconds remaining the green flag is raised straight up. When the GREEN flag is folded the start goes.

Unless otherwise stated, there will be a start exactly every five minutes.

If something happens during the race, which means that you cannot start after five minutes, the next start will be delayed five minutes. Red flag = wait outside the start box. Yellow flag enter the start box area.

330.03 - FINISH

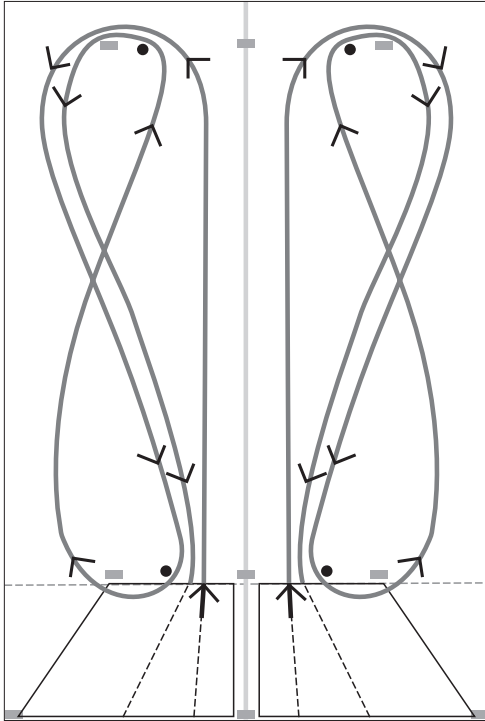
At the finish a black and white checkered flag is waved.

After finishing the boats run out of the start/finish box area in non-planning speed when both boats are out of the start/finish box area, a yellow flag is shown and the next two boats shall enter to the start area.

330.04 - GENERAL

The drivers do not own the boat in which they compete with you need two safety boats, one on each side of the track.

Racing Form follows UIMs ordinary racing rules. The racing format is new.



400 - JURISDICTION

401.01 - DEFINITIONS

Jurisdiction applies to :

The UIM Offshore, Circuit, Aquabike Rules.

401.02

No claim for damages arising from an infringement of any of the UIM rules or the Advance Notice or any of the Race Instructions or Race Bulletins shall be adjudicated upon by any Race Committee or appeal authority, but shall be subject to the jurisdiction of the courts.

The crew of a racing powerboat which acknowledges infringing a rule does not thereby admit liability for damages.

The findings of fact, and the decision of the protest committee shall be relevant only to the purposes of the powerboat racing rules and shall not be referred to in any proceedings for damage without the written consent of all parties to the protest.

402 - JUDICIAL BODIES

402.01 - THE INTERNATIONAL JURY

The Jury hears any protest raised by a driver.

1. Composition

The Jury must be composed of at least three members, the Chairman included. The Organisers of the event appoint the Chairman. Each competing country, including the hosting country, may appoint one member delegate who can represent only one country. These appointments must be made in writing, authorised and signed by a National Authority Official and delivered to the event Secretary.

If there are not enough N.A. appointed members to form a Jury of three persons, the Organisers must appoint additional members, but only enough to reach three persons.

The Jury composition will be announced at the first drivers meeting. The Jury must be present at the first drivers meeting.

Race Officials can not be Jury members.

The UIM Commissioner (s) appointed to the event is (are) full member (s) of the Jury.

In an event comprising races of several classes, it is possible to keep any authorized jury member to adjudicate on any matter of the jury, irrespective of the class he/she happens to represent.

2. Authority and Duties

The Jury sits as long as the event lasts, and hears all protests relating to that event.

The jury must decide as soon as possible on all protests presented to it during the event.

402.02 - INTERESTED PARTIES

No member of a Jury who has taken part as a competitor or Official in the race concerning which a judgement is to be given, or knows himself to have directly or indirectly an interest in one of the parties involved, may judge the protest.

However, a member of an International Jury shall not be disqualified even if one of the parties is from his N.A.

402.03 - THE UIM APPEALS BOARD

The UIM will decide on Appeals arising from International events according to the provisions laid out in Rule 405.1.

In each case, the UIM Executive Committee, or in case of an appeal lodged against an Executive Committee's decision, the appointed neutral Council member will nominate three members, one to be Chairman after consultation of the relevant commission. The members must not be from the National Authorities of the parties to the case.

One member will act as Clerk to the Board.

403 - PROTEST PROCEDURES

403.01 - VALIDITY OF PROTESTS

Only a driver may protest against facts pertaining only to the races in which he takes part.

Joint protests signed by several drivers will not be considered.

All protests must be in writing - type written or hand written in printed characters in English.

It must state the reason for the protest and be accompanied by any relevant documents that shall provide evidence within the given time and any protest fee as stipulated by the Race Organiser.

All protests must be signed by the protester.

The protest must be handed to the Race Secretariat who must, in the presence of the protester, record the time of receipt.

Any costs involved in verification are to be borne by the losing party.

A written protest can be withdrawn by the protester ; he then loses the protest fee.

403.02 - PROTEST FEE

The amount of the Protest fee must be decided by the National Authority and it must be published in the advance programme. The maximum amount for a Protest fee will be decided by the UIM General Assembly. It will be 80 € or an equivalent sum in the local currency.

The Jury must determine whether the protest presented to it, has been lodged according to the rules. If found to be invalid, the reasons must be given to the protester.

If a protest is upheld by the Jury, the fee must be returned to the protester.

403.03 - RIGHT OF PROTEST

A protest can be lodged against the posted provisional results or against any decision made by the Race Committee or against one or several competitors.

In the event of a competitor protesting against a penalty imposed on him by the O.O.D. or the UIM Commissioner and another driver protesting simultaneously

against this competitor due to the same incident, the Jury will have only one meeting with both competitors being party of this protest procedure.

A protest can only be lodged on a subject which directly concerns the protester himself. A protest which complies with these rules can not be refused by the Jury.

No protest is permitted on Jury decisions, they may only be contested by appeal.

403.04 - TIME OF LODGING A PROTEST/ PROTEST PERIOD

A protest regarding the qualification of a boat, a motor, or a driver must be made before the drivers briefing. Starting in a race is considered as acceptance of the conditions and the eligibility of the other competitors. The only exception to the above is when the protester can prove the facts were not given to him before the drivers briefing, then a protest can be lodged up to one hour after the posting of the results.

Any other protest must be lodged within one hour of the first results being posted with the following exceptions:

A protest of a driver (driver "A") against another driver (driver "B") can be lodged within 30 minutes after the posting of the protest form according to rule 404.03 if driver "B" has been penalised and himself protested against this penalty. Without lodging such a protest driver "A" shall not be entitled to appeal against the jury decision.

A protest against new results which are not caused by a jury decision can be lodged within one hour of these new results being posted, but only concerning the decision, which led to the changing of the results.

A protest against any other decision, posted after the posting of the first results can be lodged within one hour after the posting of this decision.

403.05 - JUDGEMENT

Decisions by the Jury shall be reached by simple majority of votes. In the case of equal votes, the Chairman's vote will be the casting vote.

In the event of a protest, penalties may be agreed, rejected or changed by the Jury.

Any judgement pronounced by the Jury stays in effect until altered by the Appeal Board.

403.06 - NOTIFICATION OF DECISION

All decisions by any Jury must be notified to the concerned parties in writing, including their right of appeal.

403.07 - AVAILABILITY OF DOCUMENTS

All documents relevant to any decision by the Jury must be filed with the organising N.A. and held available for a higher authority until the time limit for an appeal has expired.

404 - HEARING OF THE PROTEST

404.01 - RIGHT TO A HEARING

Any person being party to a protest, or being charged for an offence against the rules shall have the right to be heard before the Jury in order to defend himself.

The Jury must give notice to all parties of where and when the hearing will take place.

If any party duly notified, fails to appear without giving an acceptable reason, judgement can be rendered by default.

404.02 - WITNESSES AND EVIDENCE

It is the responsibility of the parties involved in a protest to ensure that witnesses appearing on their behalf together with any other evidence are present and the Chairman of the Race Jury at his/her sole discretion may take their availability into account when determining the time of the hearing.

Any costs incurred by the appearance of witnesses shall be borne by the respective parties unless decided otherwise by the Jury.

404.03 - THE HEARING PROCEDURE

The following procedure must be followed in all hearings unless otherwise stated elsewhere in these rules :

- A copy of the protest must be posted in the same way as the results. The time of posting must be noted on this copy.

- Written minutes must be taken.
- All parties to the case are entitled to be present and to hear and question all evidence at the hearing up to the time the Jury makes its decision. The protest, shall be read out to the parties.
- If the jury meeting involves a junior (less than 18 years of age) then he/she must be accompanied throughout the meeting by an adult (parent/guardian).
- The parties shall be invited to state their cases. The parties may call witnesses. Each witness, after having given his account of the case, may be questioned by all parties and by the members of the Jury.
- Any member of the Jury who is familiar with the case may give his evidence
- The parties shall be entitled to question each other and any member of the Jury who may have given evidence.
- The witnesses shall withdraw and the parties shall be invited to make a final statement of their cases.
- The Jury may recall any party, previous witness or new witness and call on other evidence to verify the facts.
- The parties must be present during the whole of the recall and must be given the opportunity to question any new evidence after which they may re-make their final statements.

405 - THE APPEALS PROCEDURE

With reference to the 405 appeals procedure, any time limits which end on a Saturday or Sunday shall end on the following Monday. If the Appellant misses any time limit the appeal will be rejected as void.

405.01 - APPEAL BOARD

The members of the Appeal Board may hold other sports functions and consist of a Chairman and two members.

The Appeal Board sits whenever the Chairman calls a meeting.

405.02 - INTERESTED PARTIES

No member of an Appeal Board who has taken part as a competitor or Official in the race concerning which a judgement is to be given, or knows himself to have directly or indirectly an interest in one of the parties involved, may judge the appeal.

405.03 - RIGHT OF APPEAL

A decision by a Jury may be appealed by the parties involved in the Jury meeting when they consider that an injustice has been made against them and/or fresh evidence has been made available that may alter the decision of the Jury.

A driver can also appeal :

- a penalty, imposed against him
- without the prior lodging of a protest, but only if this was not possible, because the race Jury has had dissolved itself before or within the protest period.

405.04 - NOTICE OF INTENTION TO APPEAL

The intention to appeal must be notified in writing to the Secretariat of the UIM by the interested party within four days of the day following the Jury decision or in the case of a decision against which a driver can appeal without the prior lodging of a protest within four days of the day following the notification of this decision to the NA of the driver or if earlier to the driver himself.

405.05 - TIME LIMIT

All appeal documents and fees must be received within ten days from the date when the notice to appeal was received by the UIM Secretariat.

405.06 - LODGING OF AN APPEAL

The appeal must be sent by telefax, letter or email and it must be signed by the appellent. The reasons for the appeal must be stated.

Address, telephone, email and/or fax should be clearly mentioned on the appeal, so that the UIM can send any correspondence directly to the driver with copy to relevant N.A.

405.07 - NOTIFICATION OF THE PARTIES OF THE APPEAL

The UIM Secretariat must within two days from receiving the appeal, notify by telefax or letter the other parties that an appeal has been lodged and that they may send a rejoinder.

Eventual rejoinders must be received within ten days from receiving this notification.

405.08 - TIME LIMIT FOR DECISIONS ON APPEALS

All appeals must be determined no later than forty-five days after the date when the appeal documents and fees were received by the UIM Secretariat.

405.09 - APPEAL BOARD DECISIONS

The Appeal Board takes its own decision based on every information available. The decision of the jury may be upheld, changed or not upheld. The decision of the Appeal Board is final.

405.10 - COSTS

The appeal fee is 2000 €. In addition the driver has to pay 3000 € to the UIM for possible costs of the appeal committee (meetings, telephone, fax, laboratory, experts or other costs of the appeal procedure). The appeal fee for a single championship/invitation race (not a Formula race, or a C1 race or similar) is 1000 €, in addition the driver has to pay 1000 €, to the UIM for possible costs of the appeal committee.

If during the procedure further costs arise, the Appellant has to deposit the missing amount accordingly within 14 days from notification.

The driver who appeals has to pay for the meeting costs anyway if the appeal is upheld or not. If the costs of the appeal committee is less than 3000 € then the difference will be paid back to the driver.

The appeal fee of 2000 € will be refunded if the appeal is upheld. If the original decision is changed by the appeal board (405.09), it can decide to refund a percentage of the appeal fee if appropriate.

If there is an infringement to rule 405.05, the Chairman of the Appeal Board can decide that the appeal is not considered valid since the expiry dates were not respected. In this case 50 % of the received appeal fee will be reimbursed to the appellant.

The driver may agree to forego an Appeal Committee meeting and for the appeal to be handled through telephone, email, and fax, if possible, in order to minimise the cost for the appeal.

406 - PENALTIES

406.01 - GENERAL

Any proven breach of applicable Rules may be penalised.

If the rules do not determine a special penalty for the relevant breach, the penalty to be given has to be proportional to the seriousness of the breach. The proportionality is at the discretion of the decision maker.

The UIM Executive Committee or the relevant National Authority shall only impose penalties, when they deem the relevant infringement to be so serious, that it has to be penalised although the deadlines for penalising or lodging a protest have already expired. After the expiry of the aforementioned deadlines, a penalty for an ordinary breach of the rules shall in general not be imposed.

The Race Jury or the Appeal Board following a protest or an appeal can (only) impose penalties, which can be imposed by the O.O.D.

406.02 - DEADLINES FOR PENALISING

The first posted results are provisional for one hour.

The O.O.D. and the UIM Commissioner can only impose Penalties until the expiry of this hour.

The National Authority and/or the UIM Executive Committee can only impose penalties within 3 months from the day, the infringement occurred.

These deadlines are not valid for penalties imposed by the O.O.D. for the reason of post race scrutineering and under the condition that this is stated on the results sheet.

In this case the results will remain provisional until one hour after the finalisation of the post race scrutineering.

The date and time of finalisation of the post race scrutineering must be recorded by the UIM Technical Commissioner or other technical scrutineer in charge.

If the post race scrutineering can not be finalised until the end of the event and the O.O.D. has already left the race site the penalty has to be imposed as soon as possible after the receipt of the scrutineering results by the O.O.D.

406.03 - UNACCEPTABLE BEHAVIOUR

In order to protect the interests of the sporting community, the following actions may also be penalised by the O.O.D., by the UIM Executive Committee or by the N.A. of the Licence Holder.

- any deliberate act taken to gain unfair advantage.
- any false act made or statement given with the intention of suppressing facts required for the proper conduct of the race.
- any attempt to bribe or the taking of a bribe.
- any abusive or unsportsmanlike behaviour.

406.04 - REPRIMAND

A reprimand is a notice of disapproval of an unacceptable action. It must be recorded by the Race Secretariat. A reprimand must be witnessed. A reprimand automatically constitutes warning that if the offence recurs, a heavier penalty will be given. A Reprimand is valid for 12 Months.

A reprimand can be given by the O.O.D., or the National Authority of the Licence Holder.

406.05 - YELLOW CARD

A yellow card may be given by the O.O.D. or UIM Commissioner for unacceptable behaviour on land or water.

A yellow card will be confirmed in writing, and will be posted with the results. The competitor will then have 60 minutes in which to protest.

A yellow card is valid for 12 months from the date of issue. A competitor having accumulated three yellow cards in National and or International events will be suspended from National/ International events for a period of two calendared racing months (from the date of receiving the third card) in the current or following season.

Yellow cards must be notified to the UIM and National Authority of the competitor concerned within three days.

The National Authority of licence issue will notify the UIM and competitor of the start and finish date of suspension.

All yellow cards including reason for the card being given must be mentioned in the UIM commissioner's report.

To be applied to all people in the paddock:

A blue card can be given by the UIM commissioner for any offensive, unsportsman, abrasive etc, behavior during any official activity (briefing, race control during event etc.)

A second card will immediately remove the offender from that activity.

Following the subject activity, the Race Jury will meet and determine if further disciplinary action is justified.

406.06 - DISQUALIFICATION

Disqualification deletes a competitor from the results of the heat or race where the offence occurred.

Disqualification is done by the O.O.D.

406.07 - TEMPORARY SUSPENSION

Temporary suspension suspends a competitor, a competitor together with crew, a crew member, or an Official from all or part of an event. Temporary suspension may be given for serious indiscipline.

Temporary suspension will be imposed by the O.O.D.

406.08 - PROLONGED SUSPENSION

Prolonged suspension can be imposed on a competitor, an Official or an organisation for deliberate fraud, repeated or very serious indiscipline or very serious misconduct.

Prolonged suspension can be imposed only by the N.A. of the licence holder or organisation, or by the UIM Executive Committee.

A Prolonged suspension is always International. The N.A. must immediately inform the UIM of a Prolonged Suspension and the UIM must inform all other N.A.'s.

406.09 - EXCLUSION

Exclusion means a permanent loss of all rights to take part in any activities falling under the UIM and its affiliated organisations. A person or an organisation who has committed a moral or sporting offence of extreme gravity is liable to be excluded.

A sentence of exclusion can be pronounced only by the N.A. of the licence holder or organisation, or by the UIM Executive Committee.

Exclusion is always International. The N.A. must immediately inform the UIM when it has imposed a sentence of Exclusion and the UIM must inform all other N.A.'s.

In the case of suspension, prolonged suspension or exclusion, all National Authorities and therefore their affiliated clubs bind themselves to respect the decision and apply it in their own country.

407 - PRIORITY OF THESE RULES

The course of the UIM has to be exhausted, before the dispute can be brought before civil court.

408 - POWERBOAT PROTEST FORM

PART A

Is for you, the driver, to complete and hand in within the specified time limit.

PART B

Is for you, the driver, to complete and give to the Protest Committee Chairman at the beginning of the Protest Meeting.

PART C

is for the Protest Committee to complete.

DRIVERS PROTEST FORM

PART A

SECTION 1

NAME OF EVENT: _____ DATE OF RACE OR HEAT: _____

HEAT NUMBER: _____ CLASS: _____

SECTION 2

YOUR NAME (PROTESTOR) _____

YOUR BOAT NUMBER: _____ BOAT NAME: _____

SECTION 3

I (the Protestor) am protesting against: (tick ✓ as appropriate)

- To seek redress from the Race Organising Committee for actions or omissions.
- Another driver, name: _____ Boat No: _____
- The Results as posted

SECTION 4

Which rule has been infringed _____

Time of incident: _____

On which lap was the incident _____

Where was the incident _____

SECTION 5

Your signature _____

Time of signature _____

SECTION 6

You must see this section completed by the official receiving this Protest:

Name of Official: _____

Time Protest received: _____

PART B**SECTION 7**

Either, explain incident with another driver; in writing and by drawing;

Or, explain your reason for Protesting against the Race Committee:

409 - ARBITRATION**409.01 - GENERAL**

A party of the appeal procedure has the right to ask for a final arbitration at the CAS (Court of Arbitration for Sport). The commencement of the arbitration procedure requires the acceptance of the mandate by the CAS. In the event that the CAS refuses to accept the mandate for arbitration the decision of the Appeal Board remains final. The CAS procedure must abide by the following rules.

409.02 - PARTIES OF THE ARBITRATION PROCEDURE

- a. The UIM will be party of the arbitration procedure and will be represented by the chairman of the Appeal Board.
- b. Any other party of the appeal procedure shall be notified of the arbitration procedure and may join the arbitration procedure as party by written declaration to the UIM office or directly to the CAS.

409.03 - DEADLINES FOR ARBITRATION

The intention to ask for arbitration must be notified in writing to the Secretariat of the UIM by the interested party within 7 days of the day following the notification of the appeal decision.

A written justification for the claim for arbitration must be received by the Secretariat of the UIM within 14 days from the day the intention to ask for arbitration was received by the UIM Secretariat.

The notification and the justification must be signed by the party.

409.04 - FEE AND COSTS

The UIM arbitration fee is 1.525 €. The CAS may decide to refund/ partly refund this fee according to rule 405.10.

Regardless of the final outcome of the arbitration procedure all costs arising out from the arbitration by the CAS (e.g. invoices from the CAS or from experts mandated by or on the initiative of the CAS, travel, accommodation and communication costs of the UIM representative or similar) are borne by the party who asks for arbitration unless differently foreseen below.

This party must deposit an amount that covers all such possible costs no later than 14 days after being notified of the amount. The Secretary General has to determine this amount after consultation with the CAS and taking into consideration an amount of 2.290 € for possible costs arising to the UIM. The deposit has to be paid by bank transfer the UIM or directly to the CAS as determined by the UIM Secretary General. If during the procedure further costs arise, the party who asks for arbitration has to deposit the missing amount accordingly within 14 days from notification.

A party according to rule 409.02. b. bears the costs that arise from the performance of its rights as party of the arbitration procedure including but not limited to travel and accommodation costs, consulting fees and similar.

409.05 - FURTHER PROCEDURE

The CAS will be mandated by the parties involved but only through the UIM Secretariat. The UIM will assist the party by drafting and transferring the mandate to the CAS. The UIM must not transfer any mandate to the CAS unless the arbitration documents and fees according to rules 409.03 and 409.04 will be received in due time.

If the interested party misses any deadline, this party loses the procedure and the decision of the Appeal Board will be automatically upheld. The UIM Secretariat in this case will not submit the mandate to the CAS or withdraw any mandate that might have been submitted. In this case the interested party has to pay 50% of the arbitration fee and any costs according to rule 409.04.

The further procedure shall be determined by the CAS.

409.06

The decision of the CAS will be final and it shall not be subject to any further jurisdiction.

500 - TECHNICAL RULES, CLASSES

500.01 - GENERAL

The 500 group rules contain general technical information.

For specific class rules see:

- Offshore General Rules	RULE 701 - 715
- Offshore 1	RULE 860
- Offshore 3	RULE 730 - 737
- V24 Class	RULE 950
- Pro Vee Class	RULE 960
- Offshore 3 - 225	RULE 1000

By word “boat” it is understood to mean any vessel used in powerboat racing.

When a matter is strongly recommended, the driver (pilot) choosing a different solution may be asked to explain to the measurer or scrutineer the advantages of his solution.

500.02 - INTERNATIONAL OFFSHORE CLASSES

The request to create any new International class has to be presented by at least three Nations.

All classes for which the rules have been drawn up by UIM are International.

All National bodies are bound to observe these rules without any alteration whatsoever. Only the UIM Cominoff has the right to change the rules. Rule changes must be ratified by UIM General Assembly.

500.03 - RECOGNISED CLASSES

All national classes for which the rules have been properly established by a National Authority are recognised by UIM.

501 - MEASUREMENT CERTIFICATE

A boat is not allowed to take part in a local, National or International race without a measurement certificate, issued by a National Authority, made up in the native language and in English according to the official UIM ***offshore measurement certificate with logbook***.

The Measurement logbook shall follow the powerboat in all its racing activities. This is to get the whole racing history of the boat.

The Measurement logbook shall be updated at any change of year, rules, engine, owner or class.

For closed cockpit boats there must also be an appendix with documentation specifying the cockpit design and dimension including material specification and layup together with calculation of hydrodynamic loads acting on cockpit and the strenght of the cockpit and primary structure (roll bars).

The certificate is available for the National Authorities only, from the UIM office.

This document shall be registered and stamped by NA (see rule 508.20).

501.01

Each National Authority stipulates the cost of measurement and the duration of the validity of the certificate.

The request to obtain a measurement certificate must be accompanied by the homologation file of the motor, if such is requested for the devices being used.

The certificate is available for the National Authorities only, from the UIM office.



Offshore

Measurement Certificate

No _____

501.02

Any owner who has made alterations affecting the measurements in the certificate must notify his National Authority at once. Non-observance of this rule invalidates the certificate. The National Authority re-measures the boat at the owner's expense. Any boat that has been altered but not re-measured is not allowed to race.

The certificate will become invalidated on change of ownership.

501.03

The serial number of the motors or engines may be omitted in the measuring certificate.

501.04

The model of the International measuring certificate as described hereafter must be adopted and used by all National Authorities of the UIM.

The certificate contains the following data in English and in the language of the country issuing the certificate.

501.05

No blank certificate may be issued by National Authorities. Documents must be completely filled in with all particulars pertaining to the series for which the boat is measured. The certificate must also be signed and stamped by the National Authority.

501.06

Drivers need only supply, and measurers need only check such particulars as are applicable to the class of boat.

501.07

Anyone involved in the building of the hull or motor is not to act as measurer or to be a member of the Measurer's Committee as measurer where they have a vested interest.

501.08

All measurements are to be taken at least twice. All measurements of length are to be expressed to the centimetre.

501.09

The owner of a vessel pays all fees and expenses for rating or measuring his boat.

The measurement certificate is only to be handed over when all dues have been settled by the owner to the National Authority.

501.10

When a re-measurement is made at the request of the National Authority the expenses and fees for such re-measurement are paid by the National Authority if the first measurement is upheld.

501.11 - HULL MEASUREMENT

All hull measurements are to be taken while the boat is ashore.

The length must be measured between perpendiculars of the external moulded length of the boat, taken at the extreme bow and the rear most planning surface (the rear most part of the keel) including all hull planning steps (regardless of height), but excluding trim tabs, either fixed or movable.

Hull extensions beyond the transom shall be deemed to be “fixed trim tabs” and shall not be included in the measured length.

Hull extensions added to the bow and/or any extending parts, rubbing strakes, fenders, outboard motor spacing brackets, stabilising trim tabs and rudder assemblies are not to be included in the measured length.

When defining “extreme bow”, any form that constitutes the bow and its construction and contributes to the performance of the boat shall be included in the measured length. Any other attachment that is added to meet a minimum length requirement shall not be included in the measured length.

Should the trailing edge of the planning surface of a hull or sponson terminate with an angle greater than 15° measured between the keel and outer chine in plan view (birds eye view), such excess shall not be included in the measured length.

The beam measurement shall be taken at the widest part of the moulded hull, excluding any extending parts, rubbing strakes and/or fenders.

Trim tabs, either fixed or moveable, shall not exceed a length of 10% of the measured length of the hull.

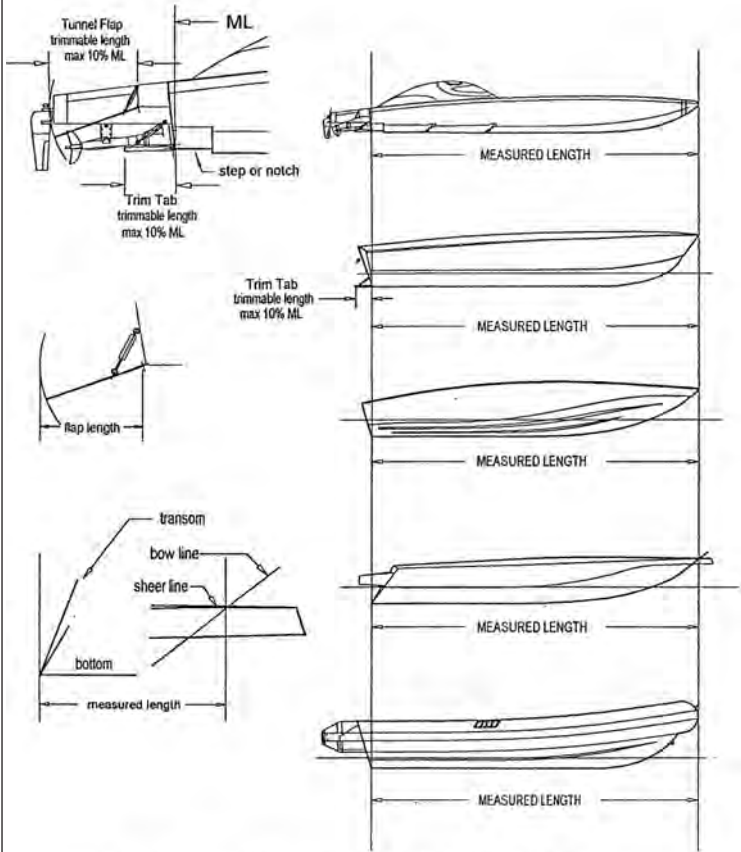
Multihull tunnel flaps shall not exceed a length of 10% of the measured length of the hull.

For Rigid Inflatable Boats (RIBs), the length measurement shall be taken at the extreme of the bow tubing (or rigid hull, whichever is the longer), to the rear most planning surface, excluding any trim tabs as for moulded hulls.

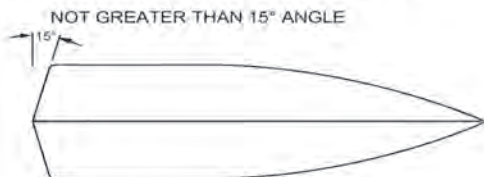
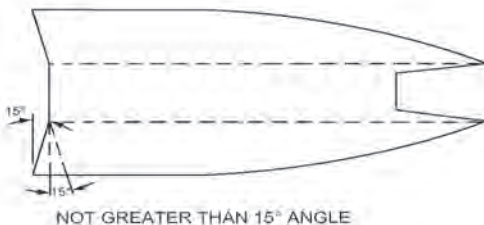
Tube extensions on RIBs extending beyond the rear most planning surfaces shall not be included in the measured length.

The RIB beam measurement shall be taken at the widest part of the inflatable tubes when fully inflated.

LENGTH MEASUREMENT DIAGRAM



OFFSHORE UIM RULES nos. 501.11, 721.1

TRANSOM MEASUREMENT CLARIFICATION**MONO HULL IN PLAN VIEW****CATAMARAN IN PLAN VIEW****501.12 - FLOTATION**

Description of the flotation (type, place(s) and volume) should be presented in the measurement certificate.

The owner may not change his flotation gear without having the corresponding changes noted in the measurement certificate, by an official measurer. However, additions to the certified flotation are allowed.

502 - INSPECTIONS

502.01.01

Hulls, motors, fuel, accessories and equipment, subject to restrictions regarding dimensions or other characteristics, must be submitted for verification.

At every race meeting the boats must be inspected before the race and practice due to safety reasons.

502.01.02

Drivers are at all times responsible for the condition of their boat (hull, motors, accessories, equipment, etc.)

502.01.03

Errors, if any, on the part of the manufacturer, builder, mechanic or even the previous owner do not justify in any way non-conformity with the rules.

502.01.04

Any driver refusing to comply with the decisions of the Technical Inspectors or who does not comply with the conditions of the rules, is not allowed to take part in a race or, should he have raced, cannot be classified and penalties may be applied.

502.01.05

Technical inspectors have the right, once races are over, to carry out all the checks they consider necessary, even when inspection has taken place before practice, they have the right to inspect as they think fit.

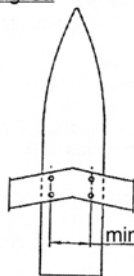
502.02 - WINGS

A wing is defined as a device above the deck that has a downward plus or minus 60 degree lower surface, with an area in excess of 0.1 square metres (1.08 sq. ft).

The use of any wing, whether for lift, down force, or stabilisation is permitted, provided that it is fitted to good standards of workmanship. However, the following rules apply:

1. The width of the wing cannot be more than 1.5 times the width of the hull for single hull craft and more than the width of the boat for multi hulls.
2. The wings must be strong enough to bear a man's weight (100 kg) at all points on it up to 1.2 m from the ends.
3. The wing should be fixed to the hull at least at four points in groups of two, spaced no less than 1.8 m apart with steel bolts of a diameter of at least 14 mm (Fig.3.A). The use of shock absorbers, which work under compression only, is recommended (Fig.3.B). A section smaller than that of the bolts must not exist at any point in the support.
4. A suitable transverse arrangement must be provided to make the fixing of the quadrilateral hyper static (Fig. 4 A.B.C.).
5. Loss of a wing means disqualification from the race concerned and re-use of the wing is subject to technical documentary evidence that structural strengthening has been carried out.
6. The driver is entitled to decide whether or not to use a wing in case of each individual race, but the existence of the wing must be noted on the certificate of weight and the fitting procedure must be checked by the weight measurer.
7. The wing must not be ahead of the crew or the centre of gravity of the boat.
8. No trimable aerodynamic surfaces are allowed on the wing.
9. Wings of other designs than described in this rule; Drawings must be sent to Cominoff for approval before they are allowed to be used.
10. Wings are not allowed in Class 3 or V Class.

fig. 3a



Rubber in compression

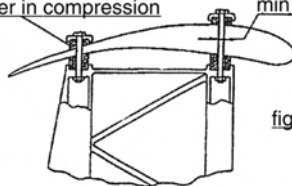
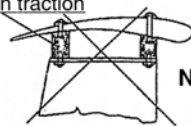
min \varnothing 14 m

fig. 3b

Rubber in traction

fig. 3c

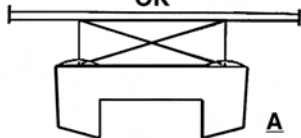
min 1.8 m.



NO!

fig. 4

OK



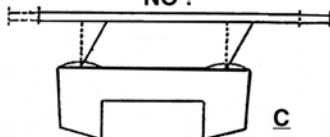
A

OK



B

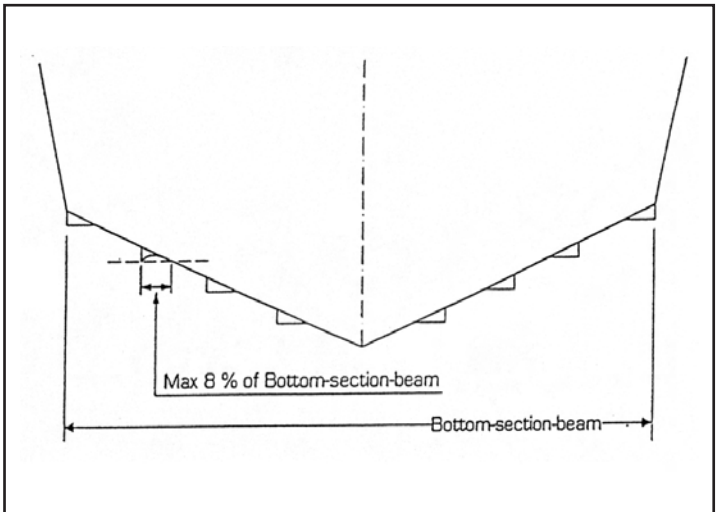
NO!



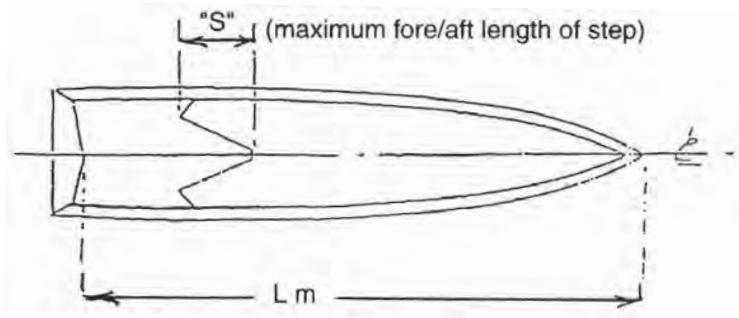
C

502.03 - MONOHULL DEFINITION

1. A monohull should be a boat with one hull.
2. A minimum distance of 80% of the “Measured Length” should, in the centreline, be the deepest part of the hull.
3. Transverse sections should, from the centreline, have positive angles up to the sections maximum beam, except in the following two cases:
 - (i) Each spray deflector may have a concave surface with a horizontal width from the hull to the outside of the spray deflector of no more than 8% of the hull’s bottom-section-beam (see drawing).



- (ii) Each spray deflector must not have a total downward angle exceeding 20 degrees.
- (iii) Steps which are swept aft may have a distance “S” of no more than 25 % of “Measured Length” (see drawing)



502.04 - ENGINES

502.04.01

The total cylinder capacity of the motor or motors forms the basis for eligibility in the corresponding classes.

The engines may be of the two-stroke or the four-stroke types.

Fuel may be fed to the engine by a carburettor or by injection.

502.04.02

Rotary motors of the type covered by the NSU-Wankel patent are admitted on the basis of an equivalent cylinder capacity.

The rotary motor cylinder capacity equivalent is twice the volume determined by the difference between the greatest volume and the least volume of the working chamber.

502.04.03

Verification of motors are carried out according to the facts contained in the homologation file.

At World records runs, World Championships and Continental Championships, inspection of the winning motor(s) is compulsory.

Inspections may also be carried out whenever necessary at all International and National events.

The following procedure is mandatory for these inspections:

1. Check that the motor type code and visual appearance conforms to the motor homologation sheet.
2. The measurements stated in the homologation sheet must be checked on the motor. If the results are within the specified tolerances, the motor should be accepted as legal. Only adjustments to meet dimensions shown in the homologation sheet are allowed.
3. When ports in cylinders are adjusted to the dimensions specified in the homologation sheet, material may only be removed in the specified opening to a depth of 10 mm, to match the shape of the original adjacent connecting passage (channel) outside the adjusted port opening.
4. Parts not specified with measurements in the homologation sheet can only be checked by comparison with standard parts. As the actual manufacturing tolerances are not published, small differences between the measurements of the inspected part and the reference standard part must be accepted.
5. Cylinder dimensions are measured when the motor is cold. Allowance is made for the carbon and oil present in the combustion chambers. Checking is done with oil. A large scale graduated cylinder or burette with adequate orifice is to be used.

502.04.04

At anytime during an event, the technical inspector, the O.O.D. or the UIM Commissioner reserves the right to change a competitors existing ECU/CDU unit with a standard OEM unit as supplied by the UIM.

UIM race officials must allow each competitor sufficient time to change the ECU and check that it is working. UIM scrutineers will supervise the installation and seal the units on completion. UIM ECU units will be removed post-race and/or the supervision of UIM scrutineer. This process is mandatory.

If a competitor refuses this request, they will be immediately disqualified from the event, without right of protest.

503 - EQUIPMENT

503.01

During a race, the boat must have onboard all the equipment required by the Race Committee in the detailed instructions contained in the advance programme.

503.02

All attachments, handholds, steering gear, pulleys, fair leads, cleats and anchor bits, etc., must be bolted to the hull.

503.03

Hatches and covers are to remain closed during racing, except for temporary checks.

504 - FUEL

N.B. Although International standards exist for petrol and for diesel, there remains a huge imbalance of specification from one country to another regarding the specification of 'pump' fuel available.

Due to these circumstances no specification of fuel is illustrated in the following rules.

Organisers are encouraged to supply a common fuel for use at International events in an effort to maintain equal opportunity for all competitors.

504.01

The only fuel to be used by any offshore powerboat must have the same characteristics as the fuel commercially available to the general public as dispensed from roadside pumps or marine fuelling facilities.

504.02

For petrol engines, the fuel used shall be unleaded petrol.

No additives are permitted except lubricating oil for two stroke engines, providing that such oil does not increase the octane or the water content of the fuel or enhance the fuel by any other means.

504.03

For diesel engines, the fuel shall be that intended for use in diesel engined road vehicles or automotive based marine engines (including red diesel).

504.04

Post race comparison testing may be used to compare petrol fuel samples taken from boats, to that of the original supplied fuel or a locally obtained sample if the fuel was not supplied by the organiser.

Post race testing shall check:

1. Oxygenates by checking the electrical conductivity using the Digatron DT-15 apparatus.
2. Specific Gravity by using a Petroleum Hydrometer.
3. Water Solubility for detection of Methanol and other water-soluble additives.
4. Ceric Nitrate re-agent test for the detection of Alcohol.

1. Oxygenates

Use of Digatron DT-15

This apparatus is to be used for comparison testing ONLY.

Comparison of ~~ē~~bench mark~~ī~~ Petrol/Petrol with two-stroke additive

1. Competitors using two-stroke oil must declare the brand of oil and the ratio of oil used to the CTO. (Take firm note of 504.02)
2. A sample of the mixture must be made using a small quantity of the 'bench mark' fuel and the declared oil.
3. The 'bench mark' fuel (now containing the appropriate oil) can now be compared to that of the competitor sample.
4. The procedure for comparison testing is as per the petrol/petrol comparison.

Comparison of benchmark Petrol/Petrol (no two-stroke additive)

5. Stabilise the electrode of the Digatron by immersion in the 'benchmark' fuel.
6. Set the indicator to Zero.
7. Remove the electrode and clean off any residue of 'benchmark' fuel. Wait a few moments for any final residue to evaporate.
8. Immerse the electrode into the competitor fuel sample and observe the reading of the indicator.
9. It is very unusual to find an identical reading. (This is due to influences of differing temperatures, aeration, etc. of the sample)
10. An indication of between -0.10 and +0.10 is normal and indicates a satisfactory comparison.
11. An indication exceeding these parameters is justifiable reason for laboratory analysis.

2. Specific Gravity

The use of a Petroleum Hydrometer should be made to compare the specific gravity of the 'benchmark' sample against the competitor sample.

Both readings should be identical. The temperature of both benchmark samples and competitor sample should be the same.

3. Water Solubility

This test should be conducted as per the text of the Digatron instructions.

Both 'benchmark' sample and competitor sample readings should be identical.

4. Ceric-Nitrate re-agent

This test should be conducted as per the Digatron instructions.

Both 'benchmark' sample and competitor sample results should be identical.

504.05

If the results of the above tests show characteristics of similarity between the locally obtained sample (or fuel supplied by race organiser) and the sample taken from the competitor, then the fuel must be accepted as legal.

A judgment of illegality cannot be made on the results of the above tests alone.

Only the results from a laboratory analysis can be used to declare a fuel conclusively illegal or not.

If the results of the above comparison tests do not show characteristics of similarity and give cause for doubt, then the following detailed 'Fuel Sample for Laboratory Analysis' procedures shall be enforced.

504.06 - FUEL SAMPLES FOR LABORATORY ANALYSIS

Procedure

Fuel samples for laboratory analysis may be taken at any time and place during any event under the authority of the UIM. Such samples shall be taken at the discretion of the Chief Technical Officer and must be as per U.I.M. procedure. Fuel Test Certificates (in quadruple) shall be made available to the Technical Officers responsible for collecting fuel samples.

Only nominated Technical Officials may take samples.

504.07

At the beginning of the event, the Chief Technical Officer must obtain sufficient fuel from the local fuel source (or the fuel supplied by the organiser) for comparison test sampling.

This fuel will be referred to as the 'Bench Mark' fuel, and will be the fuel to which all comparisons will be made.

504.08

It is mandatory to carry out fuel testing on the heat and/or race winners (to third place) at all World Championship events, and recommended at Regional Championship events.

In a multi-race Championship, the fuel testing may be carried out by testing 3 or more boats at each race.

504.09

Any competitor refusing to provide an adequate fuel sample or having insufficient fuel available in the boat's main fuel system or tanks immediately following completion of the race, will be disqualified.

504.10

Race Organisers must ensure that there is a supply of at least six one litre containers per class of boat competing for each heat/race of the event.

Such containers must:

1. Be clean and constructed of robust, fuel non-reactive, impermeable material
2. Be sealable
3. Have provision for identification.

504.11

Equipment used for the extraction of fuel from boats must be clean and constructed of non-reactive material.

504.12

Each sample must be divided into two and placed in separate one litre containers, being sample A and sample B. The containers must be completely filled up and immediately sealed and identified to the boat from which the sample was taken. This information must be entered on the Fuel Test Certificate and on the sample containers. The Fuel Test Certificate must record; the place and time of taking the sample, the identity of the boat from which the sample was taken and the identity of the driver of the boat.

504.13

Sample A must remain in the control of the Chief Technical Officer or his deputy and sample B must be given to the driver or a representative of the driver's team. The driver or the representative must sign the Fuel Test Certificate, acknowledging receipt of the sample.

504.14

A sealed sample of both the competitor's fuel and the benchmark sample must be sent to an authorised petroleum laboratory for full analysis.

The Chief Technical Officer must deliver all such samples to a courier authorised by the Organising Committee or the UIM Commissioner.

The C.T.O. must return a copy of the Fuel Test Certificate, signed by the courier for the receipt of the samples, to the Organising Committee.

The authorised courier must deliver all "A" samples, together with copies of the relevant Fuel Test Certificate to the nominated laboratory, where they must be tested in accordance with standard International scientific procedures.

If the result of the laboratory analysis proves that the fuel is not within the same characteristics as the benchmark sample, the analysis must be paid for by the competitor. If the fuel is within the same characteristics as the benchmark sample, the organiser must pay for the analysis. The same rule shall apply for testing diesel.

504.15

The results obtained from such testing must be attached to the laboratory's copy of the Fuel Test Certificate and delivered to the UIM as soon as practicable after the results have been obtained.

504.16

The UIM must as soon as practicable after receipt of the results, notify the following:

1. the relevant drivers or team representative
2. the relevant N.A.
3. the Race Organiser.

504.17 - AIR

Only air may be mixed with the fuel as an oxidant.

504.18 - DEFINITION OF PETROL

Petrol described within these rules is defined as one of the following:

1. Petrol of a kind recognised by the UIM as being on general and genuine sale to the public in a country where UIM International events are held annually.
2. The fuel used in Offshore racing shall be pump petrol as this term is generally understood. The detailed requirements of these rules are intended to achieve this purpose whilst allowing the use of absolutely consistent petrol's for racing purposes.

Any petrol which appears to have been formulated in order to subvert the purpose of this regulation will be deemed to be illegal.

Fuel suppliers are invited to supply samples of their petrol's to be checked for conformity before use.

505 - ENGINE HOMOLOGATION

505.01 - RESPONSIBILITY

COMINSPOORT controls the homologation process and appoints a POWERTRAIN committee to carry out the various processes. POWERTRAIN is composed of persons appointed from the membership of COMINSPOORT and COMINTECH and, if needed, outside experts.

505.02 - PROCESSES

UIM establishes two processes depending on the entity initiating the homologation request.

505.02.01 - PROCESS #1

An engine manufacturer applies for an engine homologation for competition in certain UIM classes.

505.02.01 - PROCESS #2

COMINSPOORT or COMINOFF requests homologation of an engine for competition in certain UIM classes.

505.03 - HOMOLOGATION APPLICATION - PROCESS #1

Engine manufacturers are invited annually to apply to the UIM for engine homologation via their affiliated UIM National Authority of the country of engine manufacturer. Homologation application must be made using the electronic homologation forms available from the UIM Secretariat.

505.04 - MINIMUM PRODUCTION QUANTITIES

The required number of engines manufactured prior to homologation is to be verified by the manufacturer and endorsed by the National Authority of the country of manufacture. If insufficient numbers of engines have been manufactured at the time of homologation, a written certificate, witnessed by a practicing lawyer, of the firm intention to manufacture the specified numbers is required from the manufacturing company which must clearly state the completion date of manufacturing schedule to meet the required minimum production numbers. Minimum required quantity of outboard engines is 250 units. Minimum required quantity of offshore inboard engines is 30. In Process #2, no manufacturer statement or statement of intention is required.

505.05 - DEADLINE FOR APPLICATION

In Process #1, Application for the request of homologation of an engine must be received by the UIM Secretariat by close of the last business day before the relevant year's GA, to be considered for validity commencing 1st April in the following year.

In process #2, COMINSPOORT must direct POWERTRAIN to complete the homologation of a candidate engine at the COMINSPOORT meeting during the General Assembly of any given year to be considered for validity commencing 1st April in the following year.

505.06 - APPROVAL OF APPLICATION - PROCESS #1

An application for homologation, running production change or extension of validity will be considered by COMINSPOORT (or, in the case of engines designated for use by the offshore category, COMINOFF). This commission will determine if the candidate engine complies with the rules of the class of racing for which the engine is designated (i.e.: availability, performance level, suitability).

Once an application for homologation, running production change or extension of validity of a candidate engine is deemed acceptable for a given class by the

relevant commission, the inspection and verification procedure (or administration of the running production change or extension of validity) will commence.

505.07 - INSPECTION VERIFICATION PROCEDURE

At a time and place agreed upon by the UIM COMINSPORT's Powertrain committee and the entity requesting homologation, a production example of the candidate engine; all its components; and any relevant inspection tools will be examined to complete and verify the declarations in the homologation file as applied.

This inspection must be completed before 1st April for the homologation to be valid for commencement in that year.

In Process #2, Powertrain will procure the example engine; examine it; complete the relevant documentation; and dispose of the engine (all @ UIM expense).

The homologation becomes valid when all correct and completed documentation including the finalized homologation document has been received by the UIM Secretariat. This must be before 1st April for the homologation to be valid for commencement in that year.

505.08 - TERM OF HOMOLOGATION

A homologation remains valid from the date of completion through to 31st December of the tenth year of validity.

505.09 - ANNOTATION OF VALID CATEGORIES

The final homologation file will display the specific UIM power boating category(s) for which the engine is approved. The annotations will be made appropriately as follows:

- CIRCUIT
- OFFSHORE
- CIRCUIT/OFFSHORE

505.10 - HOMOLOGATION FILE COPIES

Copies of homologation files are obtainable from the UIM office against payment of a nominal fee.

The copyright thus bought allows a National Authority to reproduce the file with as many copies as necessary for its drivers and technical services. A nominal charge may be levied for copies provided by the National Authority.

For restricted stock UIM classes a homologation sheet is required for engine and propulsion units. This file is Valid only for the model describer. Any model derived must be subject to a separate homologation.

A copy of the homologation sheet must accompany any engine, boat or accessory used in a class in which homologation is required.

505.11 - RUNNING PRODUCTION CHANGES

Approval of application for running production change must initially follow the rules of 505.07.

An application for a running production change may be requested by a manufacturer at any time of the year, although its commencement will be dated from 1st April in the year applied for. Powertrain will confirm whether any requested change is acceptable as a running production change, or requires the engine model to be re-homologated. Modifications to specifications dictated by manufacturing or structural requirements are admitted, but not those primarily aimed at improving performance. (as a guide, any changes or accumulation of changes likely to increase power by more than 5%, or the speed performance corresponding to more than 5% increase above that given by the original homologation specifications, may be grounds for re-homologation).

An approved running production change will be announced on the UIM Website and will be deemed legal for use in racing 30 days after the official date of approval.

505.12 - EXTENSION OF VALIDITY

Extension of validity of an homologation is determined solely by COMINSPOORT or/and COMINOFF.

505.13 - RESPONSIBILITY FOR HOMOLOGATION EXPENSES

The expenses of inspection are to be borne by the UIM in either process.

The verification inspector will be designated by Powertrain and his expenses (travel, hotel and meals) are to be reimbursed by UIM. It will be COMINSPOORT's

intent to minimize costs by designating the nearest capable person as the inspector for each candidate engine.

508 - CREW SAFETY

All other 508 Rules apply to all categories of boats.

All Class 1 mono/multi and Class 3C, 3D, 3S and 3-225 must be equipped with a Reinforced Cockpit(s) with Canopies for all riding crew members and buoyancy to ensure the boat floats. The crew, who must be seated, must have a restraint system comprising of and conforming with the following rules:

A Reinforced Cockpit with Canopies is defined as a containment area for crew and can be constructed as an integral part of the boat. This Reinforced Cockpit Area must be designed and constructed to a specification capable of withstanding the forces of a water impact when running at the highest design speed of the boat, and therefore protecting all members of the crew in the event of an accident. The various components that constitute the Reinforced Cockpit shall be properly maintained to ensure reliable operation of all components, with emphasis being placed on the canopy release mechanism, emergency air supply and restraint systems.

It is recommended that Sponson Cockpits are not used.

These rules also apply to any boat in any class using Reinforced Cockpits with Canopies.

508.01 - COCKPIT EVACUATION / IMMERSION TESTING

Before racing in a craft with restraint systems, all crews must have passed in the last fourteen months, an immersion test in a restraint system to ensure that they can exit a reinforced cockpit crew compartment successfully.

Prior to taking the immersion test, all crews must have a valid scuba certificate or have received suitable training. This alternative training should be approved by the National Authority.

An Immersion Certificate to certify the passed test, showing the expiry date, must be delivered by Experts recognised by a National Authority.

All riding crew members using restraints must sign the National indemnity form prior to competing in any race or practice.

508.02 - DRAWINGS AND MEASUREMENT

Three view drawings (plan, side and elevation) of the design of the Reinforced Crew Cockpits(s), the Bulkheads, the type of Canopy, the Buoyancy System and the Restraint System anchorage Points must be lodged with the National Authority of the measurer and verified at the time of craft measurement.

Drawings shall be provided showing canopy aperture dimensions for full or partial canopies, single or tandem arrangements. Arrangements shall describe whether fore and aft, or side by side seating is fitted.

Drawings shall show the method and construction of release devices. Drawings should show the material specification of the transparent areas.

Prior to Boat Measurement the drawing and material specifications shall be sent to the Measurer requested to measure the boat. On completion of measurement, the drawings and material specifications called for by the designer shall be lodged with the measurers National Authority before they issue a certificate of compliance and measurement.

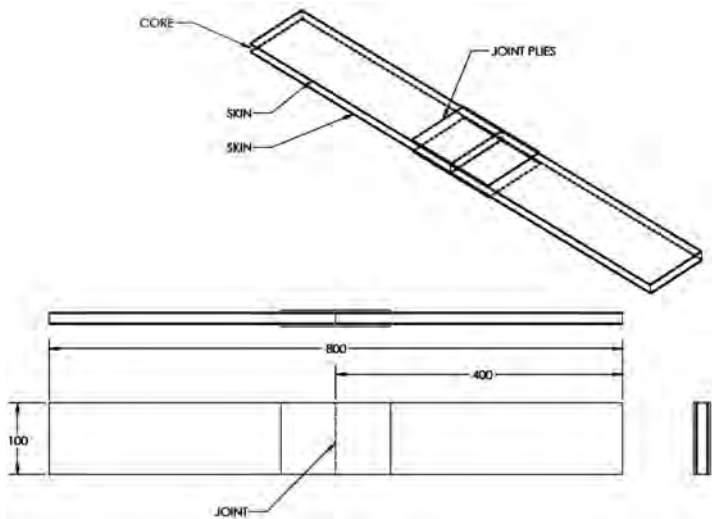
508.03 - REINFORCED COCKPIT AREA AND CANOPY

The reinforced cockpit(s) shall be of a closed type design with a minimum of one opening hatch and constructed to a similar strength as the running surface of the boat. This area must be the sides, floor, decking and bulkheads fore and aft. ***All new cockpits built after January 1, 2015 shall be built by a UIM registered cockpit builder. Constructors wishing to be registered must submit panels for test according to the following standards. Constructors meeting these standards will be registered as UIM registered cockpit builders.***

UIM OFFSHORE CLASS 3 COCKPIT/CANOPY TEST STANDARD

Sample Construction Requirements

- a. ***Sample quantity must be 3, one of which must include a sample of the joint/bonding of the cockpit parts, transverse to the length of the sample.***



- b. *Trimmed sample size must be 100 mm \pm 1.0 mm wide x 800 mm \pm 5.0 mm long, with the width being parallel.*
- c. *For fibre orientation the 800 mm length is to be parallel with the centerline of the boat.*
- d. *Sample must be laminated on a flat surface using the same manufacturing process, materials, and fibre orientations as the intended homologated cockpit construction.*
- e. *The sample must have a uniform thickness with no core crushing along any edges.*
- f. *The sample must have one moulded face and the other face being unmoulded, the moulded face will be taken as being the external surface of the cockpit during testing.*
- g. *The sample must be representative of the thinnest lay-up of the cockpit/canopy (excluding the various flanges for windshields, hatches etc.).*

- h. The sample and cockpit must be manufactured using balanced or unbiased materials.*

Sample Test Method

- a. The sample will be supported across the full width perpendicular to the 800 mm edges by two parallel 25 mm Steel bars at a distance of 500 mm apart. The load will be applied equally through two 25 mm Steel bars, each a distance of 167 mm parallel from each support.*
- b. The moulded face of the sample will have the load applied and the unmoulded face will support the sample.*
- c. The load will be applied at 0.4 mm/sec and the deflection will be measured at the two 25 mm Steel bars applying the load within 2 minutes.*

Sample Test Requirements

- a. The sample when loaded with a force of 3000 Newtons must have no more than a maximum deflection of 25 mm without the sample failing.*
- b. The sample weight in gm/sq m will be calculated, skin thickness and sample thickness will be measured to enable inspection and comparison of damaged homologated cockpits/canopies.*
- c. Further non-destructive test analysis methods may be used to compare test samples with registered cockpits during the life of each cockpit/canopy.*

Sample Manufacturing Information Requirements

- a. Ply laminating sequence (stating which ply is the moulded face).*
- b. Ply materials.*
- c. Ply weave styles*
- d. Ply material weight in gm/sq m (dry weight ie. Without resin)*
- e. Ply orientation (where 0 is parallel with the 800 mm edges).*
- f. Core material and density in lbs/cu ft or kg/cu m.*
- g. Manufacturing method (stating vacuum, pressure, and temperature).*

- h. A 100 mm x 100 mm sample of all materials used (resin samples not required)*
- i. The completed questionnaire for offshore cockpits (available on the UIM web site) along with the supporting analysis for the question on “Primary Structure Strength”*

Samples as per sample construction requirements must be sent to the UIM appointed person.

For classes 3-225, 3C, and 3D the minimum test standard shall be 3000 Newtons.

508.04

Canopies must be a composite structure with the following features.

508.05

Polycarbonate areas are strongly recommended to be as small as possible while still maintaining that the driver and co-driver have clear, safe and undisturbed visibility ahead at sea level whilst racing. For Class 1 it is strongly recommended that these polycarbonate areas are built using 12 mm thickness, or more.

The combined visibility of driver and co-driver must be through a horizontal arc of 225 degrees (112.5 degrees either side of the centre line of the boat).

These polycarbonate panels are to be recessed into the composite structure and may be bonded using a suitable bonding agent, and/or “bobbins”.

It is highly recommended that there is also a through bolted outer flange for the fitting of the polycarbonate panels.

508.06 - SCREEN FLANGES

Screen flanges shall be a minimum of 50 mm at forward direction and 35 mm towards sides and should be fastened every 100 mm if using “bobbins”; it is recommended to use metal “bobbins” with heads, as opposed to the recessed plastic type.

The outer polycarbonate area of the flange fitting must not be painted, so that the measurer/ scrutineer may monitor any discrepancies.

- Window to flanges joints must be glued and/or use bobbins of nylon or aluminium.

- Bolts: min 6 mm stainless steel, nylock nuts, washers.
- Bolt spacing: max. 10 cm if not glued
- The outer edges of the canopy surrounding the hatch, must be fitted with a water deflector, (height 10 mm min) to prevent water forcing open the hatch in the event of a capsized.

508.07 - ROLL BAR

These Restraint Cockpits must be fitted with an internal roll bar, two in a tandem cockpit as a minimum. There must also be, between the two single cockpits, an anti-compression strut or structure of similar strength to the roll bar.

- Roll bar in front of/around each crew member.
- Roll bar strong enough and well secured to the bottom stringers.
- Central compression strut to hold roll bar, for side by side cockpits. Side compression struts may also be necessary for side by side cockpits.
- Alternatively, instead of a compression strut, the design of the cockpit primary structure will consist of a center roof rib connected to the roll bar and the aft bulkhead with sufficient strength to satisfactorily react the design impact loads.

508.08 - HATCHES

Hatch openings shall have a minimum of 25 mm flange.

Hatches must have a slot for pry bar, on the opposite side of the hinges, use in emergency/rescue.

508.09 - HATCHES

Hatches should be recessed on the front and sides.

The outer edges of the canopy surrounding the hatch, must be fitted with a water deflector, (height 10 mm min) to prevent water forcing open the hatch in the event of a capsized.

Water deflector to be fitted only on front and sides of hatch, not behind of hatch. (A water deflector on back of hatch might force water into cockpit area.)

508.10

It is mandatory that the hatches are constructed to the same specification as the cockpit. The hatches shall be fitted with a catch which has a positive open and positive close mechanism and should hold the hatch against lateral forces. These hatches shall be able to be opened from both inside and outside the cockpit and must have a second emergency mechanism to allow the rescue team to easily remove the hatch from outside if necessary.

These hatches should be fitted with hinges with short release pins. This is important because long pins invariably bind the hinge.

508.11

There should be one or more divers grab handles fitted to the outside of each hatch.

508.12

Canopy hatch release handles, which must be provided both inside and out, must be painted fluorescent orange or have a fluorescent orange background panel to identify them and directional arrows to indicate the method of opening.

508.13

The canopy lid hinges and the canopy hatch covers release mechanism must not encroach within the canopy aperture area, and these hinges and release mechanisms must not in any way hinder the exiting of crew members when fully race fitted.

508.14

Canopy openings should have the entry/exit apertures located directly above the crews' heads.

The canopy aperture openings should be at least 0.55m in length and 0.55m in width. If the crew is seated side by side, then the opening should be at least 0.55m x 0.825m wide. In tandem configuration, the opening(s) should be 0.55m x 0.55m per crew member. The canopy apertures should be cut with all corners having a radius of 0.025m minimum or 0.25m maximum. The radius should be constant and have a smooth finish to relieve stress.

508.15

The canopy aperture must have a 20 mm wide (minimum) fluorescent orange band around the opening, both inside and outside of the opening.

508.16

It is mandatory that one single air supply (not oxygen) and a bottle will be provided for each riding crew member. The air supply must be securely fixed adjacent to, or on each one of them. It is recommended that sufficient air be provided in each individual bottle for ten minutes. Air bottles must have a pressure gauge fitted for visual checking at pre-race scrutineering. This gauge should be filled with liquid and be at least 5 cm in diameter for easy reading.

Air supply bottles shall be “Turned On” before starting a race or taking part in practice and/or testing.

508.17

Reinforced Cockpits must have flood tubes or other means of flooding the cockpit to equalise the pressure quickly in an accident. The floor of the cockpit should be as air tight as possible to help the cockpit pressure equalise far more quickly when in an upturned position.

508.18

Boats with restraints must have stop buttons/switches located in the cockpit area, immediately accessible to driver, co-driver and rescue officers. The stop buttons/switches must be identified by a fluorescent colour.

These switches must shut off all fuel pumps as well as the ignition circuit.

In the case of diesel boats, the stop control cable for the fuel injection pump shall be a non-sleeved cable, so as to eliminate the cable being able to bond in a fire.

508.19 - STROBE LIGHT

All boats shall have a White or Orange High Intensity Strobe Light fitted to indicate “coming off the plane” but not needing assistance. The strobe light must be able to be operated by the throttle man, and should be operated by the throttle man if a problem occurs, to enable any following race boats to take avoiding action. The

strobe light shall be mounted on the top rear of the canopy. When dual canopies are used, the light may be on or behind either one.

This strobe light may also be used as a substitute for the orange retirement flag when returning to port under reduced power.

508.20

Cockpits with Restraints must be fitted with rear of head protection for each crew member. This must be an integral part of the seat, which must be attached directly to the structure of the Restraint Compartment. The head protection must be a minimum of 0.2m wide and extend at least 75% of the height of the safety helmet as worn by the crew whilst in the normal seating position. There must be a minimum of 0.12m vertical and lateral clearance between the canopy and each of the crewmembers when in the normal seating position.

508.21

The Restraint System must consist of a 5 or 6 strap harness and should utilise a 75 mm lap belt, a 50 mm strap over the shoulder harness rated at 4,100kg (9,000 lb.) and grommets to prevent chafing or cutting of the belt. Harness straps must be attached directly to the cockpit structure. Those straps close behind the driver's head and neck must be 100 mm to 150 mm apart at point of attachment. The shoulder harness should be installed at 90 degrees to the spine at shoulder line to minimise compression injuries under high "G" loading. All straps must be free to run through intermediate loops or clamps/buckles. All anchor point bolts must be fitted with backing plates of 10cm minimum width.

The driver harness attachment bolts in reinforced cockpits must consist of minimum grade EN8 bolts, with an 8 x 1.25 mm thread and locked nuts. There must be a spacer and plain washers on each bolt. The spacers must be glued to the cockpit structure. Intention of these spacers is to prevent buckling of surface material near bolts. This always leads to local delamination which easily spreads out over cockpit structure, when it is under stress.

On the sides of the structure, which has to take up the force on the attachment bolts, there must be a stainless steel plate (washer of minimum 3 mm thickness and 100 cm² area).

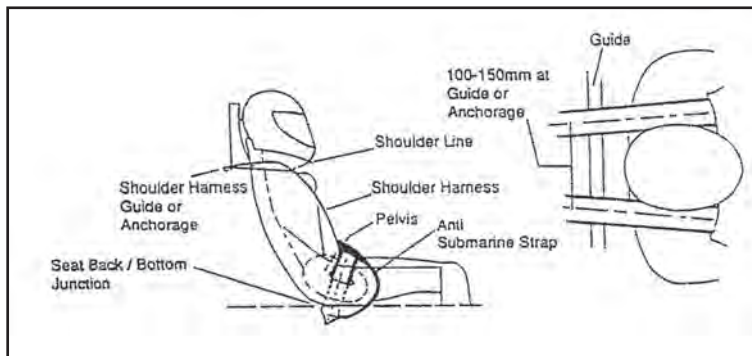
When using seats with suspension, and therefore not using a bulkhead restraint anchorage, drawings must be lodged with the National Authority of the measurer and approved prior to boat measurement.

All restraint systems must have a common method of release. The single lever method (sometimes called the NASCAR type) or rotary type, are both acceptable restraint release systems.

Both types of restraint release must be examined for satisfactory operation by the scrutineer before every race.

The harness system must comply with Drawing 2.

The shoulder harness should be installed 90 degrees to the spine at shoulder line to minimise compression injuries and the high "G" loading.



75 mm minimum/maximum to Centre line of Lap Belt at Seat Back, Seat Bottom junction. Lap Belt should continue in straight line to anchorage.

508.22

A quick release steering wheel may be fitted on a boat with personal restraints, but all drivers must be able to exit the cockpit without removing the steering wheel.

508.23

Rear view mirrors are mandatory, as well as a method of cleaning the canopy whilst under way.

508.24

For Class 3C, one extinguisher is sufficient.

Two fire extinguishers, each a minimum of 2kg, or of equivalent capacity, must be carried and be readily accessible to the crew.

The flares described in UIM Offshore Rule 715.10 may be placed in a shallow locker adjacent to the deck race number.

Should a life raft be carried, it may be placed in the same locker.

All crew containment areas of inboard engine 508 canopied boats must be fitted with a carbon-monoxide alarm.

508.25

Racing Vests - the efficiency of the racing vest is a matter of the exclusive responsibility of the wearer. Every crew member whilst on board, must wear a racing vest during the practice runs and throughout the race. Racing vests must be coloured high visibility orange or yellow. The racing vest must have epaulets/handles to help extract crew from the boat. The racing vest must have crutch straps or a method of ensuring that the vest does not “ride up”.

508.26

Each Reinforced Cockpit Area shall have one or more water activated light(s) or similar.

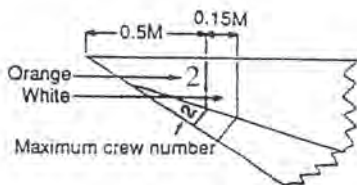
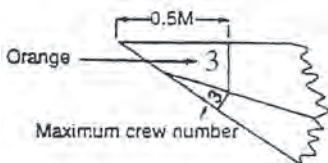
508.27

All boats with restraints must have their bows painted fluorescent orange for at least 0.5m. Only boats with restraints and closed canopies are allowed to use orange coloured bows. If the hull is of a similar colour, then there must be a white separating band of at least 0.15 m wide to ensure that the fluorescent orange band is obvious. If the number of riding crew exceeds two, the number of riding crew members must be written in black in at least 0.25m high numbering on the orange nose in the following three locations:

- a) The lower running surface.
- b) The topside of the hull/sponson.
- c) The deck of the hull/sponson.

Should any boat be found to have contravened the riding crew number requirement, the penalty shall be disqualification from that event.

Note : Numbers shall be placed on the orange nose, not the white band area



508.28

It is mandatory that sufficient buoyancy is provided in the boat, or in the material used for its construction, to ensure that the boat floats if capsized or holed. If extra buoyancy is needed, the buoyancy system described by the designer should be verified by the Measurer. This added buoyancy must be in at least four separate flotation units.

It is recommended that the buoyancy should float the hull as parallel with the surface of the water as is practical, to help in rescue accessibility.

508.29 - CLASS 3 CRITERIA

Each National Authority shall decide if its National racing shall be permitted to use Restraint Systems in Class 3 boats below Class 3C.

In Class 3 boats, all the 508 rules apply. Refer to UIM Offshore Rule 715.10 for application of flares. These flares must be readily accessible from the deck.

508.30 - CLASS 3 CRAFT WITHOUT RESTRAINTS

For Class 3A, only the overdeck water deflectors are mandatory.

The Reinforced Water Deflector must be designed and constructed of materials of sufficient strength to provide adequate crew protection, both over and under the deck and hull. The forward fairing on the deck must rise to a minimum height of the chin of the tallest crew member when in the normal driving position. It is recommended that lateral protection covering be provided up to the shoulders. The top 0.05m (5cm) of the Water Deflector must be at least 45 degrees from the horizontal, with a minimum 0.3m (30cm) width per person measured transversely in the horizontal plane.

The Reinforced Water Deflector must be designed and constructed so as to present no hazard if the crew is thrown forward and must be so designed that it would not restrict the crew from being ejected. In all cases, the visual requirements in Rule 508.05 must be adhered to.

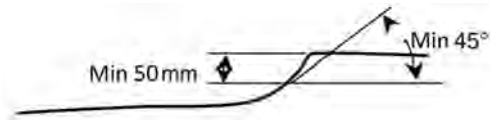
Measurers and Scrutineers will examine boats with the crew in the normal driving position.

The flares described in UIM Offshore Rule 715.10 must be readily accessible from the deck.

Rear of head protection is recommended. If fitted, it should be at least 0.3m (30cm) wide and the height of the padding must extend at least 75% of the height of the safety helmet as worn by the crew whilst in the normal driving position.

If a monohull has a canopy and/or restraints, it must conform to the same criteria as multihull boats as described in these 508 rules.

Water deflector, overdeck



600 - RECORDS AND PERFORMANCES

600.01 - GENERAL

All record trials and their control are submitted to the following rules.

The record belongs personally to the driver who has established or beaten it. The record is entered under the name and nationality of the driver.

Any race against the clock, and called kilometre or mile trials or any similar name is prohibited if not conducted in accordance with these rules.

Combining speed records with distance records is not allowed.

No direct or indirect aid is to be provided to the driver who attempts a record.

600.02

World speed records, hour records, distance records and competition records are all records open to all boats and aquabikes of International classes adopted by UIM.

600.03

National records recognised by UIM are the records established by boats of national classes duly recognised by the national authority (the rules of which have been approved by UIM). The Certificate differs from those issued for World Records.

600.04

The World's Unrestricted Water Speed Record is awarded to the fastest boat in the world piloted by the driver, irrespective of the class of boat; this record is established in conformity with UIM rules.

601 - CLASSES ELIGIBLE FOR RECORDS

601.01

World records may be attempted with the following craft :

- All UIM series and classes

- American Powerboat Association classes
- Prototypes

601.02

A boat can establish a record in its own class only.

601.03

When a class is modified so as to reduce the performance of the class, for example by :

- reducing the cylinder capacity,
- restricting the tuning permitted,
- restricting fuel permitted,
- introducing a minimum weight,
- increasing the minimum weight,
- or any other means,

the records established before the modification comes into force will be frozen and a new series of records will be started.

All proposals for new rules, rule changes and UIM General Assembly minutes shall state :

- whether or not records will be frozen,
- the classes to be frozen,
- the date(s) when the classes are to be frozen.

601.04 - QUALIFICATION

For offshore records, a boat must have completed the course, taken the chequered flag and finished first, second or third, in an UIM World or UIM Continental Championship race in the class to be attempted and after the date when the class was last frozen.

If there has been no World or Continental Championship during the year, boats that has participated in national races and finished top three will be qualified to set a world record.

602 - DEFINITION OF UNITS

602.01 - TIME, DISTANCE AND SPEEDS

The hour is the twenty-fourth part of a terrestrial day. The symbol is letter “h”.

The minute of time is the sixtieth part of one hour. The symbol is letters “mn”;

The second of time is the sixtieth part of one minute. The symbol is the letter “s”.

nautical mile	=	nm
statute mile	=	st.m
kilometre	=	km
nautical mile per hour	=	knot
statute mile per hour	=	mph
kilometre per hour	=	kmh

602.02 - STATUTE MILE, NAUTICAL MILE AND KILOMETRE

The statute mile is an Anglo-Saxon measure.

The nautical mile is used at sea.

The knot is not a unit of length; it expresses the speed of one nautical mile per hour.

One statute mile (st.m) = 5280 feet = 0.8690 nm = 1,609.3 km

One nautical mile (nm) = 6080 feet = 1,852 km = 1.1508 st.m

One kilometre = 3280.844 feet = 1000 metres

604 - BASES (THE COURSE)

604.01

Records may only be established on bases recognised by National Authorities and notified to the UIM in accordance with these rules.

604.02

The base is measured and certified by an official surveyor holding a certificate of competence from a recognised institute, in the presence of a delegate of the National Authority.

The base must be measured by triangulation or electronic distance measuring equipment (EDM), which must have a first class certificate delivered by an Observatory or similar authority. This certificate must not be older than two years.

The official surveyor provides a certificate in duplicate in which the strict adherence to the above requirements is certified.

The certificate is signed by the surveyor and countersigned by the delegate of the National Authority. One of the copies is for retention by the National Authority for its special national records files and the other is sent to UIM for its special World record files.

The base for a record must be a fixed base, as laid out for a National Authority, or must be measured for a special occasion in waters designated by the driver. It must be defined by fixed marks ashore.

604.03

No distinction is made between records established on the sea, rivers or lakes. Records must not be attempted during the period between half an hour before and half an hour after a change in direction in flow of water over the course (change of tidal flow).

604.04

Any expenses incurred in the laying of a temporary course are defrayed by the entrant attempting the record.

604.05

For speed records there shall be only one attempt on the course at any one time. An attempt must not start until the director of the trial is satisfied that effective rescue boats are on the course.

605 - CALCULATION OF SPEED**605.01**

The National Authority calculates the speed from the data provided by timekeepers. The speed is to be shown in kilometres per hour (kph) and statute miles per hour (mph), even when the attempt takes place on a course of one nautical mile.

The duration of each run is obtained by taking the arithmetical mean of the times recorded by two sets of timing devices, the mean time being given to one tenth of a second.

Should one of the timing devices stop or one of the signals fail and only one duration be registered, this duration will be accepted.

605.02

The only duration so obtained serve to calculate the speed of each run. Only two decimal places are to be used.

605.03

The speed of the trial is the arithmetical mean of the speeds, two decimal places being used.

605.04

For speed records established on a distance of one nautical mile (1 nm = 1.852 km = 1.1508 st.m) the following constants are to be used :

Where t is the time in seconds :

the speed in mph = $4142.9 \div t$

the speed in kph = $6667.2 \div t$

606 - MARGIN REQUIRED

A record will be valid only if its speed at least equals the speed of the previous record multiplied by 1.003.

607 - MOTORS

607.01

All accessories and/or elements required by the racing rules for each class of boat attempting to establish or improve a record must be on board during said attempt.

607.02

A boat which holds two certificates for two different classes is entitled to establish World records in those two classes, but shall make two distinct trials, one for each class.

607.03

Immediately after the record trial, the motor(s) or engine(s) must be sealed with a distinctive seal in the presence of the officials who have witnessed the trial.

607.04

Within 48 hours of the trial, two measurers must check that the hulls, motors and equipment are in conformity with the specifications of their class, and this inspection must be carried out in the presence of the Officer of the Day or an Official specifically appointed by the National Authority.

607.05

When attempting to break a UIM class record, the noise level of the motor shall not exceed the authorised level (rules 734.2, 734.9).

608 - REQUEST TO ORGANISE

608.01

All applications for records trials must be made in writing to the National Authority at least one week before the trial and must be accompanied by the fee stipulated by the National Authority.

The National Authority applies to the UIM for homologation of records.

608.02

Only records established under the direct control of a National Authority affiliated to UIM can be recognised.

608.03

Each National Authority decides on the application for records to be established on its territory. The National Authority may refuse to consider a record application, but must then explain the reasons in writing.

The National Authority indicates the validity of the record permit.

608.04

It is the duty of the driver to accept responsibility for any safety measures deemed necessary for this attempt.

The Officials will not accept any responsibility, they will only record the results and ensure that the rules are complied with.

609 - OFFICIALS

609.01

The National Authority shall appoint an Official Observer and other duly qualified persons to hold the official functions.

An Official so nominated shall not hold any other office than that to which he was nominated. An Official shall not himself establish a record.

609.02

The Official Timekeepers and Officials may receive payment for their services at rates fixed by the National Authority.

609.03

Time keeping may only be carried out by Timekeepers and Officials who have been recognised and authorised by the appropriate National Authority.

The stop-watches or any other apparatus employed must have a first-class certificate delivered by an observatory or similar authority. This certificate must not be older than two years.

Each Timekeeper hands to the National Authority a signed report of the record attempt, countersigned by the Official Observer.

609.04

Timing from aboard anchored boats or aboard competing boats is not allowed.

610 - SPEED RECORDS

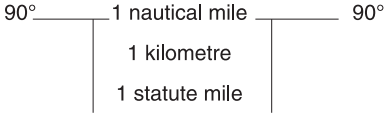
610.01

There is only one World record per class. This record can be established over the nautical mile, Statute mile, or the kilometre. For aquabikes the distance may be 500 metres.

610.02

The course is covered once in each direction consecutively.

The course is to be marked at either end by two posts in solid material, put up on shore with approved distinguishing marks affixed to them. The two lines uniting the two pairs of posts are parallel to each other and at right angles to the line of the measured course. Steering marks may be placed wherever needed.



610.03

In wide rivers and lakes, the width of the course may be marked off at each end by two buoys, moored at a distance of one hundred metres from each other. The boats must run between these buoys.

610.04

Timing is to be one tenth of a second up to 200 kph. Above 200 kph timing is to be by photoelectrical or similar apparatus to one hundredth of a second.

There must be a Timekeeper and a Timekeeper Observer at each position.

Individual timers may be used at each position or a single timer electrically activated from each position.

Where two timers are used at a position the time is the average of the two timers.

The times of day of each attempt must be recorded.

The time elapsing between the two runs must not exceed 20 minutes except for jet attempts, when the permitted interval is one hour.

610.05

A driver may re-start an attempt because of breakdown or other reason and may stop the engine or boat between runs.

If the attempt has not been completed within four hours of a starting time fixed by the National Authority, the Officer in charge may stop the trial.

611 - DISTANCE RECORDS

611.01

Distance records are established on a total course of twelve nautical miles for the outboard series, twenty-four nautical miles for other series, and six nautical miles for aquabikes.

611.02

In distance records, the base is covered up and down successively in even numbers and the turns are not included in the distance.

The course for distance records may be :

- a course established for speed records;
- a base of three nautical miles maximum, in straight lines with buoys placed at the corners if necessary, no corner to be less than 135 degrees.

The buoys placed at the corners must always be rounded.

611.03

Any base must be defined by two posts set in concrete at either end.

611.04

Timing is to be to one tenth of a second.

The timing of distance records is done by a Timekeeper with two timing devices at the starting line. He is assisted by two Officials, one with him and one at the other end.

611.05

Stopping the motor or the boat during a distance record attempt is prohibited.

612 - HOUR RECORDS

612.01

The following International and National records are recognised by UIM: 1, 2, 3, 4, 6, 9, 12 and 24 hours duration records for all the series recognised by UIM.

The homologation of a record of lesser duration than that applied for may be granted, but only within such limits as provided for by the above rule.

612.02

The base must not exceed six nautical miles from station to station, that is to say twelve nautical miles up and down. For aquabikes only, the distance may be 3 nautical miles from station to station, that is to say 6 nautical miles up and down.

612.03

The base is defined by two posts set in concrete at either end.

612.04

The boat may be driven by more than one driver, but their names must be filed in the record application.

612.05

Repairs and adjustments which can be carried out on board may be effected on the course.

The same motor or engine and hull must be used throughout the trial.

On the other hand, repairs that cannot be effected on board and refuelling and changing of drivers must be effected at the control station and are to be supervised by an Official.

612.06

Any time during which a boat stops during an attempt is included in the elapsed time for the attempt.

Turns are included in the course and are consequently not excluded from the timing.

612.07

Timing is carried out by one or several Timekeepers with two timing devices stationed at the start. The Timekeeper is assisted by two Officials, one stationed with him and one stationed at the other end.

612.08

A driver can claim several different hour records during one trial by selecting the best series of consecutive laps.

612.09

The boat must cross the finishing line at the end of the lap (the last lap) during which the record time has elapsed.

The average speed of the last lap is calculated and the distance which corresponds to the time still to be run is added to the distance covered at the end of the penultimate lap.

The result is shown as the distance covered in the record time and the average speed for that distance.

612.10

Example for a two hour record on a base of six nautical miles (nm), twelve nautical miles per lap :

boat completes 8 laps in 1 h 54 mn 40 s = 110.48 st.m

ninth lap completed in 13 mn 50 s = 830 s

time balance 5 mn 20 s = 320 s

distance balance = 13,81 st.m ÷ 830x320 = 5.32 st.m

Total distance in 2 h = 115.80 st.m

Average speed = 115.80 ÷ 2 = 57.90 mph

613 - WORLD COMPETITION RECORDS

613.01

The UIM recognises World records of 5, 10 and 15 statute miles in races for all UIM circuit classes.

No other records may be established during a race.

Unlimited hydroplanes only are entitled to statute mile records established on :

- single lap course (two laps for APBA Gold Cup races) while qualifying or racing;
- total heat average speed while racing.

614 - PERFORMANCE CERTIFICATES

614.01

UIM may issue Performance Certificates for the best heats made during races for the principal International trophies.

614.02

Application for these is to be made by the National Authority of the country where the race took place and the accuracy of the results, timing, course and class is to be certified by them.

614.03

UIM only recognise the results which beat previous performances.

614.04

The amount of the fee for Performance Certificate is decided upon annually by the General Assembly.

614.05

UIM may issue a Performance Certificate for speed trials made by boats not included in UIM classes. These trials must, however, be made in accordance with UIM rules.

UIM award Performance Certificates for boats of the Production Boat Series having established or broken speed, duration, distance or competition records according to the Record Rules.

614.06

For these performances, it is not permissible to remove any of the boat's accessories, or items of comfort, provided by the builder and these are compulsory for racing.

UIM may award Performance Certificates for any of the offshore classes, International and National, established at sea on properly measured runs.

615 - HOMOLOGATION OF RECORDS

615.01

Only records homologated by UIM are considered as World Records.

Only records established under the direct control of a National Authority affiliated to UIM can be recognised.

615.02

If a record has been established on the territory of two National Authorities, it will be entered as a record by the National Authority on whose territory the start was made.

As soon as a record trial has taken place, the National Authority communicate the results to the Secretary General of UIM by fax and confirm it by letter.

The information has no official character and is only transmitted to such National Authorities as apply for it, in order to avoid useless trials.

615.04

The Timekeepers and Measurers must complete the official UIM record application form, which must be signed as correct by the Senior attempt Official (President, Chairman, O.O.D. etc.) and be sent to the National Authority within three days of the trial, seven days for APBA controlled classes.

The National Authority transmits the application for homologation, together with other necessary papers, to the UIM no later than four weeks after the trial, sixteen weeks for APBA controlled classes.

615.05

For a request for homologation to be considered by the UIM, the National Authority must send :

- a plan of the course if not already in possession of the UIM,
- the completed official UIM application form which must include any National Authority calculations,
- a copy of the boat's measurement certificate,
- a photograph of the boat under way with the colours, inscriptions, etc. noted on the reverse ; also to be noted on the reverse, the driver's name, class and speed achieved,
- a list of the full names of the Officials with, at least, the names of the Timekeepers, Measurers, Rescue Officer, O.O.D. and Observer,
- offshore boats, a copy of the results in which the boat qualified, as per UIM rule 601.04,
- for attempts on tidal waters, the calculation stating the times of the changes of direction of water along the course (changes in tidal flow) and the source of the information,
- a cheque for the amount due.

615.06

The recognition fee is fixed annually by the General Assembly.

615.07

Pending the official confirmation by UIM, neither the driver, builder, nor any other shall publish or cause to be published, circulate or cause to be circulated, the results of the trial, until the delegate of the National Authority has given the actual results in writing, after checking the calculations in the presence of Timekeepers.

Even when such authorisation has been received, results may only be published with the addition of the words "Subject to official certification by UIM" in as large and prominent type as that in which any part of the notification itself is printed.

Neglect to comply with the above regulations automatically entails the refusal to recognise the record. The National Authority have the right to penalise whoever is responsible.

615.08

UIM issues annually a list of World records, changes being made known, when they occur, by means of newsletters.

UIM keeps a registered list of all World records and delivers a certificate for each new record.

616 - LONG DISTANCE OFFSHORE MOTORBOAT WORLD SPEED RECORDS

616.01 - AUTHORITY

The UIM through the relevant National Authority is the authority in all matters concerning world water speed records.

616.02 - RATIONAL

Records

Records will be recognised for passage between pre-determined points providing all relevant rules for independent observation are established, maintained and ratified.

Racing rules

The UIM racing rules do not apply during individual (non-race) record attempts.

616.03 - GENERAL

The purpose of the rules is to enable attempts to be made on a variety of offshore records. They do not form the conditions of any prize or trophy, nor are they motorboat instructions.

The record for each passage shall stand until the elapsed time of that record has been reduced by at least one minute. Every request to ratify a record shall be considered in time and date order.

The time used for this purpose will be Greenwich Mean Time (GMT).

The UIM/relevant National Authority cannot accept any responsibility for personal injury or loss of life, damage to or loss of property arising from any record attempt made under its rules whether supervised by its officials or not.

The official language is English.

616.04 - LONG DISTANCE OFFSHORE WORLD RECORDS

The World Record for the passages, approved by UIM, will be held by the motorboat and skipper which establishes the shortest elapsed time in accordance with these rules.

Any offshore distance record can be held in either of the following categories:

All records are to be divided into three outright classes, up to 30 ft/9.14 meters LOA, over 30ft/9.14 meters LOA, and over 50 ft/15.24 meters LOA, to be measured using the method for Offshore Class 1. Any of the above classes could also be eligible for the fastest outright record. For example : a skipper may apply for the up to 30 ft record, but if he beats the existing outright time, then he would be eligible for the outright record.

It is not allowed to attempt any records single handed.

Assistance – means that during any record attempt, a vessel may enter harbour for fuel, repairs, alterations or stores as required. A craft may be anchored or beached during the record attempt but any repairs must be made entirely by the crew without outside resources or materials. During any such stops, the timing of the voyage continues.

It is never permitted to take on board stores or equipment or get any other kind of help from another vessel whilst under way (except as permitted by 616.05 6 Emergencies).

616.05 - MOTOR BOAT RULES

Applies to all long distance offshore records and all types of vessel.

Propulsion - any method of mechanical force applied to the water.

Steering - any type of automatic steering is allowed.

Navigation - any type of navigation equipment is allowed and there is no objection to any type of information or advice being transmitted to or from the vessel. However, the vessel must be driven by the onboard crew.

Emergencies:

In the event of the record attempt being diverted or compromised by the necessity to render assistance to a mariner in distress (excluding members of the record crew), time allowances may be made subject to the provision of adequate documentary and substantive evidence of time lost.

In the event of fouling another vessel or structure (e.g. an oil rig).

It is permitted for the crew of the other vessel or structure to assist the vessel in getting clear.

Rounding - when a course calls for a mark, beacon or point of land to be "rounded", this means that an imaginary string from the start to the finish points must enclose the feature specified when pulled tight.

Tidal stream or current - no allowance is made for tidal stream or current in considering any of these records.

Refuelling - must not be transferred at sea from another vessel excepting where the record attempt passage exceeds 3500 nautical miles and no adjacent landfall or harbour is available en route. Recognised or National Authority approved fuel outlets within harbour confines must be used in all other cases. If re-fuelling takes place at sea, the re-fuelling vessel shall be stationary and properly equipped with suitable transfer hoses. Should such re-fuelling at sea be planned, the record attempt application must provide details that include the name and size of the re-fuelling vessel and its proposed location.

Responsibility - it is the sole and inescapable responsibility of the skipper of a motorboat engaged in any such record attempt to decide whether to start and, having done so whether to continue the voyage.

The Vessel - shall have communication equipment capable of maintaining contact throughout the attempt with monitoring stations along the route.

Life Raft - a certified life-raft(s) suitable for the maximum number of persons on board shall be carried equipped with full safety equipment appropriate for the planned passage.

Tow - if a tow is received, the record attempt vessel shall return to the last official recorded "own power" position before resuming the attempt.

616.06 - SAFETY RULES

It is not within the province of these rules to set safety standards. However, all vessels making record attempts must comply with the appropriate safety standards laid down by International Maritime Law and the relative marine safety authority of their country of origin or registration,

In the case of a world circumnavigation attempt, the challenge vessel shall be built to comply with the rules for classification of an International Organisation (I.M.O.) recognised Classification Organisation and as such, be of suitable class for the range and speed of the vessel. In addition, the vessel shall be certified by the Statutory Regulating Authority of the country of registry, which, as a minimum, shall be equivalent to the UK Maritime and Coastguard Agency (M.S.A.) Code of Practice for Safety of Large Commercial Sailing and Motor Vessels.

Evidence of compliance must be provided to the relevant National Authority prior to departure.

616.07 - NOTICE

At least one month's notice must be given of intended record attempts, accompanied by the appropriate fee; otherwise the relevant National Authority cannot guarantee to observe a record. Records cannot be recognised retrospectively.

616.08 - OBSERVATION DECLARATION

When proper notice of a record attempt has been received, the relevant National Authority will appoint an observer to make arrangements for the start and finish to

be witnessed and timed. The observer or a person appointed by him will also need to check that the documentary evidence provided relates to the vessel that has completed the record passage. The skipper and one other member of the crew will be asked to sign a declaration that all rules have been followed. Where practicable, there is no objection to the observer being on board during the record attempt.

616.09 - TIMING

The moment of starting and finishing a record passage must be recorded by Timekeepers nominated by the relevant National Authority to the nearest second using GMT. The accuracy of these must be checked within six hours of the start and finish of the attempt by reference to an official time signal by telephone, radio or synchronisation with GMT via the WWW. Details of the timepiece used must be logged and signed for by the observer or his appointee.

Records will always be defined as an elapsed time in days, hours, minutes and seconds. Average speed may be shown for information only.

616.10 - RECORD COURSES GENERAL

Only fixed points of land, fixed navigational beacons or other charted fixed objects can be used as reference points though they need not necessarily be sighted. In special cases, pre-approved by the Observer, such as positions derived from satellite navigation systems are acceptable.

All routes are reversible and the record time from A to B can be beaten by an attempt from B to A.

If a new route is requested then an application from a National Authority to the UIM is required. This application can be submitted to the UIM at any time during the year, however all applications must be made no later than 90 days prior to the intended record attempt. Provided that the new route does not conflict with a very similar existing route it will be approved. The application should detail any restrictions and the estimated mileage, in nautical miles, of the route including any rounding's.

The start and finish positions if A to B must be given.

The UIM shall hold records of all Record Courses and any specific rules applying to such Record Course. Any record challenger should obtain these from the UIM via their relevant National Authority prior to attempting any such challenge.

616.11 - FEES

The skipper or organisation wishing to make an attempt on a record is responsible for the costs of observing and ratifying it.

These must be paid in two parts as follows:

On making an official application to the relevant National Authority for a record attempt to be observed, a fee fixed annually by the UIM General Assembly is required plus a fee from the National Authority.

If the attempt is unsuccessful then the UIM portion of the fee will be refunded, if successful the UIM fee will be forwarded to the UIM for the ratification of the record.

616.12 - EXPENSES

The person or organisation making the record attempt is responsible for the legitimate expenses and remuneration of the official Observer, Timekeepers and all other officials and any assistants that may be required. This must be paid before any record claim can be considered.

616.13 - RATIFICATION

The following documents are to be submitted to the UIM/relevant National Authority; the submission may be by fax if followed up by the original documents.

- A statement by the skipper giving details of the vessel, the crew and the course followed. This must include a declaration that these rules have been followed, signed by the skipper and at least one other member of the crew.
- A report by the Official Observer giving details of the timing and verifying the accuracy of all observations.
- All other items as listed in Rule 615.05.

616.14 - INTERPRETATION

In any case of any dispute over any of these rules, the decision of the relevant National Authority is final.

700 - INTERNATIONAL OFFSHORE RULES

CLASS 1 - INBOARD ENGINES HULLS/LENGTH/WEIGHTS

From 1st January 2003, all Class 1 rules will be aligned with the 860 series rules.

701 - GENERAL

Rule changes are only made following approval by UIM Offshore nations at General Assembly. Such changes taking effect not less than six months after approval, unless a special decision is mentioned in the wording of the decision with its justification. Changes affecting safety may be made at any time, according to their necessity.

701.01

The following basic rules and definitions are for Offshore powerboats which have been designed and are suitable for racing in open and unprotected waters.

701.02

Organisers shall not be permitted to make any additions to these rules which by their nature would affect the eligibility of any boat EXCEPT in circumstances specifically affecting safety or in respect of special laws which may apply to the particular locality of the race. Organisers shall seek the approval of their National Authority to make such additions.

701.03

Organisers shall have the right to refuse any boat which they deem to be unsuitable and/or which does not conform to the requirements of these rules.

701.04 - RESERVE NUMBER

701.05

The finishing order is determined by the order in which the boat finish in its Class.

Points for Championships are awarded separately for the Classes.

701.06

Organisers are free to organise prize categories for varying types, such as cabin boats, diesel engines etc., provided the craft comply with fundamental rules.

701.07

Organisers may also organise separate races for the different classes and/or prize categories.

701.08

Championships: Races included in championships are governed by individual rules of the particular championships as defined under their own rules.

701.09

All boats entered for racing shall be subject to the direction and control of the Race Committee.

701.10

It shall be the sole responsibility of each boat's driver(s) to decide whether or not to start or to continue the race.

701.11

Unless specified in racing instructions, no outside assistance (including refuelling) is allowed during a race.

In an International Championship race no outside assistance (including refuelling) is allowed.

The Race Committee has the right to decide whether or not there was outside assistance or not.

It is permitted to use electronic steering compasses and/or electronic position fixing equipment.

It is expressly forbidden to interface any of this equipment to the steering system.

702 - CREW

702.1

All boats will carry a minimum of two crew members with valid **powerboating** licences and of minimum age 18 years.

For Classes 3: 3A and 3B, the minimum age is 16 years.

For 3C and above, the pilot must also have raced in at least three races in a powerboat class with minimum 90 hp.

702.2

Only licensed drivers are permitted to control the boat when under the jurisdiction of the race organiser.

702.3

Members of the crew under their age of majority must be required to produce evidence of parental consent to participate in the race.

702.4

Breach of the above rules shall entail immediate disqualification.

702.5

In championship races involving points qualification, the points will be awarded to the driver designated as the first driver on the entry form and who shall be aboard throughout the races.

703 - HULL

703.1 - DECK

The deck must be able to bear the weight of a standing man (100 kg) at any point.

703.2 - DECK, OUTBOARD MOTOR PROPELLED CRAFT

Wherever the motor is, the hull shall be fully decked to the stern, incorporating a well formation if desired.

Any holes for the passage of cables, fuel pipes, etc., shall be fitted with sealing grommets and be above the lowest point of the stern structure.

703.3 - STEERING POSITION

The steering arrangements shall be sited so that the safe handling of the boat shall not be prejudiced by interference to the driver's line of sight across the bow at any state of trim in calm water.

703.4 - GUARD RAILS

Rails or wires or hand holes shall be fitted which shall extend fore and aft to enable crew to proceed from the cockpit to the whole deck for the working of mooring, anchoring and towage and access to deck hatches.

No ropes can be used as rails or hand holds.

703.5 - MOORING CLEAT

All boats shall be fitted with a well secured cleat or sampson post on the fore-deck adequate for anchoring in a seaway and for towing at sea over a prolonged period.

703.7.1 - CRANAGE

Boats must be fitted with a minimum of three lifting eyes, either through deck or topside hull cleat type, suitably engineered to withstand lifting the boat fully equipped and fuelled with an adequate safety margin.

Each boat shall be equipped with a detachable strop assembly providing a single lifting point.

The strop must be certified to carry total boat weight.

703.7.2 - CRANAGE SAFETY

The Race Committee shall cordon off, restrict and supervise the crane area. Access to this area shall be limited to authorised race officials and designated race boat crews.

703.8.1 - CANOPIES

All craft in Class 3 / 6 litre mono/multi must have canopies and comply completely with rule 730.2.

703.8.2 - WINDSHIELD

Windshields, if fitted, shall be strong and well supported. All edges of plastic material and framing shall be effectively padded. Transparent windscreens are to be made of polycarbonate material.

703.9 - OPEN COCKPITS

703.9.1

The cockpit opening shall provide seating positions for at least two drivers, with the following minimum dimensions; length 0.76 m, width 0.53 m per driver, with a round-off radius of 0.25 m.

703.9.2

There shall be a minimum cockpit clear depth of 0.38 m everywhere, with the exception of seats, steering wheel and controls. Measurement is done from the upper part of the cockpit edge or from the top of the cockpit coming if it is substantial.

703.9.3

All cockpit seats must be securely fixed to the main hull structure. A seat for each crew member must be provided. The crew should be seated at all times when racing or attempting the race start.

703.9.4

The structure of cockpit fairings must at least equal the strength of the main deck i.e. be able to be walked on at any point as per Rule 703.1.

703.9.5

For craft without canopies and restraints, also refer to 508.40.

703.9.6.

Only boats with restraints and closed canopies are allowed to use orange coloured bows.

703.9.7

In open cockpits, the cockpit edge or the type of a substantial coaming is not to be higher than the top of the crew persons' shoulders.

704 - INBOARD ENGINES

704.1 - EXHAUST

The engine exhaust manifolds, pipes and exhaust driven turbines, from the engine outlet to the point of exit must be water cooled by the water jackets or shielded. The external surface of the shield must not exceed 150° C at any time. The exit must be located in such a position whereby the crew cannot be effected by exhaust fumes.

704.2 - SHAFT GUARDS

Inboard propelled craft must have a protected shield to each input drive shaft which shall include at least two rings which completely encircle the shaft and a 180° scatter shield over each universal joint.

In the event of a failure this protection shall be capable of containing the shafts and couplings from causing damage to the hull skin, fuel tanks, any other installation and or component, and from causing any danger to the crew.

704.3 - ENGINE COMPARTMENTS

Inboard engines are to be installed in a compartment separated from the cockpit and other accommodation which is to have adequate ventilation. Vent holes in bulkheads or covers shall not be in close proximity to the drivers and crew without flame trap protection. The compartment(s) shall have rigid covers.

704.4 - FIRE EXTINGUISHERS

For all inboard engine installations, it is mandatory to have a fully automatic fire-extinguisher system, though it is permitted to have a separate manual system.

705 - ENGINES

705.1

Turbo and/or Supercharged petrol engines are not permitted.

When a diesel engine includes a device for supercharging or turbocharging, the nominal cylinder capacity is to be multiplied by 1.4 and the boat will pass into the class corresponding to the effective volume resulting from this multiplication.

705.2

A dynamic air inlet for ducting the atmospheric air toward the engine's air intake is not considered a supercharging device.

If approved for use in offshore by Cominoff and annotated on the Homologation Certificate, EPA 2006 compliant, outboard engines utilising "air pumps" as part of the induction system are not considered as supercharged.

705.3

Turbo Compound engines and Gas Turbines are not permitted.

A Turbo Compound Engine is defined as a process in which effective power is produced not only by expansion in the working cylinders, but also in a second stage. In compound engines, the exhaust turbine, or one of the exhaust turbines, is coupled to the engine output shaft.

706 - ENGINE CUT-OUT

706.1

- a) Engine cut off switches for connection to all crew are mandatory (first man out shuts off engine). An emergency override system to restart engines shall be mandatory. The lanyards used must not exceed 120 cm between the driver and the boat. The lanyards shall be attached to all crewmembers at all times when the boat is at racing speeds.
- b) The lanyards must be able to support ten times the strength necessary to operate the device or to resist a load of ten kilos. Having chosen the biggest value, they must be capable of operating the device in any direction on a horizontal level.
- c) Lanyards are not necessary when restraint systems are used.
- d) If the engine is a Diesel, the action of the device should operate on the injection pump gear.

707 - ENGINE AND FUEL COMPARTMENT BULKHEADS

707.1

Bulkheads of engine and fuel tank compartments must be sealed to the hull across the bottom and have sufficient height above the bottom to prevent fuel and fumes flowing throughout the boat. The separate compartments thus formed must have provision for separate pumping out of bilge's.

Fuel containment systems (tanks, cells, etc.)

In all new boats built from 30th November 2001, and all existing canopied craft registered for racing after that date, boats must have fuel filling and fuel venting capabilities outside of the crew containment area. Canopied craft are to have fuel tanks contained in sealed compartments from the hull and crew area to avoid leakage of liquid or vapour being released into the area being used by the crew. Fuel filling / fillers are to be outside of the crew compartments.

707.2

Closing of the fuel flow must be done by means of a device, the control of which must be in the cockpit and within reach of every crewmember. No sleeved cables are allowed, so as to eliminate the cable being able to be bonded in a fire.

707.3

The fuel tank or tanks must be grounded to enable the discharge of static electricity.

707.4

All new Class 1 gasoline boats measured after 1st January 1995 must have all fuel tanks filled with any suitable material that will avoid gas explosions.

Fuel tanks may be constructed as an integral component of the hull construction, or may be fabricated material suitably secured to the hull.

Purpose designed and constructed rubberised collapsible fuel cells may be used.

Fuel may not be transferred during a race except by means of permanently installed fuel lines connected to the tanks.

708 - PROPULSION

708.1

One hundred per cent of the propulsive effort shall be derived from the water while proceeding at racing trim in calm water.

708.2

All craft must be able to be manoeuvred ahead and astern and have neutral capability, operated by controls at the helmsman's position. This facility must be a permanent installation and able to be demonstrated as practical at any time.

708.3

Boats running two or more propulsion systems must have either an external tie bar to stabilise out drives or some other protective device that will prevent drives colliding should a failure occur.

708.4

No multi-ratio transmissions, torque converters, or variable ratio drives are permitted in Class 3.

709 - GYROS

709.1

All moveable surfaces allowed on the boat shall be under the control of the onboard crew without any sensors in the control loop (i.e. no auto pilots, gyros, accelerometers or other attitude or acceleration sensors may be in the control system).

710 - RACE NUMBERS

710.1

All boats are to be allocated a race number by their National Authority. These numbers shall be painted in waterproof black paint or to be made of black coloured self-adhesive materials of adequate strength. They shall be applied to a white or yellow background. No number may begin with a zero. All numbers shall be plain and up-right.

710.2

On all boats the numbers shall be positioned as far forward as possible whilst adhering to the requirements of rules 710.3 and 710.4.

On catamaran boats the numbers must be placed on:

- a) The foredeck within front 1/3 of boat.
- b) The tunnel underside within front 1/3 of boat.

c) The outside of both sponsons within front 1/3 of boat.

On all other boats the numbers must be placed on:

a) The foredeck within front 1/3 of boat.

b) Both sides of the hull within front 1/3 of boat.

710.3

Those numbers displayed on the foredeck and on the underside of a tunnel must read correctly from the transom and shall be underlined by a black bar.

710.4

The individual numbers shall conform to the following minimum dimensions, the only exception being where the size of the boat does not allow the minimum size to be carried:

Height	Width	Thickness	Spacing
30 cm	23 cm	5 cm	13 cm

The background shall extend at least 23 cm in front of and beyond the end numerals and at least 7.5 cm above and below the numerals.

The beam numbers may also be painted on slightly curved non-vertical surfaces, provided that the whole number is within 30 degrees from the vertical position. The height must be to the minimum height measured vertically.

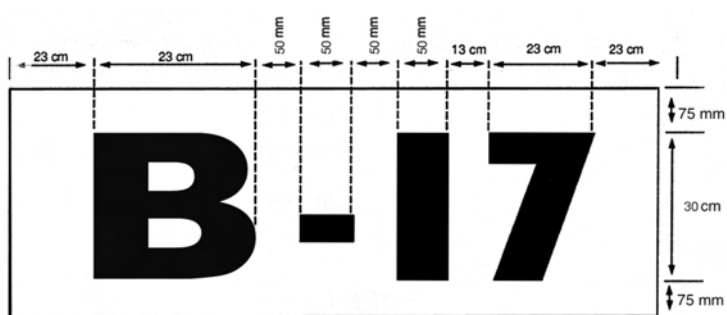
710.5

Race Organisers are empowered to require a number to be changed in the event of identical numbers being issued by different National Authorities. Except in exceptional circumstances the visiting craft has priority.

710.6

The class letter will precede the numbers and they will be separated by a dash. The class letter must conform with the same minimum dimensions as the race number.

Dimensions are as per 710.4



711 - NATIONAL FLAG

711.1

All boats are to fly their appropriate national flag of the designated driver as determined by the entry, with minimum dimension 0.45 m x 0.30 m, throughout the race.

711.2

Alternatively, the flag must be painted, or glued, on a panel of not less than 0.45 m x 0.30 m on both sides of the hull.

712 - RACING VESTS

Refer to rules 205.06 and 508.25

713 - HELMETS

Refer to rule 205.07

714 - PROTECTIVE CLOTHING

Refer to rule 205.13

715 - SAFETY EQUIPMENT

Refer to rule 205.12

715.01

The following equipment shall be carried and stowed to the satisfaction of the Race Committee.

715.02

Boats meeting the criteria of UIM Offshore Rule 508 (restraints and canopies) must comply with the requirements regarding storage of safety equipment contained in those rules.

715.03

When additional electric pumps are fitted to the fuel supply a fuel cut-off switch for the pump shall be fitted within reach of either crew member.

715.04

A manually operated bilge pump with the capability of pumping out any one of the watertight compartments.

In addition to the above minimum it is highly recommended to have any additional electric bilge pumps that appear appropriate.

715.05

Suitable gear for anchoring and berthing, including one anchor, not less than 30 metres of suitable chain or warp.

Two towing lines of sufficient strength, one twice the length of the boat.

715.06

Two (2) fire extinguishers to be immediately accessible to the crew, and must be minimum of 2kg each, or of equivalent capacity.

For Class 3A, 3B and 3C, there is only a requirement for one fire extinguisher of 2 kg, or of equivalent capacity.

The extinguishers must fulfill the classification ABC (eq. US: classification AB).

American	European	Australian/Asian	Fuel/Heat source
Class A	Class A	Class A	Ordinary combustibles
Class B	Class B	Class B	Flammable liquids
	Class C	Class C	Flammable gases
Class C	UNCLASSIFIED	Class E	Electrical equipment
Class D	Class D	Class D	Combustible metals
Class K	Class F	Class F	Cooking oil or fat

715.07

An orange flag measuring at least 0.60 x 0.40 metres and the means of hoisting it, to indicate retirement.

715.08

A fitted steering compass.

715.09

Up-to-date charts covering the whole course of the race.

715.10

Two red hand-held distress flares and two orange smoke flares.

715.11

One whistle attached to each crew member.

One compresses attached to each crew member.

It is strongly recommended that one hand held flare is attached to each crew member.

715.12

A sharp knife suitable for cutting webbing, clothes or rope. The knife shall be suitably sheathed so as to protect crew members against accidental injury. It may be located in a fixed position in the cockpit adjacent to all crew members or, be separately carried on each crew member.

715.13

One diving-mask.

715.14

For Class 3

A pail or bucket with a minimum capacity of 5 litres.

715.15

Each Organiser can, in the Advance Programme, specify additional safety equipment necessary for their race.

CLASS 3

730 - HULLS AND CLASSES

Offshore Class 3 racing shall permit the use of boats powered by Outboard engines.

730.1

Class 3 subdivisions are established in accordance with the following

	3A EPA	3B EPA	3C EPA	3D EPA	3S
Cubic Cap.	up to 1800cc	up to 1800cc	up to 3000cc	up to 6000cc	up to 6000cc
Max HP	115 hp	115 hp	200 hp	400 hp	600 hp
Min Length	5.75m	5.0m	6.5m	7.6m	7.6m
Max length	7.0m	6.8m	8.4m	10.00m	10.00m
Mono Min kg	700kg including crew	655kg <i>including crew</i>	695kg	1400kg	1755kg
Multi Min kg	730kg <i>including crew</i>	845kg	1400kg	1755kg
Add kg per m		50kg/m	65kg/m	100kg/m	110kg/m
Cube					
Length	2.35m	2.35m	2.75m	3.60m	3.60m
Width	1.20m	1.20m	1.20m	1.20m	1.20m
Height	0.46m	0.46m	0.46m	0.46m	0.46m
Buoyancy					
Mono GRP	250l	270l	370l	500l	500l
Mono Sandwich	130l	140l	150l	280l	280l
Multi GRP		280l	380l	550l	550l
Multi Sandwich		180l	200l	300l	300l

All Technical rules for 3S shall be the same as for X-cat rule Part 4: Technical rules. That is for both boat and engines.

The weight for 3B are inclusive the crew, see further rule 731.

In the A litre class, only monohulls will be accepted.

From 2006 a transition period for the use of EPA/CARB allowed engines will commence. Where non EPA/CARB engines represent less than one third of the participating boats in a World Championship event, as described in UIM Offshore rule 800.3, that event will run. However, in the following UIM calendar year non EPA/CARB engines will not be allowed in the specific class.

Hull measurement Class 3A

All hull measurement are to be taken while the boat is ashore.

The length must be measured between perpendiculars of the external moulded length of the boat, taken at the extreme bow and the interface between the hull and bracket/engine.

Hull extension beyond the transom shall be deemed to be “fixed trim tabs” and shall not be included in the measured length.

Hull extensions added to the bow and/or any extending parts, rubbing strakes, fenders, stabilizing trim tabs and rudder assemblies are not to be included in the measured length.

When defining “extreme bow”, any form that constitutes the bow and its construction and contributes to the performance of the boat shall be included in the measured length. Any other attachment that is added to meet a minimum length requirement shall not be included in the measured length.

Boat width : 1.60 m - 2.00 m.

730.2 - REINFORCED COCKPITS

Reinforced cockpits are permitted in any category of Class 3. The following specification is the minimum mandatory standard for any reinforced cockpit used in any category of Class 3 boat. See also rule 508.

Cockpit type:

The reinforced cockpit(s) shall be of a closed type design with a minimum of one opening hatch and constructed to a similar strength as the running surface of the boat.

Cockpit minimum size:

Hatch opening per person: 55 x 50 cm min.

Width: shoulder level 60 cm min.

Clearance: helmet to hatch 10 cm min.

Cockpit construction:

- Window to flanges joints must be glued and/or use bobbins of nylon or aluminium.
- Bolts: min 6 mm stainless steel, nylock nuts, washers.
- Bolt spacing: max. 10 cm if not glued
- The outer edges of the canopy surrounding the hatch, must be fitted with a water deflector, (height 10 mm min) to prevent water forcing open the hatch in the event of a capsize.
- Hatches must have a slot for pry bar use in emergency/rescue.
- Controlling crew must have clear visibility ahead with adequate panoramic view.

Window areas:

- Material: Polycarbonate or similar.
Glass (in any form) is specifically prohibited.
- Min thickness of window :
Side by side cockpits min 9.5 mm.
Screens with curvature and/or tandem cockpit min 7.9 mm

N.B. With the exception of the above specific criteria in this section, the 508 rules apply in full.

731 - MINIMUM/MAXIMUM LENGTHS AND WEIGHTS

For Class 3B only there is an additional minimum weight of 170 kg for the combined weights of driver and co-driver, including racing vests and protecting helmets. The weight of the driver and co-driver are to be checked separately pre-race with dry race suits, racing vests and protecting helmets.

If the combined weight of the driver/co-driver does not reach the minimum of 170 kg, the difference is to be added to the minimum boat weight.

When checking weights of boats post race, residual fuel (spare fuel) in the tanks will be included in the minimum weight, together with all safety equipment. Slings carried in the boat during the race will count as safety equipment. The boat must be empty of water.

For all other Class 3 categories, the minimum weight mentioned is for a boat with the mentioned minimum length. For boats longer than the minimum, the weight shall be increased according to the formula shown above.

The weight is ready to race dry, including all safety equipment (except the crew and their helmets and life vests). For control after a race, residual fuel in tanks will be included in the minimum weight. Slings carried in the boat during the race will count as safety equipment. The boat must be empty of water.

732 - HULL DIMENSIONS

732.1

For monohulls, the outer surface of the hull excluding chine-rubbers and spray rails, must contain a cube of the dimensions mentioned in the 730.1 chart.

733 - MINIMUM FLOTATION

All boats shall contain a minimum volume of buoyancy permanently installed in the hull.

The minimum volumes measured in litres are mentioned in the 730.1 chart.

If a boat is constructed of wood, the wooden weight content of the boat may be accepted as buoyancy as per the following formula : Wood weight in kg x 2 = litres of buoyancy.

ENGINE ELIGIBILITY

734 - (NON EPA/CARB) OUTBOARD ENGINES

734.1

Class 3 outboard engines shall be strictly production engines such as the standard marine production engines sold by manufacturers to leisure boating (or touring) customers.

734.2

For a newly homologated engine to be eligible for Class 3, Manufacturers must certify that their motors meet the UIM requirements on noise when fitted to an appropriate boat. Engines already homologated do not require this certification. The noise level is to be measured by means of a phonometer which conforms to the specifications laid down by IEC and ANSI, a precision sound meter using the "A" weighting and set to slow response. The registering microphone is placed at a distance of 25m from the boat. The boat must pass it at top speed. Measurements must be made on open water free of all obstructions over a minimum distance of 25m in all directions. Two measurements must be carried out, while the boat approaches and while moving away. Neither of the measurements is to exceed 91db (A) with a 4db (A) tolerance.

For Mercury XR2 and 2,5 EFI the exhaust plate must be fitted.

734.3

The only outboard engines eligible for use in Offshore are those approved (at time of homologation application) by Cominoff.

734.4

No. of manufactured engines up to	1000 cc = 1000 units.
	1001-2000 cc = 500 units.
	above 2001 cc = 250 units.

734.5

An outboard engine shall be able to run at reduced speed.

734.6

The engine being placed in water, the cooling shall be effected only by its own standard circulation pump. The intake to the circulation pump may be modified for cooling purpose only. Bearing carriers and prop shafts may be changed from original. ***Bearing carriers may be fixed to gear case by additional screws.***

Refer also to 735.16 **and 735.30** for water intake definition.

734.7

An electric or manual starter must assure a quick and easy start without external aids. It shall be used as supplied by the manufacturer, no adjustment or alterations are allowed.

734.8 - ENGINE ELIGIBILITY (EPA/CARB) OUTBOARD ENGINES

EPA/CARB approved engines used in Class 3 shall be strictly standard marine production engines as marketed by manufacturers to leisure boating (or touring) customers.

734.9

At time of homologation, manufacturers must certify that their motors meet the UIM requirements regarding noise when fitted to an appropriate offshore powerboat. The noise level is to be measured by means of a phonometer which conforms to the specifications laid down by IEC and ANSI, a precision sound meter using the "A" weighting and set to slow response. The registering microphone is placed at a distance of 25m from the boat. The boat must pass it at top speed. Measurements must be made on open water free of all obstructions over a minimum distance of 25m in all directions. Two measurements must be carried out, while the boat approaches and while moving away. Neither of the measurements is to exceed 91db (A) with a 4db (A) tolerance.

734.10

The only EPA/CARB outboard engines eligible for use in Offshore are those approved (at time of homologation application) by Cominoff.

734.11

No. of manufactured engines up to	1000 cc = 1000 units.
	1001-2000 cc = 500 units.
	2001 cc and above = 250 units.

734.12

The engine being placed in water, the cooling shall be effected only by its own standard circulation pump. The intake to the circulation pump may be modified for cooling purpose only.

Bearing carriers and prop shafts may be changed from original.

Refer also to 735.16 for water intake definition.

734.13

Violation of any of the above rules will entail disqualification.

735 - MODIFICATIONS PERMITTED (NON EPA/CARB) OUTBOARD ENGINES

Only the following modifications are allowed:

735.01

The propeller is free, together with its washer(s) and nut(s).

735.02

Spark plugs may be changed for any other.

735.03

Revolution counters, tachometers, water pressure and temperature gauges may be fitted to the engines.

735.04

Thermostats of the cooling circuit may be removed.

735.05

Over-speed switches may be blocked.

735.06

Re-boring and fitting of oversize pistons is allowed as made available by the manufacturer for that model.

735.07

The steering bar and brackets may be reinforced as a safety measure. It is also permitted to modify the standard engine cowling in order to facilitate the fitting of steering assemblies, provided that any openings are sealed so the engine can not pull in any additional air.

735.08

The rubber mounts of the engine may be changed or substituted.

735.09

A spring may be added to the butterfly valve of the carburetor.

735.10

Mechanical carburetor and ignition advance controls may be adjusted out of their normal setting range as long as the original parts are retained.

Carburetor fuel jets and air jets are free if they can be changed without alterations to the carburetor body of float housing.

735.11

All external (not contained within the powerhead) studs, screws, nuts, bolts and their washers are free as well as the method of locking them, providing there is no performance advantage.

735.12

When twin engines of the outboard type are installed, but the particular type is not available with opposite hand propeller rotation, it is permitted to modify the drive of one unit to obtain contra-rotation provided that:

- the standard underwater unit casing is retained
- the final propeller ratio to the crankshaft is unaltered.

It is also permitted to change the rotation of one unit if an alternative opposite hand rotation underwater unit, or a kit of parts to modify the unit is offered by the manufacturer or his concessionaire. In that event the gear ratio of the single altered engine may vary from standard provided it is an incorporate part of the alternative unit or kit.

735.13

The use of thrust blocks mounted on the lower unit and or on the boat is permitted.

735.14

Power Trim: a properly engineered system for altering engine trim whilst under way is permitted.

735.15

In the process of modification, minor optional alternatives as listed and priced by the manufacturer of the homologated engine, may be permitted provided they do not affect engine performance, but improve convenience and safety.

735.16

The water intake may be repositioned; it must however remain on the gear case torpedo. The width or height of the gear case torpedo must not be changed. The

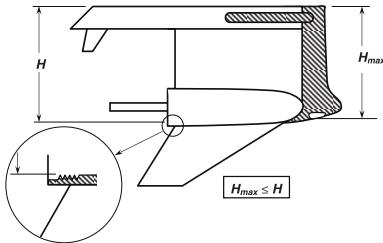
length may be increased. The rear edge of the water intake opening must not be further back than 30 % of the total length of the modified gear case torpedo.

The original intake to the circulation pump may be filled in and faired. The front of the gearcase may have material added for cooling purposes.

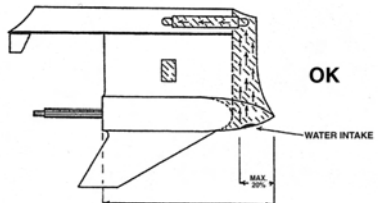
The width of the gearcase must not be increased, except where the gearcase meets the lower side of the anti-cavitation plate.

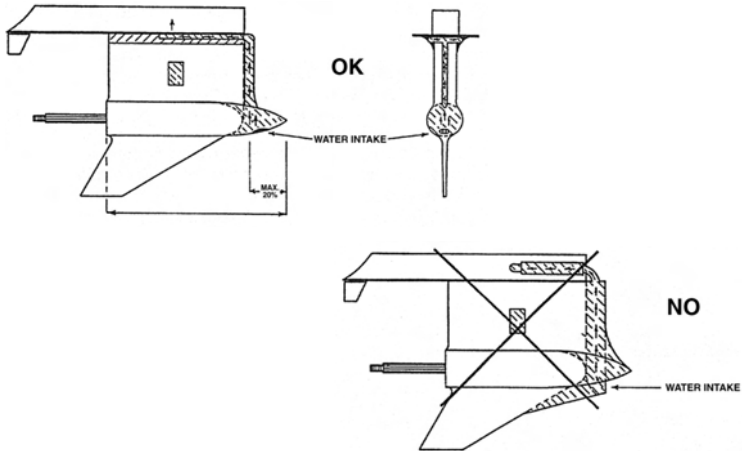
The modification shall be for cooling purposes only and shall form a radius. It is also permitted to add material to the centre section exhaust housing for the purpose of reducing noise.

Remote water pick-ups are not allowed.



Method of measuring maximum depth of water intake.



**735.17**

Lighter flywheels as made available by the manufacturer may be fitted.

735.18

Transom brackets may be reinforced, this is only by adding material.

735.19

The fuel connector in the lower cover may be removed and the fuel hose from the fuel tank connected directly to the fuel pump. If this is done, the opening left after the connector and around the fuel hose must be sealed to prevent extra air entering inside of cover.

735.20

It is permitted to replace lubricating systems using oil tanks and oil pumps with lubrication oil mixed into the fuel. Caps or covers in the motor cover cannot be removed to provide extra air inlet.

735.21

Measurements not quoted in the homologation sheet may not be altered. If a detail cannot be verified with the aid of the pictures in the homologation sheet, the part in question can be compared to a corresponding stock part. In such a comparison it is only possible approximately to use the comparable measurements as the manufacturing tolerances are not available. A component may be machined in order to reach a specific dimension, which is quoted in the engine homologation file. A component for which the weight is quoted in the homologation file may be adjusted down to the weight, by machining or grinding provided that the area where metal is removed is the same as is used by the engine manufacturer.

This is known to be at the top or at the bottom, or both for piston connecting rods. Balancing of one or all piston connecting rods is permitted, as long as this is in accordance with manufacturer's procedures. Polishing of connecting rods is not permitted. When the volume of the combustion chamber is specified in the homologation sheet, the cylinder head may be faced to achieve the volume allowed.

It is allowed to add material to the leading edge of the gear case skeg.

735.21.1

Violation of any of the above rules will entail disqualification.

MODIFICATIONS PERMITTED (EPA/CARB) OUTBOARD ENGINES

735.22

The propeller is free, together with its washer(s) and nut(s).

735.23

The steering bar and brackets may be reinforced as a safety measure. It is also permitted to modify the standard engine cowling in order to facilitate the fitting of steering assemblies, provided that any openings are sealed so the engine can not pull in any additional air.

735.24

The rubber mounts of the engine may be changed or substituted.

735.25

All external (not contained within the powerhead) studs, screws, nuts, bolts and their washers are free as well as the method of locking them, providing there is no performance advantage.

735.26

Where two engines are fitted, the counter rotation of one underwater unit is permitted providing:

- the standard underwater unit casing is retained (subject to 735.30)
- the final propeller to crankshaft ratio is unaltered

735.27

The use of thrust blocks mounted on the lower unit and or on the boat is permitted.

735.28

Power Trim: a properly engineered system for altering engine trim whilst under way is permitted.

735.29

In the process of modification, minor optional alternatives as listed and priced by the manufacturer of the homologated engine, may be permitted provided they do not affect engine performance, but improve convenience and safety.

735.30

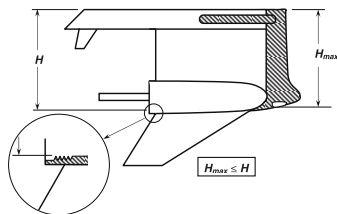
The water intake may be repositioned. It must however remain on the gear case torpedo. The width and or height of the gear case torpedo must not be changed **and the depth of the torpedo cross section from the aft part of the water intake and forward is not allowed deeper than measure H.** The length may be increased. The rear edge of the water intake opening must not be further aft than 30 % of the total length of the modified gear case torpedo.

The original intake to the circulation pump may be filled in and faired. The front of the gearcase may have material added for cooling purposes.

The width of the gearcase must not be increased, except where the gearcase meets the lower side of the anti-cavitation plate. The modification shall be for cooling purposes only and shall form a radius. It is also permitted to add material to the centre section exhaust housing for the purpose of reducing noise.

Remote water pick-ups are not allowed.

It is allowed to add material to the leading edge of the gear case skeg **up to 10 mm behind a repositioned water intake. The profile of the skeg is free.**



735.31

Transom brackets may be reinforced, this is only by adding material.

735.32

- a) The fuel connector in the lower cover may be removed and the fuel hose from the fuel tank connected directly to the fuel pump. The remaining void must be sealed to prevent any extra air entering the cover.
- b) ***Sacrificial anodes on gear house and transom bracket may be removed. Trim fin under the anti-cavitation plate may be removed or cut off.***

MODIFICATIONS NOT PERMITTED (EPA/CARB) OUTBOARD ENGINES

735.33

The following modifications are expressly NOT permitted.

Blueprinting' of the powerhead.

Alteration or modification of any powerhead components.

Removal of material from the powerhead for any reason other than recognised (by the manufacturer) repair.

ECU/PCM's equipped with data memory, must not have the memory erased or modified without the prior permission of the Technical Inspector. Any information sourced by the Technical Inspector from the ECU/PCM's race data memory which does not correspond with the technical data declared by the manufacturer in the homologation file will be viewed as reason for disqualification.

735.34

Violation of any of the above rules will entail disqualification.

800 - GENERAL RULES FOR OFFSHORE WORLD AND CONTINENTAL CHAMPIONSHIPS (EXCEPT CLASS 1 WORLD CHAMPIONSHIP)

800.1 - WORLD AND CONTINENTAL CHAMPIONSHIP

There can only be one World Championship per year for each class.

There can only be one Continental Championship per year for each region and for each class.

800.2 - REQUEST TO ORGANISE

The request to organise the Championship event must be made by a National Authority and be submitted to the UIM before the stated deadline date. The request will be considered by Cominoff and after approval, entered on the UIM Offshore Calendar as a titled meeting.

800.3 - MINIMUM PARTICIPATING

For a World Championship event to be valid, there must be a minimum of ten participating entries in the class from three Nations (unless otherwise agreed by Cominoff).

For a Continental Championship event to be valid, there must be a minimum of five participating entries in the class from two Nations (unless otherwise agreed by Cominoff).

Entries are to be sent by the National Authority to the organising club, no later than 7 days before the beginning of the Championship.

“Participation” means:

- for the crew, passing administrative control and signing the attendance register at the Driver’s Briefing.
- for the boat, passing the Technical Controls.

800.3.1 - N° OF REGISTERED BOATS

There should be at least 25 boats in that class in the previous year and the boats should be actively raced in at least 3 countries before Cominoff will award a World Championship.

There should be at least 15 boats in that class in the previous year and the boats should be actively raced in at least 3 countries before Cominoff will award a Continental Championship.

Cominoff may, in order to support new categories, give special approval for a Continental or World Championship.

800.4 - WITHDRAWN ORGANISER

If a National Authority, having been granted a Championship for which it applied, subsequently withdraws or is unable to run the Championship, Cominoff, if time permits, will re-offer the Championship to other National Authorities.

If the Championship cannot be run by another National Authority, the Championship is cancelled for that year.

800.5 - DRIVERS AUTHORISATION

Each National Authority must authorise, in writing, its drivers to participate in any Championship event.

800.6 - UIM COMMISSIONER

A UIM Commissioner is to be appointed by Cominoff to every International Championship event.

The UIM Commissioner shall at World Championship races have a "UIM Open Forum" to exchange views between race teams, organizers and UIM and to improve communication.

Time for the Forum shall be included in the official Time Schedule by the organizer.

800.6.1 - WORLD CHAMPIONSHIP - 3 HEATS

A World Championship will consist of 3 heats at one venue unless otherwise agreed by Cominoff.

800.6.2 - CONTINENTAL CHAMPIONSHIP - 2 HEATS

A Continental Championship will consist of 2 heats at one venue unless Cominoff has agreed to a series.

800.6.3 - CANCELLED RACE

If in the case of a scheduled two race series, only one heat can be run, this will be a valid result. If a three heat or more series, then the result may be decided upon two heats, if a heat is cancelled through 'force majeure'. The Race Committee should ratify the cancellation if through 'force majeure'. If a heat is not run on its scheduled day or its scheduled lay day(s), then it will be cancelled.

800.6.4 - N° 1 DRIVER

The No.1 Driver must control the boat in some way, either as driver or throttleman in all heats.

800.6.5 - N° 2 DRIVER

The title will be declared as a Driver No.1/Driver No.2 combination if the No.2 Driver has completed all heats of the championship with the No.1 Driver.

800.6.6 - RACE LENGTH

The races must conform to the following length:

WORLD CHAMPIONSHIPS:

Class 3A 1 x 50 - 70 nm 2 x 30 - 50 nm

Class 3B 1 x 50 - 70 nm 2 x 30 - 50 nm

Class 3C 1 x 70 - 100 nm 2 x 45 - 70 nm

Class 3S 1 x 70 - 100 nm 2 x 45 - 70 nm

CONTINENTAL CHAMPIONSHIP - one venue/multi - each race must be:

Class 3A 1 x 50 - 75 nm 1 x 30 - 50 nm

Class 3B 1 x 50 - 75 nm 1 x 30 - 50 nm

Class 3C 1 x 70 - 100 nm 1 x 45 - 70 nm

Class 3S 1 x 70 - 100 nm 1 x 45 - 70 nm

There may be a slight variance to these rules to allow for local situation and these must be agreed by Cominoff or at an event by the UIM Commissioner.

800.7.1 - BAD WEATHER COURSE

Should bad weather compel the organisers to have any race run on a shortened course and/or an alternative start time, points are granted provided:

That a minimum of 1 hour notification, prior to the start time, of the amendment to the course or the start time is given to each driver in a Race Bulletin. A register must be signed by every driver confirming receipt of the Race Bulletin.

Should the race be stopped for any reason of 'force majeure', the boats will be classified according to 312.01, providing 50% of the minimum race distance set out in 800.13 has been completed. If this distance has not been completed, no points will be awarded.

800.7.2 - STOPPING AT THE RACE

The organisers may stop the race at some checkpoint of the route before the official finish, and/or if the race has been run more than 30 nautical miles, and/or the drivers have been racing for at least 1 hour, the race will be approved as a Championship race. The curtailment decision must be ratified by the Race Committee immediately after the race.

800.7.3 - CURTAILED RACE

A curtailed race is a race shortened by the Race Committee after the start.

The signal to be used to announce it is waving the chequered flag (Ref. 312.01).

800.8.1 - MINIMUM LAP DISTANCE

Should any part of the course form a lap of less than 6nm for Class 3 ; Such a lap must not be repeated more than twice in a World Championship or a Continental Championship, unless it is approved by Cominoff or the appointed UIM Commissioner for the event.

Deviation from these rules is allowed only when the race is run on a shorter (storm) course as described in the Race Instructions in which case there is no minimum length.

800.8.2 - WATER DEPTH

The race courses run, including any rough weather course, should have a minimum water depth at all times of racing or pre-race testing. For all classifications, the minimum depth is 4 metres.

800.9.1 - INTERVAL BETWEEN RACES - WC

In a World Championship - If the whole event takes place from a single race site then a scheduled interval of at least 22 hours, but not to exceed 70 hours between each heat is compulsory. However there has to be one lay day during the event. Should a race be re-scheduled, the actual interval of at least 20 hours, but not to exceed 60 hours between each heat is compulsory.

800.9.2 - INTERVAL BETWEEN RACES - CC

In a Continental Championship - If the whole event takes place from a single race site then a scheduled interval of at least 20 hours, but not to exceed 60 hours between each heat is compulsory. Should a race be re-scheduled, the actual interval of at least 20 hours, but not to exceed 60 hours between each heat is compulsory.

800.9.3

If a series Championship is based on several race sites the calendar is to be as agreed by Cominoff.

800.9.4 - LAY DAY

There must not be a lay day after the final scheduled race day.

800.10 - POINT ACCUMULATION

In a World or Continental Championship, the results will be determined from points accumulated from all the heats in accordance with UIM rule 321. If at one venue there will be two or three heats, if not at one venue the number of heats will be determined by Cominoff when they agree to the calendar. It is not compulsory to participate in all heats. The named driver must drive the same boat throughout the series. A change of boat will mean a new entry into the Championship, freezing points accumulated prior to the change.

The Boat is defined as a Hull/Engine configuration as stated on the Measurement Certificate.

For events with more than 20 entries, the following bonus points should be given, so that all drivers can get points.

21 - 25 boats : 5 bonus points

Position 1) to 20) : should all have 5 extra bonus points

Position 21 : 5 points

Position 22 : 4 points

Position 23 : 3 points

Position 24 : 2 points

Position 25 : 1 point

26 - 30 boats : 10 bonus points

Position 1) to 20) : should all have 10 extra bonus points

Position 21 : 10 points

Position 22 : 9 points

Position 23 : 8 points

Position 24 : 7 points

Position 25 : 6 points

Position 26 : 5 points

Position 27 : 4 points

Position 28 : 3 points

Position 29 : 2 points

Position 30 : 1 point

31 - 35 boats : 15 bonus points

Position 1) to 20) : should all have 15 extra bonus points

Position 21 : 15 points

Position 22 : 14 points

Position 23 : 13 points

Position 24 : 12 points

Position 25 : 11 points

Position 26 : 10 points

Position 27 : 9 points

Position 28 : 8 points

Position 29 : 7 points

Position 30 : 6 points

Position 31 : 5 points

Position 32 : 4 points

Position 33 : 3 points

Position 34 : 2 points

Position 35 : 1 point

36 - 42 boats : 20 bonus points, etc...

800.11 - POST-RACE INSPECTION

Engines and engine parts may be replaced between the races. The Race Committee must be informed and have given its allowance in advance.

Replaced engines or parts shall be handed over to the Technical Inspectors, or be sealed by them. If an engine is to be inspected after the last race, the replaced parts must be inspected also.

All dimensions must be measured with the engine at ambient temperature.

Refusal to present an engine or an engine part for post-race inspection will result in immediate disqualification.

ECU boxes may be sent for special checking by the engine manufacturers authorised service agent. If so, the race results will be provisional until the organisers receive the results of the checking.

The title of champion is only granted after a satisfactory post-race inspection of the winning boat's equipment, hull and engine(s).

Should the boat fail and is deemed to be disqualified, the next placed boat is then inspected.

After each heat in a multi venue series or after the final heat of a single venue championship, the first three boats must be inspected. If found illegal, then the 4th boat should be inspected until the Race Committee can announce a 1st, 2nd and 3rd which have been found to be correct.

At all Continental and World Championship events hosting 'low emission' engines a technical representative must be enlisted to provide electronic verification of legality to the Chief Technical Inspector of the event. Except in cases of force majeure. The representative must report exclusively to the Chief Technical Inspector.

800.12 - TIE

If a tie exists in the final points accumulation then the winner of the longest heat will win. If this cannot split the two, then the boat with the fastest speed in any heat will win.

800.13 - PROTESTS

All Protests will be handled by the normal UIM Jury procedure by the N.A. running the heat from which the protest arises.

800.14 - REPORT TO UIM

Immediately after the completion of the final heat (if one venue) or after each heat, if multi venue series, but not later than 6 hours after the results are out of protest time, the Race Secretary must fax/email the following to the UIM office:

- a) Entry list with information of: Crew names, Nationality, Boat name, Boat race number, Hull manufacturer and type, Engine manufacturer and type.
- b) Results from all heats if single venue or the heat and the relative standings in the championship.
- c) The results should be typed and show distance run, times and speed in knots and must be signed and time dated by the O.O.D.

The above is to be e-mailed or faxed to the UIM office on + 377 92 05 04 60. Proof of transmission should be given to the UIM Commissioner.

800.15 - CONFIRMATION OF UIM

The points allocated will be confirmed by the UIM on receipt of the UIM Commissioner's report.

800.16 - ENTRY FEE - PRIZE MONEY

The National Authority shall guarantee an amount to UIM as agreed with Cominoff, for each race of the UIM Championship. If there is no transportation and prize money made available, no entry fee shall be charged to the visiting nations participating.

800.17

Rules and procedures from blue pages may be used for Class 3 Championship events provided they are properly announced in either Advance Programme or Race Instruction, after being ratified by the UIM Commissioner.

855 - ADVISORY REQUIREMENTS FOR UIM CHAMPIONSHIP EVENTS - SECURITY AND LOGISTICS

Recommended minimum requirements for UIM Championship events - security and logistics.

COURSE

The milling area and starting chute must comply with the relevant UIM Offshore Rules. It must be ensured that these areas are clearly marked and that all drivers are provided with written briefing describing the exact location of these marks. Immediately before and during the start, the race organisation must provide suitably identified patrol craft to keep these restricted areas clear of all spectator craft. The Starter and the Safety Officer must both receive confirmation that the milling area and the starting chute are clear before the start procedure can begin.

All turning points of the course must be clearly identified and have a control boat in close proximity, but not closer than 50 m from the turning mark. If a permanent navigational mark is not utilised, then the mark will be a temporary clearly identifiable coloured buoy that is recommended to be bigger than 1.5 m high X 1

m diameter, anchored in position. A control boat must not be the turn mark. All marks shall be provided with compass co-ordinates in the written instructions.

Where the course has a turning point presenting an angle less than 90 degrees, two orange marker buoys must be utilised, anchored at a minimum distance of 100 m apart.

The distance between the starting line and the first turning buoy must be at least one nautical mile. In any start where there are ten or more boats starting together, the distance between the starting line and the first turning buoy must be at least two nautical miles.

SAFETY ON RACE COURSE

Safety procedures that must be provided during races and official practise periods are :

- 1) The provision of, and organisation of well experienced rescue teams responsible for the recovery of drivers and/or persons involved in a race accident.
- 2) Availability of adequate rescue equipment that has been maintained and tested.
- 3) All emergency units in the vicinity of the event must be notified. These include hospitals, police/coast guard and fire services.

The provision of the rescue facilities applies primarily to persons. It does not take into consideration the recovery of race boats or equipment.

The task of the rescue teams is to maintain a watch of the complete race course or practise area during a race and/or official practise periods. Rescue teams may be placed on boats or in helicopters.

Safety briefings must take place between all persons responsible for safety and key personnel must attend the drivers briefings.

SAFETY DURING TESTS

The course for practise tests must be well defined as follows :

- defined on a nautical chart of area distributed with Race Instructions or at Drivers Briefings;
- defined by marker buoys as utilised for race course, i.e. 1.5 m high x 1m diameter buoys.

- course area to be no more than 4 Nm x 1 Nm
- patrols boats, always moving, must be positioned along the course in order to patrol a surrounding area of 2 Nm x 2 Nm ; in any case the area will be the maximum area that can be entirely visible with the naked eye at 2 m above the sea level;
- patrol boats during the whole practise session must be in contact (by VHF) with race control and they must have on board an expert crew including scuba divers;
- a paramedic trained in advanced life support in contact with the Race doctor must always be on a patrol boat;
- during the practise tests, a heli-ambulance must be on alert, in the proximity of the race course, in case of accident, for the transportation of seriously injured person(s) to a specialised hospital.

SAFETY DURING RACE

Patrol and Control Boats

Patrol boats must be positioned around the complete course, which should be divided into a grid pattern. The patrol boats must be each allocated an area of approximately 2 Nm x 2 Nm, which should not exceed naked eye visibility at sea level. All control boats and patrol boats must be clearly identified and should have the authority to keep spectator boats clear of the course.

Control boats must take a safe position, where possible inside the course, avoiding screening the mark buoy(s) from competitors.

Control boats must be limited to carrying officials and crew.

Patrol boats must be capable of rescue work. They therefore must be large enough to stay at sea if conditions should deteriorate and be able to receive 2 laid stretchers. At least three of these boats should have trained crew which shall include two divers and a paramedic trained in advanced life support. They should carry rigid stretchers.

These boats must be capable of maintaining constant communication with race control and shall have on board :

- red flags
- strong towing lines
- boat hook

- fire extinguishers for fuel fires
- first aid kit

PIT AREA

A security team must be provided 24 hours at the Dry Pits, the Wet Pits and Fuelling area during the event.

An adequate Dry Pit area shall be provided for each boat with sufficient area available for :

- 1) Service centre.
- 2) Parking for trailer/tractor units.
- 3) Organisers must provide an adjacent area for team cars and/or motor caravans.

Recommended : The boat parking area shall be minimum 25 m x 5m/boat; the truck parking area shall be minimum 20m x 4 m/truck.

The Race Organisers must provide an authorised landing area for helicopters. The Organisers must also provide two ambulances complete with specialised medical personnel for the duration of the event in the proximity of the landing area and the pit area.

For all offshore classes, a wet pit area suitably protected and with pontoon berthing should be provided for berthing of emergency boats, or other alternative berthing area be provided that is suitable for the transfer of injured persons to an ambulance.

An electrical supply capable of powering power hand tools and for general illumination of the area shall be available.

A high pressure water supply shall be provided for the washing of boats and flushing of engines.

POST RACE SCRUTINEERING AREA

A closed under cover area capable of being closed to unauthorised persons for the purpose of engine inspections by the scrutineers shall be available.

CRANAGE

Cranage must be provided at a suitable location as close as possible to the wet pits. Crane area must be cordoned off.

There should be a crane and operator or a slipway for every 15 boats.

At least one crane should operate during test periods.

At the cranage area there should be available a high capacity water suction pump with suitable flexible suction piping attached for the purpose of assisting a swamped boat.

At least two divers should be in attendance.

Access to the cranage area shall be restricted to authorised persons only.

FUELLING AREA

A designated compound should be allocated for fuelling. This compound should be within reasonable proximity to the pit area. An adequate supply of best available commercial grade gasoline and diesel must be available to every competitor. The fuelling facility should be readily available to all competitors as required.

Adequate fire protection equipment must be available with trained operators at all times when fuelling facility is open.

The fuelling area must be barricaded from the public and unauthorised persons with clearly visible warning signs displayed. All local hazardous goods regulations must be met.

All transference of fuel must be done within the fuelling compound.

The Race Organisers must make available a secure area complete with the equipment necessary to empty boat fuel tanks after testing or racing. This is in the interests of safety and also respecting the environmental rules.

To cater for the prevention of pollution at the race site, the following facilities should be provided within the fuelling compound:

- One 200 litre container for gasoline.
- One 200 litre container for oil.
- One 20 litre container for thinners with halogens.
- One 20 litre container for thinners without halogens.

- A bin for empty cans etc.
- A bin for discarded batteries.

RACE CONTROL

Race control should consist of modules. These primarily being:

- 1) **A Race Control Control Room** restricted to the race organising committee and UIM Commissioner(s). All communication to other authorities should originate from here.
- 2) **A Radio Control Room** restricted to the operating personnel and other officials such as the Safety Officer and the Officer of the Day. These two rooms should be in close proximity and have constant communication between them.
- 3) **Timekeepers.** Their module must have viewing of the course at the start/finish line. No other personnel should be permitted entry whilst a race is in progress.
- 4) **First Aid Room.** This module should be a base for the medical personnel. During the race, a qualified medic shall be present and an ambulance shall be in attendance or be available for immediate response. The room should have sufficient facilities to enable it to be used for trauma treatment. It must have direct communication with the control room and have telephone facilities.
- 5) **Briefing Room.** This module must provide seating for all crew members and officials required to be present for briefings and drivers meetings. It should have a public address system and if foreign drivers are participating, an interpretation service must be provided. Unauthorised personnel should not be permitted entry during briefing sessions. All briefings should be recorded on video and audio.
- 6) **Private Meeting Facilities** should be available for the purpose of providing private consolation for family members or friends in the event of an accident, or to conduct enquiries.
- 7) **Toilet and Washing Facilities** must be readily available for all crew members in close proximity to the pit areas.
- 8) **Security** must be provided 24 hours in the dry pits, the wet pits and the fuelling area during the event.

PRESS

At all Championship events a Press Centre shall be provided. This centre shall provide telephone, fax facilities, internet connections and a photocopier.

This centre shall include conference facilities for conducting press releases and for interviews with successful teams and/or personalities.

The size of the above facilities shall be in accordance with the number of entries and the importance of the event

The press room should be restricted to authorised press personnel, officials and successful race crews.

It is recommended that adequate hospitality be extended to all bona-fide members of the press.

V.I.P. HOSPITALITY

Organisers should be aware of the presence of and provide hospitality for VIP s including International UIM Officials and their companions. It is recommended that a covered area be provided for such persons to meet, collect current race information and have warm beverage available. Complimentary passes should be issued to these persons.

PROMOTIONAL ACTIVITY

Race Organisers should ensure that their event is adequately advertised to encourage maximum exposure to the general public.

The design of the race courses should be made considering the availability of suitable vantage points for people to view the races.

A public speaker system should be arranged to provide the viewing public with a complete commentary of the race progress and any other associated activities.

950 - U.I.M. V24 CLASS

951 - GENERAL

951.1

The general U.I.M. offshore racing rules are to be applied. In cases of conflicts between general rules and class rules (and specifications), the class rules have precedence.

ADMINISTRATION

The V24 Class Working Group consists of a minimum of one representative from U.I.M. the designer and the V24 licenseholder.

951.2 - SPIRIT OF THE RULES

The aim is to provide an offshore racing class in which driving skill is more important than the opportunities to take advantage of the latest advances in the use of exotic building materials, the art of boatbuilding and in the art of extracting still more power from highly stressed machinery. Boats are to be alike, no modifications (tuning) are allowed except the few points mentioned below.

The V24 Class is modern, has unique safety features, has a reliable serial produced power unit and is intended to provide good sport with a minimum of maintenance and repairs.

951.3 - MEASUREMENT CERTIFICATE

V24 Class boats are to have a measurement certificate according to rule 501.

The complete boat, hull, equipment, as well as engine and transmission must during races remain as delivered from the boat builder and as specified in the U.I.M. homologation. Sole modifications allowed are described below.

The measuring procedure consists of checking that the boat and machinery comply with the specifications in the homologation file.

Instructions for how this is done are to be found in the file. It is obligatory for a boat owner to:

- Have the homologation file at hand both before the measuring procedure and at the measuring procedure.
- Lend the homologation file (or a copy of it) to the measurer, at least one week in advance, in order to give the measurer an opportunity to acquaint himself with what he has to check and how to do it.

951.4 - U.I.M. V24 CLASS PLAQUE

In order to obtain the measurement certificate necessary for a V24 Class boat, it must be built by an U.I.M. certified boat builder and carry the U.I.M. plaque fixed to it by the builder.

Boats not having a U.I.M. V24 Class plaque are not to be measured, and are not to have a measurement certificate, ***unless scrutineers can determine that the boat is a real and genuine V24.***

The U.I.M. V24 Class plaque is available to the boat builders licensed by the V24 Class Working Group. The plaque is available from the V24 Working Group.

For boats that do not comply with the V24 Class rules, U.I.M. general rule 502.01.04 on eligibility is applicable.

951.5 - HOMOLOGATION FILE

The homologation file is to be distributed by U.I.M. and National Authorities in the same way as other homologation files.

To be valid, a homologation file must have an original U.I.M. or N.A. (not photocopied) stamp on every page.

951.6 - CHANGES OF THE HOMOLOGATED SPECIFICATIONS

Changes of the homologated specifications must be approved by the V24 Class Working Group and are not allowed until one month after they are published in a U.I.M. bulletin.

Changes in spare parts are allowed ***since the engine manufacturer has since the start modified some parts during these years, but with no improvement of performance. The driver at races must be able to show that parts changed actually are manufactured OEM parts and will not give a performance gain.***

Changes of class rules must be approved by the U.I.M. General Assembly and can not be valid until published in a U.I.M. offshore rule book or in a widely distributed supplement to a rule book. Even so, a rule change is not to be implemented until the calendar year after the year in which it was approved by the U.I.M. General Assembly.

951.7 - OWNERSHIP OF BOATS

All boats entitled to race must be owned by a physical person, a group of persons or a legal entity. The owner is stated in the measurement certificate.

951.8 - COURSES

V24 Class boats are intended to race in open and unprotected waters as U.I.M. Class 3 boats and courses of similar but not necessarily the same to the OCR type according to rule 900.16.

951.9 - ECM BOX

Technical inspectors are allowed to switch (by lottery) the engine ECM box between boats.

The V24 race committee reserve the right to switch ECU's and or propellers from any boat it chooses.

952 - MODIFICATIONS ALLOWED

952.1

The Volvo 5,7 GXI engine may not be modified in any way, including the ECU. It is allowed to change bolts, nuts and washers. The Volvo Penta DPX outdrive must have a 1:1,59 gear ratio in it. No modification is allowed to the out drive including the exterior of the drive. The height of the out drive may not be altered from manufactured.

Only the following modifications are allowed.

952.2

Sparkplugs may be changed for any other.

952.3

The propellers may not be hidden from competitors at the race venue when fitted to the out drive. The propellers to be only Volvo Penta E2 duo props, these may be modified. The Volvo Penta E2 markings must be clearly visible on each propeller.

952.4

It is allowed to fit flat anchor plates for bolting the dash to the deck and the fitting of aerials are allowed.

952.5

Internal cockpit arrangements are free as long as both crew members have free and unobstructed exit to both doors. No sharp edges or protrusions are allowed anywhere.

Cockpit seats, steering wheel, instruments, choice of power trim button, placement of dashboard, throttle and footrests are free. Cockpit seats as per U.I.M. rule 508.20.

952.6

On-board cameras are free as long as they do not interfere with visibility.

952.7

An air-conditioning system may be fitted.

952.8

The replacement of **bolts**, nuts, or the addition of lock nuts, drilling and wiring, split pinning or keying is allowed, the screws and pins do not need to be OEM parts, as long as no performance gained is achieved.

952.9

It is allowed to fit water drain off's from the engine exhaust manifolds - no performance gained - only available from the V24 builder.

952.10

It is allowed to fit a water pickup to spray water onto the top of gear case only - no performance gain - only available from the V24 builder.

952.11

It is allowed to fit hard engine mountings - no performance gain - only available from the V24 builder.

952.12

It is allowed to add rubber hose to the end of the exhausts.

952.13

It is allowed to fit exhaust couplings - only available from the V24 builder. No performance gain.

952.14

Any engine lubricant or drive lubricant may be used.

952.15

Boats running "silent exhausts" are allowed to compete in all V24 events, provided all other specifications are met.

952.16

It is allowed ***with the engine compartment*** to fit an oil cooler of any kind, necessary adapter and oil filter re-locator, as long as no performance gained.

952.17

It is allowed to add a support / brace to the exhaust to help them take up shocks from running in rough conditions.

952.18

It is allowed to mount the detachable nose and wing tips permanently on to the boat.

952.19

Placement of bilge pumps, batteries, power trim pumps & fuel filter is free within the engine compartment.

953 - FUEL

953.1

Fuel regulations according to U.I.M. rule 504.

953.2

Fuel may be supplied by a competition or event sponsor. It is a requirement that all boats use the fuel supplied. Testing should take place and boats found not to be using the supplied fuel will be disqualified.

954 - IDENTIFICATION

954.1

Class identification letter is V. Number is same as the U.I.M. V24 Class plaque number. However, if the race organiser thinks it is necessary, he has the right to order a number to be changed.

Size and positions of the race number according to U.I.M. rule 710.

955 - SAFETY

955.1

The U.I.M. V24 Class boat is an approved one-design class, consequently rules 508 to 508.07 do not apply.

Boats must always comply with the homologation sheet.

Each boat will be weighed at the end of each race. Every boat must not weigh less than 1490 kg, including the crew, boat, safety gear and balance of fuel, but not water ballast and excluding strops. The teams needing to add weight must either permanently fix it under the seats in the cockpit, or under the cockpit liner or must carry extra fuel.

955.2

All V24 Class boats must have the detachable nose painted red/orange with the number 2 on each side of the lower running surface. The number must be written in white and be at least 0.07 m high.

955.3

Canopy hatch handle and release mechanism must be painted fluorescent orange to identify them, both inside and outside the cockpit. The text “ _ OTHER SIDE _ “, written in red must be clearly visible on both doors from the outside.

955.4 - ENGINE CUT-OUT

The engine cut-out must work on engine circuits only as a complement to the ignition key switch. It must be possible to operate the switch from the outside. The text “ _ Engine Cut-Off _ “ written in red/orange must be clearly visible from the outside.

955.5 - RESTRAINT SYSTEM

The restraint system shall be according to U.I.M. rules 508.21 to 508.22. Attachment points shall be as shown in the homologation specification.

955.6 - REAR VIEW MIRRORS

All V24 Class boats must have two rear view mirrors fitted. Mirrors must have a minimum area of 60 cm These may be of any design.

955.7 - RACING VESTS, HELMETS AND AIR SUPPLY

Racing Vests, helmets and air supply shall be according to U.I.M. rules 205.06, 713 and 508.16.

955.8

Flares (excluding personal flares), anchor and warps must all be carried in the safety box in the bow.

955.9

It is a U.I.M. requirement that the overtaking boat will give way, and this rule will be firmly enforced.

Any contact between boats during a race will mean that the boats involved will be penalised, and this may include disqualification. This includes the claimed innocent party.

956 - EQUIPMENT

956.1

Safety equipment according to U.I.M. rules 715.01 through to 715.15.

956.2

For U.I.M. V24 Class boats, it is permitted to use electronic positioning fixing systems. It is forbidden to interface any of this equipment to the steering system.

957 - CREW

957.1

The minimum age of the Driver is 18 years old. The minimum age of the navigator is 17 years old. Number of crew members is 2.

958 - ENGINE SEALS

The engines are factory sealed. A broken or missing seal may render in the engine to post race inspection. If a seal is broken in connection with an engine repair, the engine must be checked against the rule requirements by a National Association

Technical Scrutineer or the appointed V24 Technical Scrutineer when the engine is re-assembled and re-sealed by him. The kind of seal used must be noted in the measurement certificate.

959 - CONTINENTAL/EUROPEAN CHAMPIONSHIPS

The European Championship will consist of two heats at one venue unless Cominoff has agreed to a series.

The U.I.M. published calendar announces the dates and venues.

The 70% rule does not apply to this class for European Championships. The Championship points system will include all boats that cross over the start line.

Boats that do not finish a heat will be awarded points in relation to their position at the end of the race, excluding disqualified boats.

The point system for each race will be 400 points for 1st place, reducing by 15 % for each subsequent position, rounded down to the nearest whole number.

Boats disqualified receive no points.

A U.I.M. Commissioner will be appointed to each Championship heat.

V 24 boats must be positioned together in the pits area.

Smart team clothing must be worn to drivers briefing.

Race suits must be worn to the podium presentation for podium teams.

V24 Championship sponsors must be displayed on all boats. This may be on top of the canopy.

960 - RULES FOR UIM PROVEE CLASS

This section has been deleted from the rulebook since there are no longer international races in this class. If required, the rules are available at the UIM secretariat.

1000 - UIM CLASS 3 - 225 OFFSHORE RULES

The title of the Championship will be known as the UIM Class 3 - 225 Offshore Powerboat Championship.

1000.1 - GENERAL

The general UIM offshore racing rules are to be applied. In cases of conflicts between general rules and class rules (and specifications), the class rules will prevail.

1000.1.01 - SPIRIT OF THE CLASS

The ultimate aim of this Class, is to create a very competitive racing formula by the use of similarly powered, **strictly** standard engines, mounted on the same weight but different design boats. With the use of a single outboard engine and a minimum weight, which allows simpler, cheaper construction materials to be used, the cost of racing is kept to the minimum. The selection of propellers, the design differences of the boats, along with the abilities of the drivers will be the determining factors on winning. The boats will have all the necessary safety features available today.

1000.1.02 - WORKING GROUP

For the purpose of ensuring professionalism to the 3/225 class, a Working Group will be nominated by Cominoff.

The Working Group will be composed of 1 member representing UIM Cominoff , 1 representative of the Organiser, 1 drivers representative. The Working Group :

- will propose the Championship International calendar for approval
- will decide all rules for this class , which must be ratified by Cominoff
- will decide on any dispute concerning any question on the rules.

1000.2 - CHAMPIONSHIP

1000.2.01 - CALENDAR

The number of the races to form a Championship should not be less than six (6).

Once the compiling of the Race Calendar is done, the inscription deadline date will be announced.

1000.2.02 - INSCRIPTION FEE

Permission to participate to the Championship is only delivered following the payment of the inscription fee. No permission is to be granted to the participants in arrears of payment.

The inscription fee will be decided and announced yearly. The registration fee is to be doubled for any inscription requested after the deadline date set by the organisation committee.

All teams are required to give an irrevocable letter of guarantee in the amount of 5.000 Euro at the time of registration.

1000.2.03 - PARTICIPATION RULE

It is mandatory for each entrant to participate in all races of the C 3 - 225 to accumulate points unless otherwise agreed in writing by the organiser. The organiser will decide the penalty which may be a fine of minimum 5.000 Euro and maximum 10.000 Euro.

1000.2.04 - SCORING SYSTEM

Unless otherwise specified, the following scoring system will be adopted:

POSITION	POLE POSITION	RACE
1st	20	200
2nd	15	150
3rd	12	120
4th	9	90
5th	7	70
6th	5	50
7th	4	40
8th	3	30
9th	2	20
10th	1	10

1000.3 - PROMOTIONAL RIGHTS

The Organisation Committee holds the promotional rights to front 1/3 of every race boat participating in the Championship. The required area should be kept clean of any advertising or similar. Country regulations on cigarette and alcohol advertising will be enforced.

1000.4 - RACING TEAM AND LICENSE

A team must be composed of two drivers, one reserve driver / team manager and a PR responsible. All three drivers must hold an International Racing Licence.

At any titled event, each boat must designate its representative for use in case of an accident or force majeure.

1000.4.01 - DRESS CODE

Every team member must wear a team uniform / short / shirt during the race weekend. No open shoes allowed, penalty 100 Euros. The organization Committee will announce a Dress Code Regulation for each year at the start of the championship.

1000.5 - THE RACE

1000.5.01 - COURSE

The course will not be less than 2.0 Nm and can be repeated.

If a long lap will be foreseen, it can be 0.5 Nm longer than the regular course.

1000.5.02 - DEMOLISHING A BUOY

If a boat hits a buoy on the correct side, no action will be taken. If a boat goes over and/or demolishes a buoy, the team will be penalised by a stop and go penalty and will be fined Euro 1.000 as refund of the buoy cost. 50 points will be deducted from the total championship points of the team.

1000.5.03 - MISSING A MARK

The retaking of a missed mark is not permitted. The team will be penalized by a Stop & Go penalty.

1000.5.04 - DURATION OF THE RACE

The races should have a maximum time limit of one hour and should be run minimum 20, and maximum 40 laps. The number of laps will be announced on the Saturday and Sunday Pilots Briefing; therefore 70% rule will be announced accordingly.

1000.5.05 - TIME LIMIT

The chequered flag will be waved when the lead boat crosses the finish line after 1 hour 15 minutes maximum time limit, and all boats will finish as they complete that lap. The number of laps completed by the winning boat will determine the number of laps for the 70% rule.

1000.6 - PROCEDURE FOR CLASS 3 - 225 OFFSHORE STARTS

1000.6.01 - START DISTANCE

In any start where there are ten or more boats starting together, the distance between the start line and the first turn buoy must be around 1.5 nautical miles.

1000.6.02 - START POSITION BEFORE THE RACE

Every race boat must take its relevant position on the start pontoon before the race in order to leave on the correct position for start line. The start boat will lead race boats to the muster area at which point it will raise a yellow flag, and then plane slowly to allow race boats to get on plane safely before accelerating towards down the start chute.

1000.6.03 - SAFETY DISTANCE

The minimum safety distance between competitors, following the start boat, is 10 metres until the raising of the green flag. All boats have to maintain a distance not less than 10 meters and not more than 15 meters between them. A Stop & Go Penalty or a fine may be applied to a boat that gains a competitive advantage. All boats will maintain their start line until the first turn mark.

1000.7 - POLE POSITION / QUALIFICATION

1000.7.01

A timed qualification session will be held the day before the race, except for force majeure. The qualification course is recommended to be the same as the race course.

1000.7.02

It is strictly forbidden to go out of the race course to gain speed before passing in front of the timing line. All Grand Prix boats must maintain the racing line.

The pole position will be divided into two sessions. First session will be open to all boats and will last 40 min. Following the first session, there will be a break for 5 minutes.

During the first session of the pole position every team will have the right to do maximum 10 full timed laps. It is forbidden to do more than 10 laps, if more than 10 laps done a penalty will be decided by the OOD either in the form of deduction of points or monetary penalty.

The second session will be as follows: The first 3 finishers will have 2 full timed laps alone on the race course starting from the 3rd position to the first.

It is strictly forbidden to refuel before the fueling hours as shown in the program.

Best time will be considered as Pole Position time.

1000.8 - PIT STOP

1000.8.01 - DEFINITION

Visit to the designated pit stop area for assistance during the race and / or pole position i.e. from the time the start of the race to the time the last boat crosses the finish line. If the pit stop area is not defined in the Race Instructions, then the pit stop area will be the inside of the harbour.

1000.8.02 - PIT STOP RULES

1000.8.02.1

Minimum one pit stop during the race is mandatory.

1000.8.02.2

During the race, a boat may have unlimited pit stops, each limited to two minutes.

1000.8.02.3

If several boats are returning at the same time, use of the harbour and / or crane will be designated by the Wet Pit Official based on the order of the boats entering the harbour.

1000.8.02.4

Propeller changing is permitted.

1000.8.02.5

All boats coming into the harbour for pit stop must turn on their strobe light.

1000.8.02.6

No accidented boats will be recovered during the pole position and the race.

1000.8.02.7

All boats must be equipped, on its' four corners, with four (4) pull / push, 30cm long, handle bars for pit stop crew use.

1000.8.02.8

One pit stop might be substituted by a long lap.

1000.8.02.9

All race boats must be off the plane by the entry mark when approaching the pontoon. If a boat passes the pit stop pontoon, it will have to join the race circuit and come back for pit stop once again.

1000.10 - PENALTIES

1000.10.01 - STOP & GO PENALTY DEFINITION

The competing boat can be penalised with a stop and go penalty.

The jury boat will expose the race number of the boat that is penalised along with a same size panel showing the letter S for stop.

The competing boat, at the following lap, must stop approx 10 sec in front of the jury boat until the green flag is waved

Stop and go penalty will be inflicted to a boat/team:

- Missing a turn. It is strictly forbidden to turn back to round the buoy during the race, if so, disqualification can be inflicted.
- Demolishing and /or going over a turn buoy.
- failing to take it's pit stop on the requested lap.
- that endangers another team during the race.
- approaching to the pit stop pontoon after the red buoy with a speed more then the idle speed.
- passing closer then 50 metres of an incidented boat or an official boat.

1000.11 - SEA SAFETY

In case of an incident or accident on the race course, a yellow flag will be flown or waved by any "Official Boat". Race boats should keep a distance of at least 50 metres away from any boat displaying this flag and the accident.

1000.12 - TECHNICAL RULES

1000.12.01 - ENGINES

All boats must comply with 508 rules in full. All boats must have DFI engines. All engines shall be strictly production engines such as the standard marine production engines sold by manufacturers to leisure boating (or touring) customers. Maximum tolerated variation will be 10 % on the engine outputs.

The list of the 2014 eligible manufacturers and their engines are as follows :

Mercury	DFI V6 Optimax	225 HP
Mercury	V6 Pro XS	225 HP
Mercury	Verado 4 stroke	225 HP
Evinrude	E-Tec	225 HP
Evinrude	E-Tec H.O.	225 HP
Yamaha	HPDI	225 HP
Yamaha	HPDI V-Max	225 HP
Yamaha	VF 225 SHO 4 stroke	225 HP
Yamaha	V6 Offshore 4 stroke	225 HP
Suzuki	D.F. 4 stroke	225 HP
Honda	B.F. 4 stroke	225 HP

Engine list include some non-homologated engines that are eligible for Class 3-225. No modifications to match the homologation sheet values are allowed.

1000.12.01.1 - DRIVES (GEARCASES)

All drive units available from the manufacturer as a listed option can be used with the related engine. The units have to be shown on the catalogue, of the manufacturer. No purpose built racing drives will be allowed.

1000.12.02 - CREW SAFETY

All race boats must be equipped with a Reinforced Cockpit(s) with canopy for all riding crew members and buoyancy to ensure the boat floats. The crew, who must be seated, must have a restraint system comprising of and conforming with the following rules:

A Reinforced Cockpit with canopies is defined as a containment area (completely closed and separated from the hulls) for crew and can be constructed as an integral part of the boat.

In any boat, cockpits will not be allowed in the sponsons.

The reinforced cockpit must meet the UIM 508 rules.

1000.12.02.1 - POLYCARBONATE AREAS

The polycarbonate areas are to be built using 10 mm thickness, or more.

1000.12.02.2 - ROLLBAR

The Restraint Cockpits must be fitted with an internal roll bar, two in a tandem cockpit as a minimum. There must also be, between the two single cockpits, an anti-compression strut or structure of similar strength to the roll bar.

1000.12.02.3 - GRAB HANDLES

There should be one or more divers grab handles fitted on the outside of each hatch.

1000.12.02.4 - BOTTOM ESCAPE HATCH

All race boats must be equipped with a bottom escape hatch. An area of 15 x 15 cm of the hatch must be constructed of polycarbonate for day light accession to the cockpit when overturned.

1000.12.02.5 - HANS DEVICE

It is mandatory that both pilots use HANS DEVICE during all practice, pole position and race hours.

1000.12.02.6 - AIR MASK SYSTEM

It is strongly recommended for both pilots to use an Air Mask system, that covers both nose and mouth and attached to a carried on air supply system during all practice, pole position and race. It is mandatory to have another spare air unit, installed with a quick release system, in the cockpit.

1000.12.02.8 - AIR BAG

It is mandatory for boats to have an airbag system. The function of the system is to raise the cockpit above the water level after a turn over. The system must be able to be operated manually as well from either inside or outside of the boat.

1000.12.03 - RACE NUMBER

The individual numbers shall conform to the following minimum dimensions, the only exception being where the size of the boat does not allow the minimum size to be carried:

Height	Width	Thickness	Spacing
40 cm	30 cm	7.5 cm	13 cm

1000.12.04 - MINIMUM / MAXIMUM LENGTH AND MINIMUM WEIGHT

Min length	Max Length	Minimum Weight
7.0 meter	9.0 meter	1100kg

1000.12.05

All boats must be equipped with removable steering hub.

1000.13 - POWER LIFT

The use of power lift during the race and the pole position is allowed.

1000.13.05 - MINIMUM WEIGHT

The total weight of the Grand Prix race boat is the total weight of the boat with both the driver and the throttleman, wearing their complete racing apparel and safety gears, at all times during the Event.

The total weight of the boat after the pole position and after the race together with both pilots must not be less than 1.350 kgs at all times during the Event.

1000.14 - SPECIFIC RESTRICTIONS

1000.14.05.2 - EXHAUST

It is permitted to make additional holes to the lower section of the drive, to the exhaust housing.

1000.14.05.3 - FOOT THROTTLE

It is strictly forbidden to use a foot throttle.

1000.14.05.4 - MID - SECTION

Short mid-section is not allowed.

1000.14.05.5 - TUNNEL FLAP

The use of movable tunnel flaps during the race and pole position is strictly forbidden. Tunnel flaps should be fixed before the race and pole position.

1100 - RULES FOR UIM RIB RACES

1100.1 - GENERAL PRESENTATION

These rules have been established as guidelines for Offshore RIB power boat rallying in open offshore seas. They provide the ability to compete in different weather conditions with safe rules designed to provide all competitors with a fair opportunity of winning with endurance as the main parameter, not only speed.

So that owners of the many different designs of RIBs may take part in competitive events in a standard, safe, fair and enjoyable manner.

To foster and develop a design of boat that is expressly built with massive inflatable buoyancy for safety and stability combined with an efficient hull for speed, practicality, comfort and pleasure.

To provide guidelines for National Authorities to be able to certify and assess compatibility RIB and conformity to participate in offshore RIB rallies.

To discourage designs that compromise safety for speed.

It is preferable that boats should have the driver on wheel and throttle with the co-driver and navigator.

It is preferable that boats have standing seats position. But if racing seats are fitted they should provide proper comfort and assure high, safe support to avoid back injuries.

1100.2 - COMPETITORS QUALIFICATION

All competitors in a World Championship have to have participated in at least two races certified by a recognised club and National Authority.

For non titled races;

There must be a minimum of two persons on board the boat during a race.

Both driver and co-driver should have first aid knowledge and resuscitation knowledge.

Both driver and co-driver should have a racing licence issued by their National Authority. Alternatively they may present a medical declaration to Race Control that ensures that each driver has good acceptable health and is free from any back, neck or heart diseases to be able to purchase a Special Event License.

All competitors should have attached to themselves at all times their passports and event licences contained in a safe submersible Ziploc pack.

All competitors must wear an approved acceptable protection helmet coloured red, orange or yellow. If helmet is of another colour, orange stickers should cover at least 50% of the helmet.

1100.3 - CLASSES

International RIB CLASSIFICATION		
Mono Hulls Only		
ACCEPTED	SMALLEST BOATS ACCEPTED	BIGGEST BOATS
RIB SPORT	PRO & RECREATION	PRO & RECREATION
-All Outboards 2 & 4 Strokes	MIN SIZE 6.5 metre	MAX SIZE 8.5 metre
-All Inboards (Petrol and Diesel)	MIN HP 150 HP	MAX HP 250 HP
-Min Length 6.5 m in PRO	MIN HP Diesel 240 HP	MAX HP Diesel 350 HP
-Min Length 6 metre in RECREATION	HANDICAP 3.5 KG/HP / FOOT-THROTTLE AND OR STEPPING 4 KGMS / HP	
RIB SUPERSPORT TWIN	PRO & RECREATION	PRO & RECREATION
-All Outboards 2 & 4 Strokes	MIN SIZE 8.5 metre	MAX SIZE 11 metre
-All Inboards (Petrol and Diesel)	MIN HP 300 HP	MAX HP 600 HP
-Min Length 8.5 m in PRO	MIN HP Diesel 650 HP	MAX HP Diesel 700 HP
-Min Length 8 metre in RECREATION	HANDICAP 3.5 KG/HP / FOOT-THROTTLE 4 KGMS/HP AND OR STEPPING 4 KGMS / HP	
Inboard Handicaping	HANDICAP 3.5 KG/HP / FOOT-THROTTLE 4 KGMS/HP AND OR STEPPING 4 KGMS / HP	
RIB OPEN	PRO & RECREATION	PRO & RECREATION
-All Outboards 2 & 4 Strokes	MIN SIZE 8.5 metre	MIN SIZE 13 metre
-All Inboards and Diesel)	MIN HP ANY HP	Max HP 1000hp Petrol (Petrol 1200 HP Diesel
-Min Length 8.5 metre in PRO EXPERIMENTAL	ANY NUMBER OF ENGINES ALLOWED AND ANY TYPE OF ENGINES	
MINIMUM CREW 2 - MAXIMUM CREW 4		

INTERNATIONAL RIB CLASSIFICATION HAVE IDENTIFIED ANY BOAT WITH EITHER A STEPPED HULL OR A FOOT THROTTLE AS A PRO CLASS AND THEREFORE TO RUN WITH SAME POWER RECREATION CLASS AN EXTRA 0.5 KG/HP IS REQUIRED AS HANDICAP FOR PRO CLASS

1. RECREATION CLASS 3.5 KGMS PER HORSEPOWER

2. PROCLASS and inboard engines 4 KGMS PER HORSEPOWER

1100.3.1 - CLASS DEFINITIONS

PRO Class

Any RIB fitted with either a foot throttle or a stepped hull, or special racing engine.
Example: a 250 Optimax XS is a PRO series boat.

RECREATIONAL CLASS

Any RIB not fitted with a foot throttle or stepped hull and with no modifications to engine(s).

No boats with canopies or restraints will be permitted.

1100.3.2 - HANDICAPPING PURPOSE IF RACING 150 TO 250HP UNDER ONE CATEGORY SPORT AND 300 TO 600HP UNDER ONE CATEGORY SUPER SPORT

To allow different boats with different power and size to have equal chance of winning and competing for one prize.

All boats will be weighed post race and must be within class weights.

1100.4 - ENGINES SPECIFICATIONS AND MODIFICATIONS

1100.4.1 - GENERAL MODIFICATIONS

The original propeller may be replaced by any other.

Sparking plugs may be changed for any other.

Revolution counters, tachometers, water pressure and temperature gauges may be fitted to engines.

Thermostats of the cooling circuit may be removed.

Over speed switches may be blocked.

Re-boring and fitting of oversized pistons is allowed up to 0.9mm as made available by the manufacturer.

The steering bar and brackets may be reinforced as a safety measure. It is also permitted to modify the standard engine cowling in order to facilitate the fitting of steering assemblies, provided that any openings are sealed so the engine cannot pull in any additional air.

The rubber mounts of the engine may be changed or substituted.

A spring may be added to the butterfly valve of the carburettor.

The carburetion and ignition advanced controls may be tuned, but without altering the original parts.

The replacement of nuts, or the addition of lock nuts, drilling and wiring, split pinning or keying is allowed, provided the screws and pins are the original ones.

1100.4.1.1 - TWIN INSTALLATION - OUTBOARD ENGINES

When twin engines of the outboard type are installed, but the particular type is not available with opposite hand rotation, it is permitted to modify the drive of one unit to opposite hand propeller rotation provided that:

The standard underwater casing is retained

The final propeller ratio to the crankshaft is unaltered.

1100.4.1.2 - MODIFICATION TO OUTBOARDS

The use of thrust blocks mounted on the lower unit and/or on the boat is permitted.

Power trim - a properly engineered system for altering engine trim whilst underway permitted.

In the process of modification, minor optional alternatives as listed and priced by the makers may be permitted provided they do not affect engine performance, but improve safety.

Plastic reed valves may be fitted as made available from the manufacturer.

Remote water pick-ups are not allowed.

Lighter flywheels may be fitted as made available by the manufacturer.

Transom brackets may be reinforced.

1100.4.1.3 - MODIFICATIONS ALLOWED - INBOARD ENGINES

The preparation and modification of inboard engines is completely free, with the following restrictions:

The cylinder block must be the original one as supplied by the manufacturer with no modifications permitted except the re-boring of the cylinders, which may be done within the limits indicated by the manufacturer, and provided that this does not make the engine exceed the horse power of its class.

Modification of the original crankshaft is entirely free, provided that the original stroke is maintained.

Modification of the cylinder head is entirely free, provided that no more valves per cylinder are provided.

1100.5 - SPECIFIC RULES FOR BOATS AND EQUIPMENT

1100.5.1 - COLOURS

All boats should not be complete white or blue or grey or black. If so they should have stickers of phosphoric orange or yellow or red on both sides and on top visible from a distance by helicopter or large vessel.

1100.5.2 - FUEL

The only fuel to be used by any offshore powerboat must have the same characteristics as the fuel commercially available to the general public as dispensed from roadside pumps or marine fuelling facilities.

For petrol engines, the fuel used shall be unleaded petrol. No additives are permitted except lubricating oil for two stroke engines, providing that such oil does not increase the octane or the water content of the fuel or enhance the fuel by any other means.

For diesel engines, the fuel shall be that intended for use in diesel engines road vehicles or automotive based marine engines (including red diesel).

1100.5.3 - FUEL TANKS

Shall be secure in all directions and shall not leak.

Shall have appropriate filling and venting arrangements, where applicable, that are not close to any hot parts such as exhaust manifolds.

Shall be insulated or isolated from the engine, etc, preferably by bulkheads.

There shall be an easily accessible means of shutting the fuel supply off from the tank(s).

Permanent metal tanks shall be earthed.

The carrying of a refuelling funnel is advised.

1100.5.4 - FUEL LINES

Fuel lines shall be leak resistant and run in a manner to avoid damage.

1100.5.5 - STEERING

All steering and throttle controls and linkages, quadrants and any fitting to the stock shall be in good condition and secure.

Engine and rudder controls shall operate with full and free movement.

Rudder assemblies, glands, keys, etc. shall be in good condition and secured with lock nuts and/or tight split pins.

Tiller steering is not permitted.

Hydraulic steering shall be fully operative, hoses and unions shall be free of leaks and adequately protected.

1100.5.6 - ENGINE CUT-OFF

Engine cut-off devices for connection to all crew are mandatory (first man out shuts off engine)

1100.5.7 - THROTTLE CONTROL

All foot throttles shall return the engine(s) to idling speed when released.

The lever unit shall be securely attached.

The control unit shall be properly connected, work freely and shall not be in a position where it can be fouled.

The control unit shall be within easy reach of the driver in his normal position.

1100.5.8 - GEAR CONTROL

All craft shall be able to be manoeuvred ahead and astern and have neutral capability operated by controls at the helmsman's position.

1100.5.9 - ENGINES PROPULSIVE EFFORT

One hundred per cent of the propulsive effort shall be derived from the water while proceeding at racing trim in calm water.

Boats running two or more propulsions systems must have either external tie bar to stabilise out drives or some other protective device that will prevent drives colliding should a failure occur.

1100.5.10 - ENGINE MOUNTINGS AND TRANSMISSIONS

Outboards

Mounting brackets and clamps shall be secure and in a satisfactory condition.

Engine mountings shall be attached to the transom with at least four bolts.

Inboards

Engine mountings shall be sound, and the mounting bolts securing to the hull shall be pinned, lock-nutted or locked.

Where an outdrive is fitted, the method of connection to the transom and the unit to the ring shall be secure.

Bearers shall not be saturated with oil.

1100.5.11 - ENGINE HAZARDS

The engine shall be free of dangerous corrosion, oil or fuel leaks or excessive heating likely to be a fire hazard or a danger to any adjacent structure.

1100.5.12 - PROPELLER SECURITY

It shall be the responsibility of the competitor to ensure that the propeller or propellers are sound; particularly at the blade roots and that it/they are securely locked by the propeller nut(s).

1100.5.13 - BATTERY STOWAGE

Batteries shall be secured suitably vented with fittings sufficiently strong to withstand any anticipated shock, or inertial force.

1100.5.14 - FIRE PREVENTION

All boats with inboard engines shall carry a fixed automatic fire extinguishing system. This system shall be properly installed and engineered.

In addition to any fixed automatic extinguishing system all boats shall carry a minimum of one fully charged dry powder 2kg fire extinguishers fitted adjacent to the engine space and others (if fitted) readily accessible to each crew member.

Fire extinguishers shall be one 2kg or two 1kg suitable for marine use.

1100.5.15 - INFLATABLE COLLAR(S)

PVC, Hypalon, PU/PVC are accepted collar material. All materials should be of an approved Marine Fabric.

Minimum compartments accepted 3 independent.

The collar(s) is/are to be inflated to the proper pressure recommended by manufacturer for scrutineering.

And this pressure is to be maintained throughout an event, except under circumstances of force majeure only.

1100.5.16 - HAND AND FOOT PUMPS

A hand or foot pump capable of being operated below the gunwale shall be carried.

1100.5.17 - SEA ANCHOR

A sea anchor appropriate to the size and weight of the boat shall be carried aboard at all times and be stowed in an accessible place.

1100.5.18 - ANCHOR LINE

The anchor line shall be of a size and strength appropriate to the boat, be in good condition and at least 50 metres in length (3 metres of chain advised).

1100.5.19 - TOWING ROPES

A towing rope must be connected to the boat at the towing cleat on the front of the boat at all times whilst racing, and must be of adequate construction and strength for the boat to be towed when waterlogged.

1100.5.20 - WINDSHIELDS/WINDSCREENS/ (IF FITTED)

Shall be well secured.

Shall be of non-splintering material in all classes. The use of glass for windscreens must be of triplex or similar safety glass.

Shall be masked by rubber or plastic on any bare edges.

1100.5.21 - SHARP EDGES

All mascots, lights, bow fittings and other sharp edges, shall be adequately protected or removed.

1100.5.22 - SEATS

Seats shall be of adequate strength and firmly secured.

It is preferable that the boats have standing seats position. But if racing seats are fitted they should have proper comfort and assure high, safe support to avoid back injuries.

1100.5.23 - VHF/GMDSS RADIOS

Each boat must carry at all times whilst racing; a hand held VHF radio or a fitted 25 watt radio each of which must be 'certified waterproof by the manufacturer'.

1100.5.24 - BILGE PUMPS AND BAILERS

All boats should have the deck completely isolated from the hull and should be at least 5cm above the waterline with maximum loaded weight. Only 1/4 of deck can be below the water line.

For all boats participating in offshore rallying. As boats are sometimes more than 10 nm from shore they are subject to high waves and might be completely submersed and thus should have a self bailing system which is not dependent on mechanical or electrical systems. Extruder 50cmx50cm capable of extracting all deck water when moving at minimum power to dry the deck in a maximum of 3 minutes, as per CE marking regulation for class B vessels is acceptable.

A satisfactory bailing device, either automatic or electric, shall be fitted. In addition, a manual backup, either a bailer or bucket must be provided.

1100.5.25 - FIRST AID KIT AND MANUAL.

A substantial first aid kit and manual suited to the potential hazards encountered when racing this class of boat shall be carried, stowed in a self-contained waterproof box within easy reach of the crew.

Crew might be asked to demonstrate to scrutineer their capability to use first aid to stabilize their partner(s).

1100.5.26 - FLARES AND MARKERS

A minimum of 2 red parachute or rocket flares, 2 red location flares, 2 white location flares and 2 orange smoke flares are recommended. All flares shall be stowed in a conveniently accessible place, shall bear either the date of manufacture or the expiry date in original manufactured form, not handwritten, shall be not more than 3 years old and have a minimum burn time of 40 seconds. One mini flare pack must be personally carried by each crew member.

1100.5.27 - MAPS AND CHARTS

Up-to-date paper charts of suitable scale or other relevant publications covering the event area(s) shall be carried.

1100.5.28 - PROTECTING HELMETS

Unless otherwise stated at briefing, protecting helmets must be worn by all persons on board at any time when the boat is on the plane during the journey to and from the race and during racing and practice or testing.

Helmets must be to B.S.6658: 1985 (with temple protection) or equivalent and it is recommended that they be coloured «orange»; the efficiency of a helmet is the responsibility of the wearer.

Helmets may be removed during the race if the boat is adrift or at anchor with engines stopped or idling.

Helmets which are painted orange are acceptable (if they meet the other requirements of this rule) but, because it is not possible at scrutineering to determine the compatibility of the paint with the material of the helmet, competitors intending to paint protecting helmets are advised to consult the helmet manufacturers regarding the suitability of the paint which they propose to use.

Modifications shall not infringe on the standard and bolts used for fittings and attachment of a visor shall be small and on no account shall they protrude into the inner surface of the helmet.

Chin straps shall be in good condition and operative.

Helmets shall be devoid of dents or splits.

Helmet visors shall be in good condition and devoid of cracks and easily detachable (i.e. not bolted down).

1100.5.29 - RACING FLOATING VESTS

Racing vests of minimum floatation and buoyancy are mandatory and shall be worn at all times by all persons on board whilst on the water.

The efficiency of a racing vest is the responsibility of the wearer.

Tears or rents or bad repairs through which buoyancy material may leak out shall not be permitted.

Vests shall not be able to wash up over the wearer's head, and shall be secure to his body. Crutch straps are mandatory.

Open class competitors must use purpose made racing vests that comply with UIM offshore rule 205.06.

1100.5.30 - PROTECTIVE CLOTHING

All crew members whilst racing must wear suitable protective clothing that covers the torso and all limbs to the wrists and ankles.

1100.6 - ASSISTANCE RULES

The orange flag is to be displayed where a vessel requires assistance, or has retired. Unless another vessel is already rendering assistance it is the responsibility of each racing boat to investigate the situation where either:

- a) an orange flag is displayed or
- b) another race boat is stopped and making no signals.

1100.7 - WEIGHING OF BOATS

It is the responsibility of the Organisers to secure a weighing facility convenient to their location.

1100.8 - CRANAGE AND RECOVERY

No persons shall be on board whilst craning in or out.

On no account shall craned boats pass over the head of any person or another boat.

Only certified and approved crane drivers are allowed to crane the boats and they have to be officially approved by the local Organiser and National Authority for insurance purposes.

1100.9 - UIM FLAG SIGNALS

FROM THE START BOAT:

Red Flag BEFORE the start procedure	Rally Cancelled
Red Flag DURING the start procedure	Start Aborted
Yellow Flag	Start Procedure Commenced
Green Flag	Rally Started
AT THE FINISH LINE:	
Chequered Flag Finish	All Classes
Red Flag at safety waypoint or rally mark	Rally Stopped All Classes

FROM ALL OFFICIAL PATROL AND MARK BOATS:

Red Flag (waved)	Rally Stopped
Yellow Flag	Caution
Reddish Brown with event Logo	Official Boat Identification
FROM COMPETING BOAT:	
Orange Flag (Flown)	Retired

1100.10 - RIB RACING COURSE RULES

4 legs per event Grand Prix, International Championships, Rally, or offshore marked course

Minimum course length is 40 nm. In the case of a circuit, the length of a lap multiplied by the number of laps should be equal to over 40 nm to be equivalent to the Rally course and the races that run from port to port.

Minimum one long leg of a course to be over 60 nm.

Maximum accepted course length per day is 100 nm.

Total of 4 legs to be over 200 nm and maximum 400 nm.

1100.11 - POINTS SCORING SYSTEM

All competitors will be eligible for points on the successful completion of each leg of the rally. The first boat in each class will receive 1000 points, the second boat 900 points this being a 10% reduction and each successive boat 10% less points than the previous boat. The overall winner in each class will be the boat with the highest number of points.

Retired boats will get 0 points and will be able to proceed in next race as total of all races is the winning calculation.

The protest fee is 80 euro and if protester is ruled against this fee is non reimbursable. For international events, the UIM 400 jurisdiction rules apply.

1200 - RULES FOR UIM MARATHON RACING

1200.1 - GENERAL

1200.1.1 - RULES

The general UIM offshore racing rules are to be applied. In cases of conflicts between the general rules and the class rules (and specifications), the class rules have precedence

1200.1.2 - MINIMUM AGE

The minimum age for competitors in all of the marathon classes is 18 years.

1200.1.3 - MEASUREMENT CERTIFICATE

Measurement certificates are required for all classes.

1200.1.4 - CLARIFICATION

In these Rules, the words “shall” or “must” means mandatory; the word “should” means recommended.

1200.1.5 - LICENCE

The issuing of International Offshore licences, is the task of the National Authority of the competitor. The National Authority of the licence holder must verify that the competitors have the necessary experience and still to take part in Offshore Marathon events.

As a reference for minimum competence, The Competitors who have not held a Marathon Licence must submit a detailed CV of their racing and boating experience to their National Authority. This CV must demonstrate practical experience and theoretical knowledge to a minimum “RYA” qualification or military equivalent of: RYA Day Skipper – or RYA Coastal Skipper – or RYA Yachtmaster – or RYA Advanced Powerboat (not RYA Powerboat Level 2 on its own). National equivalent qualifications approved by the applicants National Authority will also be accepted.

Both driver and co-drivers must have first aid and resuscitation knowledge as defined by Red Cross or similar.

1200.1.6 - SEA SURVIVAL CERTIFICATE

A current sea survival certificate must be held by all crew members.

1200.2 - COMPETITION RULES

1200.2.1 - DECALS

Competing craft will be asked to display the race decals on either side and on the deck of the craft. The decals will be provided by the Organisers/Sponsors.

1200.2.2 - OUTSIDE ASSISTANCE

No outside assistance is allowed during a race. Competitors may however refuel during a race at recognised port or marina fuel outlets within harbour confines. Ship to ship or air to ship refuelling at sea is not permitted. Any competing craft may be towed and if a tow is accepted, then that competitor will be treated as a retirement from the race or that particular leg of the race.

1200.2.3 - IDENTIFICATION

All competing craft must carry race numbers as laid down in the UIM Rules; this includes deck numbers. Race numbers shall include a prefix letter, 'A', 'B', 'C', 'D', 'E', 'F', 'G' or 'H' to match each sub-class of Marathon classification. Numbers shall be painted in waterproof black paint on a yellow or white background or be made of black coloured self-adhesive materials of adequate strength. No number may begin with a zero. All numbers shall be plain and upright. The numbers shall be painted as to be clearly visible from both sides and above. Numbers must be placed on both sides of the hull within the front third of the boat and on the foredeck within the front third of the boat. Numbers displayed on the foredeck must read correctly from the transom and shall be underlined by a black bar.

Minimum dimensions for individual numbers must be:

Height = 30 cm; width = 23 cm; thickness = 5 cm; spacing = 13 cm.

The background shall extend at least 23 cm in front of and beyond the end number and at least 7.5 cm above and below the numbers.

British boats will be expected wherever possible to make a race number change where an overseas competitor has the same number.

1200.2.4 - ADVERTISING

No part of any advertising, sign writing, flags, badges, emblems or marks of any kind shall be within 24 inches of the race number. No material may be displayed which advertises any product which is in conflict with the race sponsor's products unless prior permission in writing is obtained. Display material considered to be offensive in the opinion of the organisers will be required to be removed. Failure to comply could result in disqualification.

1200.2.5 - RACE FUEL

The Race Fuel must be standard Pump Fuel as per UIM Rule 504.

1200.3 - RACE BOATS

1200.3.1 - HULL LENGTH

The minimum hull length for all competing craft is 24ft / 7.32 metres and the maximum for all is 50ft / 15.20 metres using UIM standard measurement procedure. See Class classifications. A diagram is available from the Organisers for clarification.

1200.3.2 - SPEED

All competing craft must be capable of exceeding 40 knots. This does not apply to Classic Class boats which must have a speed capability of 30 knots.

1200.3.3 - MONOHULL BOATS

In the Spirit of Offshore Endurance Racing, all competing craft must be Monohull.

1200.3.4 - CANOPIED BOATS

All competitors and crew members who race in boats with restraints, canopies, and partial canopies must hold a current immersion test certificate.

Canopied boats must have a current Measurement Certificate and comply with full UIM 508 rules for canopies.

Additionally the following rules apply:

Boat builders/designers must confirm in writing that the boat is designed for such activities and to race in a given class. This confirmation is to be supported by detailed technical drawings as appropriate.

Crew must be able to demonstrate that they can safely exit the boat (maximum recommended exit time 30 seconds) This may be subject to scrutineer testing at events.

The nominated skipper of the vessel accepts ultimate responsibility for complying with all of the above requirements.

Carbon monoxide sensors and alarms must be fitted in all canopied boats.

1200.3.5 - PARTIAL CANOPY BOATS

All competitors and crew members who race in boats with restraints, canopies, and partial canopies must hold a current immersion test certificate.

Partial canopies may be permitted subject to the technical inspector's approval, this process must be commenced a minimum of 3 months before the event.

There must be an ***opening hatch*** with a minimum open space sufficiently large (minimum 55cm X 82.5cm) for each person in the boat to exit immediately. Alternatively, there must be an open space in the rear of the craft sufficiently large (minimum 1.3m X 1.3m) for all crew to exit the boat immediately. Access at this opening must not be restricted in any way whatsoever.

Partially canopied boats may have restraint systems fitted which, if fitted, must comply with the following Offshore Rules: 508.01 (Crew Immersion Test) , 508.16 (Air Supply), 508.18 (stop buttons for engine cut-off), 508.20 (Rear of Head Protection) and 508.21 (Specification of 5 or 6 strap Harness).

Additionally the following rules apply :

1. Boat builders/designers must confirm in writing that the boat is designed for such activities and to race in a given class. This confirmation to include key safety points below, with detailed technical drawings supplied as appropriate.
2. Crew must be able to demonstrate that they can safely exit the boat (maximum recommended exit time 30 seconds) This may be subject to scrutineer testing at events.

3. Seat belt mounts must be of appropriate strength and position, considering the boats maximum designed speed – see 508 rules.
4. Structure of the partial canopy must be of similar strength to the hull/running surface of the boat.
5. The screen must be of a suitable materials and have flanges adequate to offer the strength required to meet the anticipated loads and speed of the craft.
6. All crew must have in date immersion test training and hold a current immersion test certificate.
7. Doors or hatches must be so designed to allow them to be easily opened from inside and out and must be labelled to allow rescuer to immediately understand opening system and backup system – hinges must have removable pins.
8. There must be an air system provided for each crew member – spare air systems are not acceptable.
9. There must be a minimum clearance between seats or door aperture of 40cm if this is the primary exit route.
10. The nominated skipper of the vessel accepts ultimate responsibility for complying with all of the above requirements.
11. Carbon monoxide sensors and alarms must be fitted in all canopied boats.

1200.3.6 - SEAT BELTS / RESTRAINTS

No seat belts or restraints whatsoever are permitted in open boats, i.e. boats with no canopies or boats with no partial canopies.

All competitors and crew members who race in boats with restraints, canopies, and partial canopies must hold a current immersion test certificate.

1200.3.7 - REINFORCED WATER DEFLECTOR

ALL Craft with a top speed in excess of 50 knots which do not have a forward cabin structure must have a Reinforced Water Deflector over and under the deck, designed and constructed of materials with sufficient strength to provide adequate crew protection. The forward fairing on deck must rise to a minimum height of the chin of the tallest crew member when in the normal driving position. The top 5cm of the water deflector must be at least 45 degrees from the horizontal with a minimum of 30cm width per person measured transversely in the horizontal plane.

The Reinforced Water Deflector must be designed and constructed so as to present no hazard if the crew is thrown forward and must be so designed that it would not restrict the crew from being ejected in all cases. Open RIBs must have a solid fitted console to deflect water. In addition, all vessels must have a means of preventing the riding crew from sliding forward under the foredeck when in their normal racing position. A bulkhead or suitable kick-board in front of each of the riding crew must be fitted and be of sufficient strength to prevent the riding crew from forward movement in the event of rapid deceleration. The bulkhead/kick-board must be secured so that there is no more than 1 inch space between the crews' floor and the bulkhead.

1200.3.8

All competing craft must be capable of going astern.

1200.3.9 - BOW BALLAST TANKS

Bow ballast tanks are permitted.

1200.3.10 - LIFTING POINTS

Where possible, teams should ensure that their boat has fixed lifting points, and their own certified strops, which must be carried in the boat. For ALL boats, the lifting positions of the craft for emergency lift by crane slings must be clearly marked on either side of the boat.

1200.3.11 - BUOYANCY

It is recommended that enough buoyancy is provided in the race boat or in the material used for its construction to ensure that the boat floats if capsized or holed.

1200.3.12 - BILGE PUMPS

Suitable & automatic bilge pumps shall be fitted to the boat capable of pumping out all sections of the boat even where water-tight bulkheads are fitted. They shall be accessible and be fitted with a suction pipe leading to the lowest point of the bilge and with a discharge pipe overboard. There shall also be at least one manual bilge pump in the boat.

1200.3.13 - PUMP

All RIBS shall carry a hand or foot pump capable of being operated below the gunwale.

1200.3.14 - DECK FITTINGS

All boats must have guard rails or handholds as rails or handles raised above the deck of a suitable material or wire in stanchions. A single rail would suffice. A rope secured to the bow and made fast in the cockpit will not be accepted.

1200.3.15 - TOWING

All vessels must be fitted with a properly designed towing point of adequate construction and strength for the boat to be towed when waterlogged for extended periods. Tow lines suitable in length and strength for the boat in all weathers must be carried by all craft and must be permanently attached to the strong point during races unless the crew can demonstrate that a towing line can be easily attached to the towing point whilst at sea. This towing line must have a fixed loop to provide ease of attachment.

In addition each boat should carry sufficient fenders & warps as to be able to dock or raft independently in port or to be able to raft suitably to another vessel if assistance was either required or being offered.

1200.3.16 - ANCHOR

Anchor/s with adequate anchor lines must be carried at all times and shall be of a weight and type adequate to hold the boat and shall be properly stowed in an accessible place. In addition for Ultra Marathon races all boats must carry a sea anchor (drogue) suitable the size and weight of the vessel.

1200.3.17 - COMPASS

Compass, Charts/back-up navigation and Nautical Almanac:

1. Compass - All boats must be equipped with a liquid filled magnetic compass. Compass deviation cards for magnetic compasses must relate to adjustment within the 12 months prior to the date of the event. Electronic compasses may be used in addition.

2. Charts/back-up navigation system – All boats must carry at all times a comprehensive set of paper charts which must be in date and corrected to current notice to mariner publications. These charts must be of suitable scales and detail to enable safe navigation over the entire course OR an electronic chart plotter containing chart data of suitable scales and detail to enable safe navigation over the entire course must be fitted. In addition to the on board fixed chart plotter a portable chart plotter containing chart data of suitable scales and detail must be carried to enable safe navigation over the entire course. This additional unit must be provided with additional emergency batteries to ensure a minimum of 8 hour operation.
3. Nautical Almanac – All boats must carry at all times a nautical almanac providing detailed navigational information of the entire course.

1200.3.18 - NAVIGATION LIGHTS

Navigation lights in working order shall be carried in accordance with International Rules for the Prevention of Collisions at Sea.

1200.3.19 - FOG HORN / TORCH

All boats shall carry an efficient fog horn and a powerful torch.

1200.3.20 - ENGINE CUT-OFF DEVICES

Engine cut-off devices for connection to the crew are mandatory (first man out shuts off engine). An emergency over-ride system to restart the engine/s shall be mandatory. The lanyards used must not exceed 120cm between driver and the boat. The emergency cut-off devices must be positioned so that when they operate the lanyard and cap (or clip) will not catch or foul. The lanyards shall be attached to all crew members at all times when the boat is racing. For boats using restraints, see 3.5 above for stop buttons.

1200.3.21 - FUEL TANKS

Fuel tanks must be purpose made and permanently fixed. They must be secure, non-leaking, vented, grounded and have an easily accessible means of shutting the fuel supply off from the tank/s. For safety, it is strongly recommended that fuel tanks in engine compartments are suitably encased to prevent any flying objects in the engine compartments from penetrating the bare skin of the fuel tank. When additional electric pumps are fitted to the fuel supply of outboard motors, a fuel cut-

off switch for the pump shall be fitted in easy reach of all crew members. The positioning of the fuel cut-off switch must be clearly marked for safety reasons.

Fuel transfer at sea is only permitted between your own fixed onboard tanks. Race fuel may not be stored or carried on board in any jerry-can type of container.

1200.3.22 - BATTERIES

Batteries shall be housed in ventilated compartments, mounted upon a secure and solid platform and be fitted with an isolator switch. The battery isolator switch position must be clearly marked for safety reasons.

1200.3.23 - ELECTRICAL HARNESS

Electrical Harness: Properly protected terminal boards shall be used with flexible (not solid core) cabling supported well up to the terminals and at suitable intervals throughout the length of any run. Where relative movement or vibration occurs across any gap, cables shall be sheathed in plastic or metal tube anchored at both ends. Reinforced cable suitable for marine duty shall be used. Electrical equipment in engine compartments shall be kept to a minimum and sited away from heat or fuel.

1200.3.24 - FIRE PREVENTION

All boats with inboard engines shall carry a fixed automatic fire extinguishing system. This system shall be properly installed, engineered and maintained. Sensors and injectors shall be in danger regions of the engine compartment/s. ALL boats including inboards shall carry a minimum of 2 fully charged dry powder 2kg fire extinguishers.

1200.3.25 - RETIREMENT FLAG

All boats shall carry an orange rectangular flag of minimum size 60cm X 40cm as a means of announcing retirement from a leg of the race, and the means of maintaining it aloft in a well visible position.

1200.3.26 - NATIONAL FLAG

All boats shall display their national flag in either fabric or graphic form so as to be visible from both sides of the craft.

1200.4 - ENGINES

1200.4.1 - ENGINE LIMITS

Engine limits are as described in the Class classifications.

1200.4.2 - ENGINE DESCRIPTIONS

ENGINE DESCRIPTIONS for all classes except A, B & C

Note: The term 'OEM' = Original Engine Manufacturer

- a) Engines eligible for Marathon classes other than the classes A, B and C (all these are 'free') must be based on currently, or previously advertised/available mainstream production units (ie, Mercruiser, Volvo, OMC, Yanmar, Cummins, FPT etc) available to the general public through normal distribution channels with a minimum production run of 500 units. Engines should be from the pleasure line, not the race line of engines from any manufacturer,

If the engine is listed with separate runner exhaust, as described in section (g), the motor is not acceptable. As an example, the Merc 525hp motor used in P1, has CMI headers with runner lengths greater than 6" long, so these motors would not be permissible. The old Merc 500 carb, had GILL/Merc alloy manifolds with common plenum, they would be acceptable, as would STD Merc 496HO's, or 502efi's.

- b) The original bore & stroke must be retained, however, an allowance will be made for OEM piston oversize, i.e., +0.030.
- c) Inlet system (intake manifold, intercooler, throttle body etc) MUST remain OEM; throttle bodies must retain OEM internal dimensions at butterfly. Turbochargers must be OEM as specified as STD for motor to which it's fitted.
- d) Internal engine modifications are allowed, such as camshaft choice, cylinder head gas flowing etc, however, rules for OEM induction systems, intercoolers, turbochargers, etc will be strictly enforced.
- e) Engine components may be compared to standard OEM components to establish eligibility.
- f) Transom or side exhausts are permitted, providing that the total volume of water passing through the engine cooling system mixes with the exhaust gases from the manifold.

- g) Choice of exhaust manifold is free, so long as they are not of the separate runner type, i.e., Stellings, CMI, PF Marine, KE Lightning, etc. Exhaust ports must exit to a common plenum within the manifold, with each branch not exceeding 150mm in length. It is in the competitors' interest to contact the RYA prior to purchasing exhaust systems to ensure eligibility if there is any doubt. No dispensation will be given for exhaust manifolds that do not meet the rules.
- h) Drive systems are "free".
- i) Height adjustable hydraulic engine jacks are NOT permitted.

1200.4.3

Single engined craft and single shaft driven boats may compete. All such boats MUST carry an auxiliary engine for use in emergency breakdowns capable of propelling the craft at 5mph for a minimum of 35 miles. This engine can be stowed in the boat but must be shown to be sufficiently retained so that it will not work loose or cause damage.

1200.4.4

Boats with more than one outboard engine or outdrive must have a properly engineered tie-bar system

1200.4.5 - ENGINE MOUNTINGS

Outboards: Engine mountings shall be attached to the transom with at least 4 bolts, which shall be either pinned or lock-nutted.

Inboards: Engine mountings shall be sound and the mounting bolts securing to the hull shall be pinned or lock-nutted. Engines shall be enclosed in their own compartment/s with the exception of normal ventilation. Each and every engine need not be contained in its own compartment. The compartments shall have rigid covers.

1200.5 - SAFETY EQUIPMENT FOR THE BOAT

1200.5.1 - EMERGENCIES

It must be understood that in Marathon (Endurance) Racing every team/crew must be prepared for any/all eventualities with regard to their own safety. If any team experiences difficulties or an emergency during the race, then they must be aware

and prepared to be effectively on their own until rescue teams (from whatever source) can arrive. Every team must therefore have emergency plans which must include a risk assessment setting out how they would deal with an on board emergency situation until such time as rescue teams arrive. A template is available from UIM website.

1200.5.2 - INFLATABLE LIFE RAFT

It is mandatory that every boat carries one certified and in date Inflatable Life Raft capable of carrying the complete crew, together with adequate fresh water and a first-aid kit. The life raft shall be stowed in an easily accessible position so that in the event of emergency the life raft can be quickly released, particularly in the event of fire. Aviation style life rafts are not acceptable for Ultra Marathon races where the total race length exceeds 500nm. The life raft is recommended to comply with ISAF Offshore Special Regulations (OSR) Appendix A Part 2 or ISO 9650 Type 1 Part 1, Group A standard, and should be fitted with a boarding ramp. (Or equivalent NA approved standards)

1200.5.3 - FLARES

All boats will carry an Offshore. standard flare pack comprising: 4 red parachute rockets, 4 red hand held, 2 orange smoke hand held, and 4 white collision hand held flares in a waterproof canister. If any flares are deployed must be replaced before the start of the next race.

1200.5.4 - VHF RADIO

All boats will carry a shipboard installation of a fully synthesized VHF transceiver with GMDSS capability carrying all 55 International Channels, including Channel 37 (M), 157.850 MHz Simplex, with an aerial output of not less than 25 watts. In addition, a multi-channel waterproof hand held VHF radio, which should be carried by one crew member at all times when racing. Each vessel must have a Ship's Licence for their radio equipment and at least one crew must have an Operator's Licence, both Licences to be checked at Race Control and be available for scrutineering.

1200.5.5 - E.P.I.R.B.

A portable emergency position indicating radio beacon (E.P.I.R.B.) operating on 406 MHz shall be carried and stowed in an easily accessible position.

1200.5.6 - TRACKER SYSTEM

Some long distance Marathon Races may require tracker systems, this decision will be made by the organisers prior to the event and details provided in the Advance Race Instructions, For all Ultra Marathon Races a tracker system as defined by the race organizer is mandatory.

1200.5.7 - FIRST AID KIT

All boats shall carry an adequate medical kit and emergency thermal protective aids for each member of the crew.

1200.5.8 - RADAR REFLECTOR

All boats must carry a RADAR reflector suitable for the boat or a Search & Rescue Transponder - SART.

1200.6 - SAFETY EQUIPMENT FOR EACH CREW

1200.6.1 - CRASH HELMETS

Crash helmets are to be worn by all persons on board at any time when the boat is on the plane during the journey to and from the race and during racing and practising or testing. Crash helmets may be removed when undertaking repairs providing the boat is off the plane. Crash helmets must be orange in colour and have temple protection. Helmets must have the boat's race number on the top, painted in black and minimum height of 7.5cm.

Helmets must be devoid of dents or splits and any modifications must not infringe on the standard. Bolts used for fittings and attachment of a visor must be small and must not protrude into the inner surface of the helmet. Visors must be devoid of cracks and easily detachable (i.e. not bolted down). Chin straps must be in good condition and operative.

All crash helmets must comply with the following specifications:

It is mandatory to wear a helmet made to a minimum standard to include UN ECE22-05 or Snell M2000 or M2005 or Snell SA2000 or SA2005.

It is also recommended that helmets are replaced when they reach five years old from date of manufacture. When a helmet is produced for scrutineering the wearer must prove compliance with the above standards; this may be by means of product literature.

1200.6.2 - RACING VESTS

All crew must wear a racing vest suitable for their class. Specification: All vests must have inherent buoyancy of at least 150 Newton and be fitted with collars. The Grabner & Hutch Wilco Motorboat Offshore Racing vests are also acceptable. Lacing ties and/or straps must be adequate & in good condition. All straps must be at least 40mm wide & have a minimum breaking strain of 500kg. There must be lifting straps at the front or on the shoulders.

Zips are not permitted as the sole means of fastening a racing vest. Where zips are used as an ancillary means of closure, they must be in working order.

Tears/rents or bad repairs through which buoyancy material may leak out are not permitted. Vests shall be at least 70% orange or yellow or have orange or yellow panels.

Vests must have a lifting eye or strap attached to the main harness.

Vests must not be able to ride up over the wearer's head & be secure to wearer's body.

The disposition of the solid buoyancy must be such as to ensure that an unconscious person shall float face up in the water.

The vest must have impact protection material covering the back.

For crew using restraints, the racing vest must comply with UIM rule 508.25.

1200.6.3 - EFFICIENCY OF CRASH HELMET & RACING VEST

The efficiency of the crash helmet and racing vest is the sole responsibility of the wearer.

1200.6.4 - PROTECTIVE CLOTHING

All crew members whilst racing must wear suitable protective clothing that covers the torso and all limbs to the wrists and ankles. Protective clothing used must be durable enough to provide bodily protection and is recommended to be fire retardant.

1200.6.5 - MEDICAL COMPRESS & KNIFE

All crew must carry on their person whilst racing a whistle, medical compress and a knife.

1200.6.6 - SAFETY EQUIPMENT

All Boats must start and finish and carry throughout the race the following minimum equipment:

UIM RULE ITEM OF EQUIPMENT

- | | |
|----------------|---|
| 1200.3.4 & 3.5 | Carbon monoxide sensors and alarms for boat with full or partial canopies |
| 1200.3.10 | Certified Lifting strops (for boat fitted with lifting points) |
| 1200.3.11 | Manual Bilge Pump |
| 1200.3.12 | Foot Pump RIBS only
Foot Pump RIBS only
Tow Rope and Warps
Anchor and rope/chain minimum length 50 metres.
Additionally all boats must carry a sea anchor of suitable size for their particular vessel. |
| 1200.3.19 | Fog horn and Torch |
| 1200.3.24 | Two 2kg dry powder fire extinguishers suitably mounted close to engines or crew |
| 1200.3.25 | Orange retirement Flag 0.60m x 0.40m |
| 1200.4.3 | Auxiliary engine (single engine race craft only) |
| 1200.5.2 | Inflatable Liferaft compliant with rules |
| 1200.5.3 | Flares: -4 x red rocket. 4 x red handheld.
2 x orange smoke. 4 x white collision
1 mini flare pack to be carried per person |
| 1200.5.4 | VHF handheld radio with channel 37 (M) |

- 1200.5.5 1 x E.P.I.R.B operation on 406 MHz. Recommendation 1 per person carried
- 1200.5.6 Tracker System as specified or provided by event organiser.
- 1200.5.7 Adequate first aid kit. 1 x foil blanket + thermal protective aids for each crew member.
- 1200.5.8 Radar Reflector
- 1200.6.1 Crash Helmet compliant with rules
- 1200.6.2 Life jacket compliant with rules
- 1200.6.3 Adequate protective clothing
- 1200.6.4 Whistle, Knife, Compress
- Also the following safety equipment must be attached to the Boat.
- 1200.3.13 Guardrails, handholds
- 1200.3.17 Liquid Magnetic Compass
- 1200.3.20 Engine cut-off attachments for all crew
- 1200.3.21 Battery Isolator switches easily accessible to crew
- 1200.5.4 Fixed VHF transceiver with GMDSS capability, as per rule.
- GPS, RADAR equipment as per Racing Instructions.
- Satellite Telephone. For all Ultra Marathon Races an operational satellite telephone must be carried on board at all times.
- Emergency Grab Bag: An Emergency Grab must be carried on board at all times to include, fresh water, day and night flares, Thermal Protective Aids, First Aid Kit. Recommended to include portable VHF Radio, passports, money and credit cards.

1200.7 - TURN MARKS AND MISSED MARKS

Turn Marks

All marks, including GPS marks will be laid in the approximate positions referred to in the race instructions. It is the competitor's responsibility to follow the geographical course as described in race instructions and to drive close enough to the course marks to ensure that they have been seen to pass them correctly.

Competitors who do not follow the geographical course as described in race instructions will be disqualified from the race or in the event of multi-leg events they will be disqualified from that leg of the race.

The Event Race Instructions will specify the colour and shape of the course marks.

Should a mark go missing the driver must pass through the approximate geographical position referred to in race instructions.

If a boat hits a turn mark – A Yellow Card penalty or other penalties may be applied.

If a boat destroys a buoy or cuts the securing line – A Yellow Card may be applied.
If a boat damages a buoy a fine will be applied.

Missed Marks

A competitor may NOT return to a missed mark. The competitor must continue to follow the designated course passing all remaining marks correctly. Dangerous driving resulting from missing marks will result in further penalties.

The missed mark penalty is as described in race instructions.

1200.7.1 - STOPPING THE RACE

In case of force majeure or an accident, the race may be stopped by waving red flags or the transmission of a group MMSI VHF DSC call. Red flags will be flown from official boats and a group MMSI call if sent will be transmitted from Race Control. On seeing red flags or receiving a group MMSI call from Race Control shall stop racing immediately as is.

On seeing red flags, boats shall stop racing immediately, reduce speed to less than 10 knots and contact race control as soon as possible for further instructions.

In the event of the race being aborted by the use of the Red Flag procedure the finishing positions will be taken as at the passing of the last recorded turn mark.

Any boat deemed to be the cause or a contributory factor in the race being aborted will be disqualified.

1200.7.2 - TIME LIMIT

The time limit applicable for boats finishing a marathon race is based on the following: If the total race distance is 100 nautical miles it is assumed that all boats will achieve an average speed of 25 knots. Therefore the maximum permitted time

for the race is 4 hours. Race Organisers must stipulate the maximum race time in Race Instructions which may be longer than the four hour time limit as given above.

The race may be curtailed or stopped at the finish line at any time by waving the chequered flag. This designates the end of the race.

1200.8 - CLASSIFICATION TABLE

WC7. CLASS CLASSIFICATIONS TABLE – January 2012.

Key: 2S/EPA = 2 Stroke EPA, 4S/SC = 4 Stroke Supercharged, 4S = 4 Stroke, * = Non EPA Outboards

CLASS	LENGTH As per UIM measurement	INBOARD PETROL ENGINES (TOTAL LITRES) (Forced Induction penalty 1.4)	INBOARD DIESEL ENGINES (TOTAL LITRES)	OUTBOARDS (TOTAL LITRES)	Min Installed Engines	LIMITATION MINIMUM WEIGHT (KG per Metre) Including residual fuel at end of race
A	11.3 – 15.3m (37' – 50')	27	27	22	2	410kg/M
B	9.15 – 15.3m (30' – 50')	18	18	8.000 2S 10.500 2S-EPA 9.000 4S/SC 13.500 4S	2	360kg/M
C (Sport Class)	8.23 – 15.3m (27' – 50')	14	14	N/A	2	O/B 280kg/M
C (Stock Class)	8.23 – 15.3m (27' – 50')	13	13	6.000 2S 7.000 2S-EPA 6.000 4S/SC 9.000 4S	2	I/B 328kg/M
MINIMUM 2 ENGINES ENGINE MODIFICATIONS ALLOWED (FREE), CRASH BOXES ALLOWED						

CLASS	LENGTH As per UIM measurement	INBOARD PETROL ENGINES (TOTAL LITRES) (Forced Induction penalty 1.4)	INBOARD DIESEL ENGINES (TOTAL LITRES)	OUTBOARDS (TOTAL LITRES)	Min. Installed Engines	LIMITATION MINIMUM WEIGHT (KG per Metre) Including residual fuel at end of race
D	8.23 – 15.3m (27' – 50')	12.5 (T)	10.0	6.100 2S-EPA 4.200 4S/SC 7.300 4S	1	O/B 197kg/M
		8.5 (S)		I/B 262kg/M		
E	7.35 – 15.3m (24' – 50')	6.5	6.5	5.200 2S-EPA 3.500 4S/SC 5.800 4S	1	O/B 180kg/M
						I/B 246kg/M
F Motor Cruiser	9.15 – 15.3m (30' – 50')	27	27	ALL	2	460 kg/M
G Motor Cruiser	7.35 – 15.3m (24' – 50')	13	13	ALL	2	394 kg/M
H1 Classic	6.7 – 15.3m (22'-50')	18	18		1	No Limitation
H2 Classic	6.7 – 15.3m (22'-50')	8.5	13	8.5	1	No Limitation
SINGLE ENGINES PERMITTED, ADVERTISED 'MAINSTREAM' PRODUCTION PLEASUREBOAT ENGINES ONLY, NO OUTSIDE MODIFICATIONS/OEM TURBOCHARGERS AND SUPERCHARGERS ONLY, SHIFTABLE GEARBOXES, ONLY STANDARD INLET/EXHAUST MANIFOLDS ETC.						

Motor Cruiser (F) cabin capacity minimum H 1.75 x 1m2. Length/Beam ratio < 3.75:1

Motor Cruiser (G) cabin capacity minimum H 1.50 x 0.8m2. Length/Beam ratio < 3.75:1

Classic (H1 & H2) Boats can be conventional or stern drive, surface drive or outboard.

All competing craft must be capable of exceeding 40 knots, except for Historic Class boats. Historic Class boats must have a speed capability of 30 knots up to a maximum of 50 knots.

Owners of historic/classic boats must certify that the average speed of their boat during a race will not exceed 50 knots. 10% more engine capacity than the original installation is accepted. Classic Boats must be at least 25 years old from date of build.

1200.9 - ULTRA MARATHON RACES

Ultra marathon races are defined as races where the total distance at one or more races exceed 500 nautical miles in total.

Alternative Starting Procedure: For Ultra Marathon Races standing or near standing starts will be permitted instead of the procedure defined in Rule 305. Full details of this procedure will be included in race instructions and described at drivers briefing.

Safety requirements for organisers of Ultra Marathon Races

- a) The organisers must establish a central race control location from which a dedicated team can monitor the progress of boats via the boat tracker system.
- b) The organizer must also present risk assessment and an environmental risk assessment to the National Authority at least 90 days prior to the start of the race
- c) The organisers must clearly define the financial implications applicable in the different countries should a boat have to be rescued or team members have to be transported ashore by rescue services.

For all Ultra Marathon Races a tracker system as defined by the race organizer is mandatory.

Satellite Telephone. For all Ultra Marathon Races an operational satellite telephone must be carried on board at all times.

For Ultra Marathon races, the organisers must provide full details of the race entry fees and conditions.

1300 - RULES FOR UIM STOCK

These rules are intended to ensure safe and competitive racing in a race series at a reasonable cost to the participants; to expand and broaden the general base of participation and the enjoyment of leisure craft and to help the marine industry to develop and demonstrate the performance of its products particularly in the use of new environmentally friendly engines. Any development that is contrary to this policy may give rise to a rule change as provided for under these rules.

INTRODUCTION

S1. Concept

- (a) “Offshore Powerboat Racing” means competitions between motorboats on the sea or on open surfaces of water.
- (b) In the Stock series all boats are strictly one design and only boats and engines complying with these rules are eligible.

Permission to race in the series is at the discretion of the series organiser and the organisers reserve the right to refuse entry into the series.

- (c) The boats are constructed in glass reinforced plastic or kevlar and carbon composite. The boats must carry a minimum of two crew members; a driver and a navigator.
- (d) No physical modifications may be made to the hull from the original one-design drawings or specifications however Stock Technical Scrutineers may approve minor running repairs if deemed necessary. No modifications may be made to the engine or engine position as set by the factory and specified. Original hull drawings are available from the series organiser on request and templates will be used at events to ensure compliance with the original design.
- (e) Outside communication during races ***is not permitted***. Crew members ***should rely*** on their own resources to plan and follow the pre-determined course.
- (f) Safety takes priority over racing at all times.

S2. Classification and Approval of Races

- (a) The race calendar is co-ordinated and managed by the series organiser.

LICENCE TO RACE

S3. Offshore Powerboat Racing Licence

In order to enter the Stock series, all crew members must be in possession of a valid International Licence issued by their National Authority.

S4. Age Regulations

- (a) In Stock series the minimum age for the driver is 18 years old and for the navigator is 16 years old.
- (b) All age regulations apply at the date of the race.
- (c) All persons under 18 years of age must be accompanied at the event by a Parent/Guardian. All competitors who are under 18 on the date of the race will be required to submit written consent of their parent or guardian to their participation in the race and confirmation of their acceptance of the rules governing the race.

S5. Control of Boat

- (a) Only valid Licence Holders are permitted to control the boat whilst racing.
- (b) Boats are allowed out on the water at the times designated in race instructions ONLY or with prior agreement with the OOD. Non-conformance may result in disqualification from the event and/or a fine of 100 Euros or as stipulated in race instructions
- (c) Any team with a crew member competing in his or her first Stock events must display fluorescent orange race numbers on either side of the transom. These teams will be positioned furthest from the start boat during their first four events.

BOAT REGISTRATION AND COMPLIANCE

S6. Entries

- (a) All entries must be made via the Official Entry Form and all accompanying terms and conditions complied with.
- (b) Entry of any team into any race of the championship is fully at the sole discretion of the series organiser who reserves the right to refuse entry.

- (c) Entries are accepted as Team entries. Teams are responsible for the behaviour of all team members and their associates. All rules are enforceable if breached by any team member or associates.
- (d) The series organiser has the right to refuse any boat that they consider unsuitable and /or which does not conform to the requirements of the present rules. Reasons for refusal shall be submitted in writing.
- (e) No individual or team who have been expelled from UIM events, or who are currently under suspension by the series organiser or their own National Authority shall be permitted to officiate, serve as a race committee member, act in any capacity of assistance or participate as a boat owner or crew member in any racing event run by the series organiser. No boat belonging to that individual or team shall be allowed to compete, nor shall it be chartered or otherwise transferred except by final sale to render it eligible for a Stock series sanctioned race. The series organiser reserves the right to deny future membership and entry to anyone who has previously been expelled or suspended.
- (f) Race entries are only accepted for the full series unless otherwise agreed with the series organiser in extenuating circumstances.

S7. Obtaining the Boat Race Number

Race numbers are allocated by the series organiser and in conjunction with the local National Authority. See also rule 47.

RACING RULES - GENERAL

S8. Rules for Stock series Races

- (a) The Stock specific rules for boats and crews competing in the Stock series are contained herein. On arrival in race pits/venue/event area, all teams are deemed to be under race rules and regulations. All rules will apply, as will penalties.

S9. Terminology

In these regulations the word “shall” and “must” are mandatory, whereas the word “should” and “may” are advisory.

S11. Interpretation of the Rules

- (a) The Organisers shall judge, in accordance with the current rules, all cases not foreseen or seemingly inaccurately defined. Any driver who infringes any rules will be penalised.
- (b) IF THE RULES SAY “YOU CAN DO THIS.....”, YOU CAN. IF THE RULES DO NOT SAY YOU CAN DO IT, THEN ASSUME THAT YOU CANNOT UNTIL YOU CHECK WITH THE SERIES ORGANISER.

BOAT AND EQUIPMENT

S12. Charts

Valid chart(s) number(s) shall be given to competitors for the course of each race in Race Instructions and these charts should be used to plot the course. Charts are no longer included in the mandatory equipment list unless stated in the Race Instructions. However it is recommended that drivers and navigators are fully conversant with the information available on nautical charts. See Rule 61 (c).

S13. Craning Regulations

- (a) All boats must be fitted with a minimum of three lifting eyes suitably engineered to withstand lifting the boat fully equipped and fuelled with an adequate safety margin. Lifting eyes fitted to the boat during construction must not be altered. The positions of the lifting eyes must be identified using standard stickers as supplied by the series organizer.
- (b) Boat numbers MUST be on the trailer on the starboard side at the hitch. This forms part of scrutineering.
- (c) No persons shall be on board a boat whilst being lifted by a crane.
- (d) Anyone in the craning area MUST wear a hard hat or protective helmet at ALL TIMES. Penalty for non-compliance is a fine of 100 Euros.
- (e) Crane and/or launch times will be scheduled in race instructions or by a launch order issued by race control. This must be complied with to guarantee launch, otherwise competitors will be responsible for their own launch arrangements NB The crane/tractor may not be available outside posted times.
- (f) A minimum of one team member must be present at launch or recovery. All boats must be “launch ready” on arrival at the crane (ropes, fenders on, covers off, etc) otherwise the boat will be refused launch or recovery.

S14. Towing Cleat and Eyes

Towing cleats and eyes shall be of adequate construction and strength for the boat to be towed when waterlogged, and shall be securely fixed to the main hull structure, not merely to the deck.

S15. Painter/Towing Line

Painters and mooring lines shall be of sufficient length and strength for the purpose of towing or mooring. All ends shall be whipped or welded and chafed or worn ropes should be renewed. At least 2 lines shall be carried. Note: Painter length shall be less than the length of the boat to avoid fouling the propeller.

S16. Navigation Lights

Stock race boats do not have to be fitted with navigation lights.

S17. Sharp Edges

All sharp edges above the static waterline shall be adequately protected or removed.

S18. Steering

- (a) The steering wheel shall be secured and locked on to the shaft. No lateral movement of the steering wheel is permitted.
- (b) The steering wheel unit shall be fixed to, or through the dash panel, or a steering mounting bar, and shall be through bolted and locked.
- (c) The steering wheel strengths shall be checked; if the wheel is split or cracked, the wheel shall be rejected. Wheels of the laminated rim type shall be checked for weakness caused by the breakdown of the laminations. Plastic composition wheels shall be checked for early fatigue where the spokes join the boss to ensure the wheel cannot be forced to spin on the internal boss.
- (d) There shall be no undue degree of play in the steering system.
- (e) All steering linkages, quadrants and any fitting to the stock shall be in good condition and secure.
- (f) Engine controls shall operate with full and free movement.

- (g) Hydraulic steering shall be fully operative, hoses and unions shall be free of leaks and adequately protected.
- (h) IT IS RECOMMENDED THAT ALL HYDRAULIC HOSES ARE OF A STAINLESS BRAIDED TYPE, IN ORDER TO MINIMISE THE RISK OF WEAR AND/OR HYDRAULIC FAILURE. COMPETITORS ARE ALSO REMINDED THAT THE CONDITION AND SUITABILITY OF THE EQUIPMENT, STEERING AND OTHERWISE, IS THEIR RESPONSIBILITY AND REGULAR INSPECTION AND MAINTENANCE IS ESSENTIAL.

S19. Engine Cut-Off

- (a) Engine cut-off devices (“kill switches”) for connection to all crew are mandatory (first person out shuts off engine). An emergency override system or additional connection cord and clip to allow the engine to be restarted is mandatory.
- (b) Any kill switch connection cord must not exceed 120cm between the crew member and the boat.
- (c) The emergency cut-off devices must be positioned so that when they operate, the connection cord and cap or clip will not catch or foul. KILL SWITCH CONNECTION CORDS SHALL BE ATTACHED TO ALL CREW MEMBERS AT ALL TIMES WHEN THE BOAT IS RACING, TESTING, PRACTICING or UNDER THE DIRECTION OF THE START BOAT.

S20. Throttle Control

- (a) All foot throttles shall quickly return the engine to idling speed when released.
- (b) The lever unit shall be securely attached.
- (c) The control unit shall be properly connected, work freely and shall not be in a position where it can be fouled.
- (d) The control unit shall be within easy reach of the driver in his normal racing position.
- (e) ***Stock boats should be fitted with either a foot or hand throttle. If the foot throttle is in use, the hand throttle should be disabled. If the hand throttle is in use, the foot throttle should be disabled.***

S21. Other Control Cables

All control cables shall be in good working order, securely fastened and any loose ends taped over.

S22. Seats

- (a) Seats for the driver and navigator shall be fit for purpose, have suitable head support and protection and be firmly secured. Standard seating as supplied by the series organiser must be used at all times.
- (b) Optional Restraints & On Board Air Systems in Stock

Teams may choose to use the optional safety restraint system but if they do both crew members in the same boat must be restrained. All restrained crews must race with a personal air system and must successfully take an annual immersion test. Clearly the decision to use restraints in an open boat is not without risk and all competitors must consider the balance of risk and decide for themselves. Details of these systems are available from the UIM web site www.uimpowerboating.com

S23. Fuel

- (a) Fuels are limited to petroleum-based fuels. Any additives deemed to be performance enhancing by the series organiser or Race Officials are not permitted. The Penalty for the use of performance enhancing additives will be disqualification and/or a 200 Euro fine.
- (b) All fuel must be carried in permanently installed fuel tanks - see Fuel Tanks rule.
- (c) Competitors are reminded to check current or relevant directives regarding the transportation or carrying of fuel.
- (d) Fuel may not be transferred between tanks during a race.
- (e) Fuel transfer outside any designated fuelling areas will result in a 200 Euro fine.
- (f) The unleaded petrol must be as defined by the European Committee for Standardisation (CEN), EN 228 (1993), or other comparable national or regional standards.
- (g) Where fuel is available to purchase from the event marina or other on-water facility, no other fuelling will be permitted.

- (h) In the event of a fuel sponsor agreement, all boats must use the fuel specified in race instructions. In these circumstances boats must arrive capable of taking fuel for a full race distance.
- (i) Except where a fuel sponsor has provided fuel, fuel must be standard road-side or marina available fuel. The fuel must be available to the public and dispensed by standard road-side or marina methods only.
- (j) Fuel sampling may take place during race events.

S24. Fuel Tanks and Lines

- (a) Shall be secure in all directions, in original standard position and shall not leak.
- (b) Fuel tank compartments must be so constructed as to prevent fuel flowing into the cockpit area.
- (c) Shall have practical filling and venting arrangements.
- (d) Shall have an easily accessible and clearly marked means of shutting the fuel supply off. Fuel shut off valves must be clearly marked with identifying stickers as supplied by the series organiser.
- (e) Fuel tanks must be grounded if constructed in alloy or metal to enable the discharge of static electricity.
- (f) Fuel lines shall be leak and chafe resistant and run in a manner to avoid damage.

S25. Engines

- (a) No modifications whatsoever are allowed to engines.
- (b) No engine cowling modifications are permitted other than those approved by Stock Technical Scrutineers. Cowlings must be in place throughout the entire period of a race.
- (c) Infringement of Rules 25(a) or 25(b) will result in disqualification and/or yellow card and/or a fine.
- (d) All engines must be inspected and sealed by the Stock appointed technical representative prior to a published date specified by the series organisers. This pre-season inspection and sealing will be charged. Details of these costs will be provided by the series organiser annually and prior to the start of each

new season. This charge will include the production of the boat's measurement certificate which must be presented at every race event.

- i. If an engine seal is broken prior to arrival at an event, the engine cowl will be sealed in place and any points gained at the event will remain provisional until the engine has been performance tested and inspected.
 - ii. If any work that requires an engine seal(s) to be broken is carried out it must either be done by the series organisers appointed agent or done in the presence of a series organisers appointed scrutineer.
 - iii. It will be the competitors' responsibility to produce their log book at each event during scrutineering. Failure to do so will result in points being held until the engine is deemed legal to race and the log book provided.
- (e) Spare units. Teams are expressly prohibited from carrying a spare engine and replacement units can only be obtained through the series organiser in exchange for the old unit.
- (f) Engines for the Stock series can only be purchased through the series organiser. Privately sourced engines will not qualify for the series. (*)
(* indicates that charges may apply)

S26. Engine Mountings and Transmissions

- (a) Mounting brackets and clamps shall be secure and in a satisfactory condition.
- (b) Engine mountings shall be attached to the transom with at least six bolts.

S27. Engine Hazards

The engine shall be free of dangerous corrosion, oil or fuel leaks or excessive heating likely to be a fire hazard or a danger to any adjacent structure

S28. Electrical Harness

Properly protected terminal boards shall be used with flexible (not solid core) cabling supported close to the terminals and at suitable intervals throughout the length of any cable run. Where relative movement or vibration occurs across any gap, cables shall be sheathed in plastic or metal tube anchored at both ends. Reinforced cable suitable for marine duty shall be used. Electrical equipment in engine compartments shall be kept to a minimum and sited away from heat or fuel.

S29. Batteries

Batteries shall:

- (a) Not be moved from the position as detailed in the boats log book.
- (b) Be secured with fittings sufficiently strong to withstand any anticipated shock or inertial force and housed in ventilated compartments and mounted upon a secure and solid platform.
- (c) Be free of corrosive leaks and well maintained to the satisfaction of the scrutineer.
- (d) Have the positive terminal of all batteries shall be protected against accidental contact.
- (e) Shall be fitted with a clearly marked battery isolation switch in the positive supply line. Battery Isolation Switch must be clearly marked with identifying stickers as supplied by the series organiser.
- (f) In the event of a battery sponsor agreement, all boats must use the battery specified in race instructions.

S30. Engine Well Integrity

All holes cut in the bulkheads of the engine well, for the purpose of passing control cables, etc shall be watertight and shall be as high as possible and shall be above the level of the lowest point of the transom cut-out.

S31. Bilge Pumps and Bailing

- (a) All boats must carry one manual bilge pump and one electrical bilge pump.
- (b) All electrical bilge pumps must be securely fixed to the boat and shall be in proper working order. Secondary manual bilge pumps must be carried and safely stowed during the race.

S32. Buoyancy

- (a) It is mandatory for all boats to have sufficient buoyancy to keep afloat when waterlogged.
- (b) It is the competitor's own responsibility to ensure sufficient buoyancy is fitted at all times whilst racing.

S33. Propellers

- (a) Any propellers used for racing must be registered with the series organiser prior to use for racing.
- (b) They must meet the propeller specifications as given by the series organiser and will be subject to testing.
- (c) Failure to comply with either registration or conformity will result in a fine and/or disqualification.
- (d) Propellers of the first three finishers, in any race, will have their propellers checked; other propellers may be checked at random. Competitors must make them available immediately when requested - failure may result in disqualification.
- (e) Propeller technical specification sheet will be provided by the series organizer prior to the start of the racing season, giving minimum thickness, minimum pitch and minimum diameter details.

S34. Propeller Security

It shall be the responsibility of the competitor to ensure that the propeller is sound, particularly at the blade roots and that it is securely locked by the propeller nut(s).

S35. Minimum boat weight and minimum crew weight

Stock boats shall have the following minimum post race weight of **1625** kilos excluding crew and personal safety equipment.

The minimum weight is the sole responsibility of the team.

The minimum weight shall be calculated post race. All or any boats may be subject to post race weighing and must be made available at the request of the scrutineer and directly after racing finishes. ***No water is allowed in the bow tank at the time of weighing.***

Non-conforming boats will be disqualified.

S36. Fire Extinguisher

All boats shall carry a minimum of one full charged dry powder 2kg fire extinguisher, not two 1Kg extinguishers, sited conveniently and readily accessible to each crew member.

S37. VHF Radio

- (a) EVERY race boat must have on board an operational, multi-channel 25 Watt PEP VHF radio which has channel M2.
- (b) This VHF radio shall be capable of transmitting on the international distress frequency.
- (c) The appropriate ships licences for VHF Radios must be obtained from the local governing body and displayed on board as required by their regulations.
- (d) It is mandatory for one riding crew member to hold a VHF Operators licence.
- (e) It is MANDATORY that the radio be integrated with the intercom in the helmet to enable monitoring of race boat radio channel whilst racing, so as one team member must be able to listen for any instruction at least by use of ear piece connection to radio. Penalty for non-conformance is 100 Euros.

S38. Anchor/Anchor Line

- (a) Anchors shall be of a weight and type adequate to hold the boat and shall be securely stowed in an accessible place.
- (b) The anchor line shall:
 - i. Be of a size and strength appropriate to the boat.
 - ii. Be in good condition.
 - iii. Be at least 50m in length.

S39. Compasses

- (a) There is no mandatory requirement for Stock race boats to be fitted with a magnetic steering compass for the purpose of racing.
- (b) It is permitted to use electronic steering compasses and/or electronic position fixing equipment. It is expressly forbidden to interface any of this equipment to the steering system.

S40. Whistle, First Aid Compress & Knife

All crew members shall carry a whistle and first aid compress, both of which must be attached to them whilst racing, and each crewmember must carry a sharp knife in a protective sheath.

S41. Flares

- (a) Each riding crew member shall carry one mini flare pack (8 flares) about their person.
- (b) Boats may carry further flares. If carried these flares shall be stowed in a conveniently accessible place, shall bear either the date of manufacture or the expiry date, shall be in date and have a minimum burn time of 40 seconds.
- (c) Any flares where the printed instructions and/or date are illegible, or the condition poor, shall not be accepted.

S42. Retirement Flag

- (a) All boats shall carry an orange rectangular flag of a minimum size of 0.60m x 0.40m as a means of announcing retirement from a race
- (b) Stock boats will be fitted with a strobe light and to be used on retirement
- (c) Once a boat has retired it cannot re start or re join the race

S43. Structural State

- (a) It shall be the competitor's responsibility not to offer for scrutiny a boat that has any structural or mechanical defects.
- (b) Stock boats shall be fitted with their racing deck cowl whilst racing. Boats must finish with this cowl in place to be classified as a finisher.

S45. Mandatory Markings/Decals

- (a) All boats must display mandatory series and event logos, as directed by the organisers.
- (b) Failure to do so will result in a fine of 150 Euro per sticker, per race.
- (c) All decals including non mandatory must be intact and in good order at the start of each race.

S46. Protective Clothing

- (a) All crew are required to wear purpose made race overalls whilst on the water and at prize giving ceremonies. Compliance with current FIA homologation is strongly recommended.

- (b) Wet suits are recommended in cold water conditions in addition to race overalls.

S47. Race Boat Number decals

- (a) Legibility - Stock series boats are required to have black numbers over a white background. The font is the choice of the Team - see (c) below.
- (b) Dimensions - numbers shall be a minimum of 300 mm tall, on both sides of the hull, and on the deck (read correctly from the drivers position), and shall be legible for scoring purposes. The minimum stroke shall be 50 mm. The two digits shall be separated by more no less than 50mm and the white border shall be at least 50mm from the digits.
- (c) Location - the deck number panel shall be placed immediately behind the series organiser reserved area on the deck. The hull number panels may be placed anywhere within 3m of the bow.
- (d) It is the responsibility of the boat owner to have numbers that are legible for scoring purposes. If the non standard numbers do not meet minimum requirements, suitable numbers should be installed by competitors or the boats may risk not being scored.

S48. Equipment & Safety

- (a) All safety equipment must be carried on board and fixed/stored to the scrutineer's satisfaction

RACING REGULATIONS

S49. Drivers' Responsibility

- (a) The driver accepts responsibility for every eventuality that may occur as a result of entering a race. It is the driver's sole and ultimate responsibility to decide whether or not to start or continue in a race once he has passed scrutineering.
- (b) It is forbidden to start the motor with the propeller rotating in the air.
- (c) ALL SMOKING OF ANY TYPE IS PROHIBITED IN THE PIT AREAS. Adequate signs will be posted to this effect and the rule will be rigorously applied by pit marshals.

S50. Scrutineers' Inspections

- (a) Scrutineers shall work with the current scrutineering list as a guide and equipment listing (Appendix 1).
- (b) Scrutineering does not constitute a condition survey of the boat.
- (c) The scrutineering sheet may not necessarily cover every item that a scrutineer may wish to check on a particular boat nor does every item necessarily refer to every class of boat. A scrutineer shall reject a boat if it does not comply with all of the requirements of the organiser or the UIM. The scrutineer shall refer any non-compliance to the Race Committee. Any decision related to eligibility shall be made in good time before the start of the race.
- (d) Any incomplete entry may, at the scrutineers' discretion, be put back to be re-examined later if time permits.
- (e) Post Race scrutineering will take place at the discretion of the OOD/Chief Scrutineer. Competitors must make their boats available when requested - failure to do so may result in disqualification. The first three boats in each class in any race may not leave parc ferme or crane area without prior permission of the chief scrutineer.

S51. Engine Eligibility

- (a) The organisers appointed Technical Sub Committee may appoint a number of engine inspectors who can attend any engine inspection or carry out their own checks at any event.
- (b) If a Race Committee decides to check an engine in order to verify its eligibility, the cost shall be borne by the competitor if the engine fails the eligibility check otherwise the cost will be borne by the race organiser.
- (c) When a competitor raises a Protest against another competitor over the eligibility of an engine and in order to prove this eligibility an engine is required to be checked, then the costs involved are to be borne by the losing party. This means that following a Protest, if the engine is found to be illegal the Protestee shall pay for the checks. If however, the engine conforms to all the rules then the cost of checking shall be borne by the Protestor. Both parties to the protest may attend the engine inspection.
- (d) Parts not supplied by the original engine manufacturer shall not be fitted to any engine unless specifically allowed within the rules - See Rule 57 below
- (e) Engine scrutineering checks will be made in comparison with standard production parts and/or UIM engine homologation papers.

S52. Engine Lubricants and Parts

- (a) Spark plugs, engine oil and lower unit oil must be as per the specification in the owner's manual, or as supplied/provided by the organisers sponsor.
- (b) Powerboat P1 will notify all teams of any parts permitted to be changed from those originally supplied by the engine manufacturer.

S53. Pre-Race Declarations

- (a) All crew members who are racing must sign the indemnity form provided by the organiser who will be responsible for the provision of the insurance. Details of this insurance are available from the organiser.
- (b) All competitors who are under 18 on the day of the race will be required to submit written consent of their parent or guardian to their participation in the race and confirmation of their acceptance of the rules governing the race.
- (c) Each boat must designate a representative that is present on event, who is responsible for team equipment, trailers, and vehicles and is the designated point of contact for the team at that event.

S54. Drivers' Briefing, Signing in, All Classes

- (a) Race boat drivers and navigators shall 'sign in' to signify their attendance prior to the start of their briefing. Only crew members and race officials will be allowed to attend this briefing.
- (b) Failure to sign in by the time stated will result in a 100 Euro fine per person.
- (c) Non-attendance at this briefing may result in a 500 Euro fine per person, with a re-scheduled briefing being held at the OOD's convenience.

S55. Starting Procedure

- (a) MINIMUM VISIBILITY.

For Stock races there must be a minimum of one mile visibility over the whole race course and there must be no forecast or other reason to believe that there will be any lesser visibility on any part of the course or its vicinity throughout the expected duration of the race.

- (b) FROM WET PITS AREA

- i. Start Boat - Flashing amber lights signalling 2 minutes to commence leaving Wet Pits or as alternatively described at Drivers Briefing.
 - ii. All race boats to follow start boat to Muster Area in position order as briefed at the driver's briefing. **NO RACE BOAT MAY PROCEED AHEAD OF THE START BOAT.**
 - iii. Red flag raised at wet pits signals wet pits closed. Race boats may not leave the wet pits at this time. A late start procedure may be given at drivers briefing.
 - iv. A parade lap, where possible, around the course to the muster and/or start area will be described at the briefing.
- (c) FROM MUSTER AREA
- I. The Muster Area will be described in the Race Instructions.
 - II. In the muster area, the yellow flag will be raised, meaning that the boats must follow the alignment procedure.
 - III. Start boat - Yellow flag and flashing amber lights (Lights if applicable) - when the Start Boat leaves the muster area, towards the start line, the boats will line up in the pole position order and 30m ASTERN, of the Start Boat, and must observe a safe spreading distance (minimum 3 metres) between boats with absolutely no contact between boats. The penalty for not staying 30m behind the start boat (jump start) will be a 1 minute.
 - IV. A race official on the Start boat will use a radio channel to control the fleet prior to the start. They will have the authority to impose penalties, as detailed below. Radio channel to be announced at drivers briefing.
 - V. When the starter is satisfied that all boats are in a satisfactory line, at a satisfactory speed, he raises the green flag and starts the flashing green lights, lowering the yellow flag simultaneously. The raising of the green flag indicates the start of the race.
 - viii. The start boat speed should be at a speed of approximately 30 knots when the green flag is raised.
 - ix. The official timing starts when the leading boat crosses the start-finish line, unless otherwise designated in racing instructions. Crossing the line constitutes the start of Lap 1 of the race.
 - x. If at any time during the start procedure after the yellow flag has been raised, a red flag is raised, the start has been aborted and all boats must

return, at a safe speed, to the muster area for restart, or follow specific instructions from the start boat.

- xi. Any boat that fails to line up in the muster area will not be allowed to start.
- xii. Should a boat fail to start, having lined up in the muster area, it may start within ten minutes of the time of the start provided the boat:
 - a. Gets permission from the safety officer / OOD to start
 - b. Starts at the start position and
 - c. Follows the start run going to the start/finish line when it is safe to do so.
- xiii. A description of the start will be published in Race Instructions.
- xiv. During the start run, if a boat passes the start boat before the green flag, it will be disqualified.
- xv. Safety takes precedence over racing AT ALL TIMES. Competitors should do nothing to endanger crew, spectators or other participants.
- xvi. Video camera evidence may be used by Race Committee for any reason.
- xvii. Standing starts : Standing or jetty starts may be used, full details will be provided in Race Instructions and confirmed at Drivers briefing.

Diagram 1 - Start procedure under yellow flag

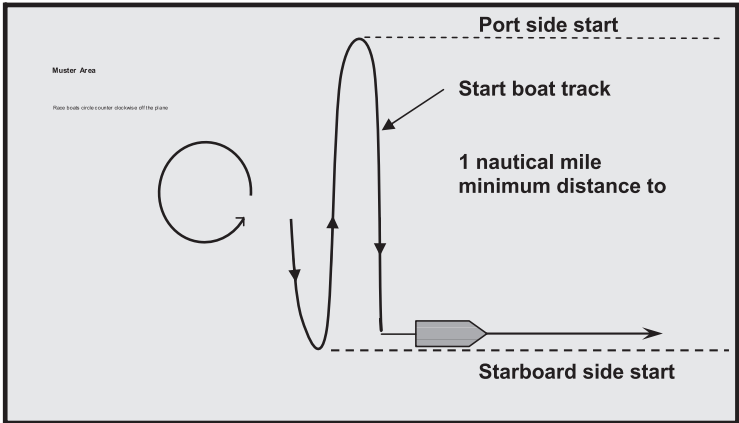
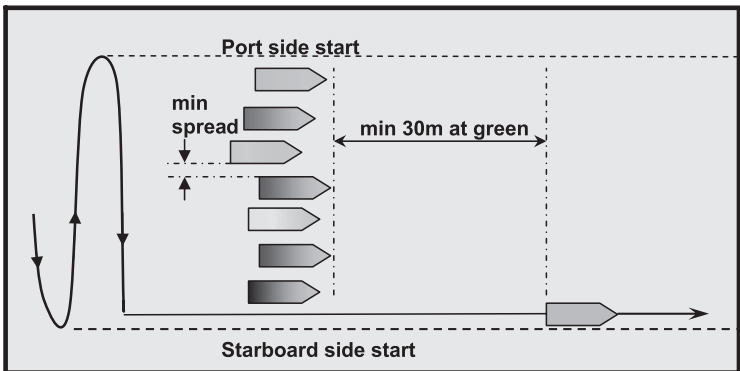


Diagram 2 - Start procedure, green flag



“POLE POSITION”

2.6 Pole Position

2.6.1 *Will be defined by means of a lottery for the first race of the Event. The series organiser reserves the right to use the lottery method of determining pole position order at any event (notified at briefing).*

2.6.2 *Grid positions for racing will be advised at Drivers Briefing.*

The grid for Race 2 (and subsequent races as appropriate) will be reversed with the final race grid being determined from the combined results from all previous races, highest points then achieving pole.

2.6.3 *Teams where either one or both Driver and Co-Drivers on board has not competed and finished in a minimum of 2 races and achieved “competent” sign off by the OOD .Until this is achieved the team will continue to start on the outside of the grid.*

2.6.4 *Pole position will be nearest to the Start Boat.*

S56. Valid Starters

An entry accepted by the organisers and having satisfactorily completed scrutineering, attending all required briefings and having crossed the start line as described in the racing instructions at the prescribed time, under the starting arrangements specified by the race programme or at drivers’ briefing, is considered to be a valid starter.

S57. Outside Assistance

(a) The Race Committee has the right to decide whether there was other outside assistance or not, i.e. safety grounds.

S58. The Racing Rules

General - It is the duty of each crew member to ensure that their boat is driven in a manner that does not compromise the safety of themselves, any other competitor or any spectator. Powerboat racing is a non-contact sport and crew members may be penalised if contact occurs. Whilst not racing or participating in official practice sessions, boats are bound by the International Rules for the Prevention of Collision

at Sea. All Race Boats must observe the International Rules for the Prevention of Collisions at Sea when encountering any other non-racing craft at all times.

- (a) Responsibility of all crew members - It is the responsibility of each crew member to ensure they are fully aware of the consequences of their actions and to anticipate the actions of other boats in their immediate vicinity. All crew members must take any action necessary to avoid a collision, even if they have the right of way. A constant look out must be maintained and crews are reminded of the importance of good and efficient communication at all times.
- (b) Flag Signals - Boats must obey all flag signals given by officials during testing, practice and racing. Failure to comply with flag signals may result in a penalty or disqualification.
- (c) Overlap - An overlap can be attempted and exist on either side. An overlap exists when one boat is not clear ahead of another. See diagrams 1 and 2 below for clarity.

Diagram 1

No overlap

A is clear ahead of B

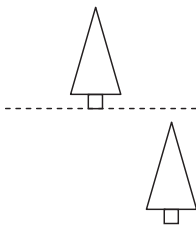
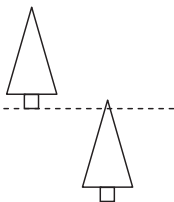


Diagram 2

Overlap exists

A is not clear ahead of B



DEFINITIONS

OVERLAP - An overlap exists when one boat is not clear ahead of another and the overtaking boat has no longer a free choice on which side it shall pass.

OVERTAKEN - An Overtaking boat is deemed to have overtaken when its aftermost point is in front of the overtaken boats bow. If the overtaken boat then closes on the other it then becomes the overtaking boat and therefore must act accordingly.

(d) Rules of the road - Right of way

The international Regulations for the prevention of Collisions at Sea apply at all times between race boats and non race boats.

(e) Overtaking

1. Any overtaking boat must give way to the overtaken boat.
2. A Boat being overtaken must not deliberately alter its course in any way that would hinder the overtaking boat, once an overlap has been established.
3. An overtaking boat that has established an overlap must in all circumstances be prepared to alter its course in order to avoid collision.
4. If a boat, in consequence of its neglect of any of these rules fouls another boat it may be penalized or disqualified.

(f) Fouling Competing Boats

If a boat, in consequence of her neglect of any of these rules compels others to foul each other, she may be disqualified.

Definition of Fouling - By consequence of a boats actions it either collides with another boat, or causes another boat to have a collision, or causes another boat to take avoiding action to avoid a collision which may or may not result in that boat infringing another rule (e.g. - Missing or hitting a mark of the course)

(g) Fouling Marks

If a boat is forced on to a mark by another boat, the driver may lodge a protest.

S59. Incident Procedure

- (a) In the event of a hazard or incident on the course, the yellow flag procedure will be used to control the fleet - see Rule 67. The start boat may also use its yellow lights in conjunction with the yellow flags.
- (b) There is NO OVERTAKING in the vicinity of the yellow flags / yellow lights.
- (c) When it is deemed the course is clear, yellow flags will be removed. If the start boat is showing yellow lights these will be switched off or converted to green lights.
- (d) In the event of a serious incident, the red flag procedure will apply - see Rule 67.

- (e) It is the crew members' responsibility to monitor the all flag signals whilst deployed.

S60. Accident Aid

- (a) It is mandatory for the first competing boat to arrive at the scene of the incident to stop and offer assistance if no qualified rescue assistance is present. All racing will be stopped by the OOD through the Red Flag procedure should a driver or co driver be thrown into the water during the race or should a boat capsize.
- (b) Crews that have stopped to assist at an incident shall immediately advise Race / Safety Control of the incident.
- (c) The team shall inform Race / Safety Control if they resume racing. Times relating to stopping and re-starting racing will be recorded by Race / Safety Control.
- (d) Any boat which stops to render assistance because there was no help or insufficient help will be awarded the championship points related to the position they held at the time they responded to the incident. No other finishers will have their points or positions affected by this action. However, should the boat that rendered assistance restart the race and improve its position from when it stopped to render assistance, it will keep its better position and points,
- (e) Podium places will only be allocated to boats that finish the race in first, second and third place.

S61. Turn Marks and Missed Marks

- (a) All marks will be laid in the approximate positions referred to in the race instructions. It is the competitor's responsibility to drive close enough to the course marks to ensure that they have been seen to pass them correctly.
- (b) Unless otherwise specified in the Event Race Instructions the colour of the course buoys will be as follows:

All Outside Course Marks: YELLOW (to be left to port when racing anti-clockwise).

All Inside Course Marks: ORANGE (to be left to starboard when racing anti-clockwise).

- (c) Should a buoy go missing the driver must pass through the approximate geographical position referred to in race instructions.

- (d) If a boat hits a turn mark - A Yellow Card penalty may be applied.
- (e) If a boat destroys a buoy or cuts the securing line - A Yellow Card may be applied. If a boat damages a buoy a fine of up to 300 Euros will be applied.
- (f) A competitor may NOT return to a missed mark. The competitor must continue to follow the designated course passing all remaining marks correctly. Dangerous driving resulting from missing marks will result in further penalties.
- (g) The missed mark penalty is 1 minute per missed mark.

S62. Stopping the Race

- (a) In the event of a driver or Co-driver being thrown into the water or a capsize the race will be stopped using the Red Flag Procedure.
- (b) In case of force majeure or an accident.
- (c) In the event of the race being aborted by the use of the Red Flag procedure.
- (d) On seeing red flags.
- (e) In the Event of the race being Red Flagged a restart will take place using the standard start procedure as identified in Rule S55. When the red flag is shown if the lead boat has completed more than 70% of the total course then the race may be curtailed at the discretion of the OOD.

S63. Finishing a Race

- (a) When the leading boat in each class completes the course and crosses the finishing line, the chequered flag will be waved. It designates the 'winner' of the race and the start of the finish procedure of the race.
- (b) The chequered flag will also be given to all subsequent boats that cross the finish line after the first boat.
- (c) Boats shall not begin a new lap if they cross the finish line after the leading boat has taken the chequered flag.
- (d) A boat shall be timed for completing a race when her bow crosses the finishing line.
- (e) All boats crossing the finishing line after the 'winning boat' will be classified as having finished the race provided they have covered the minimum number of laps.

- (f) Upon completion of a race, a boat shall;
 - I. turn in to the centre of the course as described in drivers briefing
 - II. conform with the International Regulations for Preventing Collisions at Sea,
 - III. not hinder other boats that are still racing and
 - IV. await escort back to the Wet Pits.
- (g) Time Limit: All boats have a maximum of 15 minutes from the time the winning boat crosses the finish line to complete their final lap.
- (h) The race may be curtailed or stopped at the finish line at any time by waving the chequered flag. This designates the end of the race – see Rule 67 below.
- (i) Boats will be ranked according to the number of laps completed and then the time it has taken to complete those laps.
- (j) The race may be curtailed or stopped at the finish line at any time by waving the chequered flag. This designates the end of the race.

S64. Curtailment of a Race

A curtailment decision can be made at any time during a race provided the curtailment is signalled on the Start/Finish line to the leading boat. The leading boat shall then stop racing immediately. All other boats shall stop racing when they next cross the Start/Finish line. Retiring from a curtailed race prior to curtailment will not preclude a driver from receiving points. See Rule 72 for curtailment flags.

S65. Retirement Procedure

- (a) On retiring from a race, all boats must hoist and display an orange flag when practical to do so until the boat has been recovered from the water. The crew should attempt to contact Race / Safety Control immediately by radio and inform them of the boat's retirement. If radio communication is not possible, the nearest Safety/Patrol boat should be informed.
- (b) A boat which has retired after completing 70% of the laps completed by the winning boat is eligible for the award of championship points.

S66. Championship Points Scoring System - all classes

- (a) Points will be awarded to race boats.

- (b) Points are not transferable into other classes or between boats.
- (c) No points will be awarded if a boat is disqualified for an infringement of the technical rules.
- (d) The championship points will be awarded as follows:

First place 20 points

Second place 17 points

Third place 15 points

Fourth place 14 points

Fifth place 13 points

Sixth place 12 points reducing by one point for each subsequent position.

(e) DNF : 0 points

DNS : 0 points

DQ : 0 points

S67. Flag Signals

The following flags having the meanings shown and will be used in all SuperStock races unless Race Instructions state otherwise:

Flag	Where flown	Meaning
Yellow Flag	From Start Boat	Start procedure underway
	From Safety/Patrol Boat	Incident in vicinity, proceed with care, no overtaking
Green Flag	From Start Boat	Race Start
Red Flag	From Start Boat (during start)	Start Aborted
	From official boats	Race stopped
Chequered Flag	At Finish line	Race finished
Chequered &	At finish line	Race curtailed

Red Flag together

Black Flag & race Boat	From Start boat number	Boat disqualified from race - boat stop racing
White Flag	At Finish line	Last lap indicator
Orange Flag	From competing boat	Retirement
Black Flag with Yellow 'X'	Official Boat	Identification as official boat

YELLOW FLAG - on seeing the yellow flag signal from an official boat, competitors **MUST ACKNOWLEDGE THE SIGNAL** proceed with caution, not overtake other race boats whilst in the area of the incident and keep clear of any danger areas.

Any competitor who ignores or abuses the yellow flag signal may be penalised. The Start boat yellow lights may be used in conjunction with yellow flags. These lights shall have the same meaning as a yellow flag.

RED FLAG - on seeing the red flag signal, all boats shall stop racing immediately, turn in to the centre of the course and await further instructions.

Infringement of any of the above may result in disqualification and/or yellow card and/or a time penalty or any combination.

BLACK FLAG - any race boat deviating from the course into restricted areas, or displaying conduct deemed to be unsporting or dangerous, will be shown the black flag along with its race boat number the next time it passes the start boat and will be disqualified from the race. Disqualified boats must return to wet pits area, safely moving off the course and avoiding any interference with the race or other race boats. Disqualified boats will not be awarded points for that race and may be fined.

S68. Posting of Results

- The provisional results will be posted within an hour of the end of the races, where possible.
- The OOD must sign the results and state the time of posting on the provisional results. The OOD will be present for one hour from the time of posting the results to receive any protests.
- Provisional results will become official one hour after posting provided no protests have been received.
- It is the teams' responsibility to monitor the posting of the provisional results.

S69. Medical Examination

- (a) The Race Committee may instruct the Medical Officer (Doctor) who is present at any Powerboat Racing Event, to submit any crew member entered to a physical, alcohol or drugs test at any time during the event. Anyone refusing any test shall be suspended from the event and reported to the UIM for possible suspension. The maximum blood alcohol level for crew members, mechanics and officials is the same as that defined by the legislation, in the country in which racing is taking place, relating to driving a motor vehicle on the public highway.
- (b) The results of such examinations shall be communicated to the OOD/Race Committee who, based on the report of the Medical Officer, may exclude the crew member from the competition.

S71. Gross Misconduct and Unacceptable behaviour

- (a) In order to protect the interests all those involved in the sport, the following actions can be penalised by the OOD, the UIM Commissioner or by the National Authority of the Licence Holder;
 - II. any deliberate act taken to gain unfair advantage,
 - III. any false act made or statement given,
 - IV. any attempt to bribe or the taking of a bribe,
 - V. any abusive or unsportsmanlike behaviour,
 - VI. any instances of verbal or physical abuse against any race or event official.
- (b) Penalties may include yellow cards, red cards, suspension or disqualification from an event, exclusion from the Championship and the imposition of a points or financial penalty.

S72. Yellow Cards, Red Cards and Associated Additional Penalties

- (a) Yellow and red cards can be given to any crew member by the OOD or the UIM Commissioner.
- (b) Up to two yellow cards can be given for each incident of dangerous driving and a red card can be given for any incident of extreme blatant dangerous driving.
- (c) A crew member given a red card is immediately prevented from taking any further part in the event, will lose the right to take part in the next Stock series

event and their team will also lose the right to take part in the next Stock event. Thereafter the red card expires.

- (d) A crew member with three or more yellow cards will lose the right to take part in the next event, after which all yellow cards expire.
- (e) All yellow and red cards will be confirmed in writing and will be posted with the results. Any crew member issued with a yellow or red card has sixty minutes from the time of posting the results in which to lodge a protest.
- (f) The organiser shall inform the National Authority of the crew member / UIM within three days if any cards are issued.
- (g) Each yellow card is valid for 12 months from the date of issue unless removed following suspension from an event - see (d) above.
- (h) Yellow and red cards will be recorded in the Commissioners report.
- (i) Yellow and red cards may be issued on the evidence of any media/TV footage.
- (j) Associated Additional Penalties

In addition to the above, the associated additional penalties listed in the table below will be applied. If the resulting associated additional penalty includes a time penalty, then the appropriate time will be added to overall elapsed race time if the card is given for an on water offence.

Penalty Card	Number of offence	Associated Additional Penalty
Yellow Card	First yellow card	100 euro fine
	Second yellow card	200 euro fine plus 1 minute penalty
	Third yellow card	300 euro fine plus 2 minute penalty
Double Yellow Card	First double yellow card	250 euro fine plus 2 minute penalty
	Second double yellow card	400 euro fine plus 3 minute penalty
Red Card	First offence in calendar year	500 euro fine and disqualification from that event.

Second offence in calendar year	500 euro fine disqualification from the rest of the series for the crew member and the team.
---------------------------------	--

S73. Championship Specific Penalties

At the discretion of the OOD, a yellow card penalty may also be applied in conjunction with the penalties listed below. A written penalty notice will be given and the time of the penalty written on the notice.

- (a) Smoking in the defined wet or dry pits: 200 euro fine.
- (b) Fuel transfer outside the designated fuelling areas: 200 euro fine.
- (c) Launching: persons on boats whilst being lifted or craned: 200 euro fine to the driver of the boat.
- (d) Failure in launch/ramp discipline;
 - i. first offence: warning and/or 100 euro fine,
 - ii. second offence: 200 euro fine.
- (e) Late or non-attendance of prize winners at the prize giving, without previously tendering apologies to Race Secretary: 150 euro fine
- (f) Not wearing race overalls at podium presentation: 150 euro fine.
- (g) Failure to present engine or craft for technical examination: disqualification.
- (h) Misrepresentation or providing untrue statements: See Rule 76
- (i) Failure to raise and fly retirement flag when able: 100 euro fine.
- (j) Failure to report Retirement to Race / Safety Control: 100 euro fine
 These penalties are not exclusive and do not protect the offender from further action by the organisers / UIM. Penalties will be applied by the OOD. Penalties (a) to (j) above may be protested. Protests must be lodged within one hour of the penalty being issued.

S74. Failure to pay any fine

Teams will not be allowed to participate in the next event of the Stock series until all outstanding fines are paid. This will be carried over between seasons if applicable.

S75. Injured Drivers

If during an event a crew member is injured their licence may be suspended until the competitor has been given the all clear by a suitably qualified medical doctor.

S76. Identity Tags

- (a) All crew members will be issued with waterproof temporary identification which must be worn on the right wrist at all times during racing or testing.
- (b) These identification tags may be collected at the boat retrieving ramp/crane area after the final race to validate the driver/crew/boat combination.

S77. Podium Presentations

All podium finishers must wear their race overalls and any race winners caps provided. Failure to do so will result in penalties as detailed above.

Appendix 1 – Scrutineering form

Scrutineering Check List

No	Item	Fine	Race 1		Race 2	Race 3	Post
			Rule Number	Stock			
1	Event Race Instructions			✓			
2	Ship Operators Radio License (race control)			37			
3	Charts (Admiralty / Imray) As Per Instructions *			12			
4	Race Boat Numbers (Timekeepers?)			7 & 47			
5	Stock Powerboat Decals *			45			
6	Trailer Number/Jockey Wheel			13 (b)			
7	Event Sponsorship Decals (If applicable) *			45			
8	Towing Eye			14			
9	Sling Points, Lifting Eyes, All Cleats			14			
10	Strops & Certification (If applicable) & Ident			✓			
11	Transom Plug/Drain			✓			
12	Engine Mountings			26			
13	Reverse Gear			✓			
14	Standard Skeg Shape			✓			
15	Engine Trim Tab & Anode			✓			

16	Steering System								18
17	Controls & Cables								18, 20 & 21
18	Electrical Harness & Wiring								28
19	Fuel Tanks, Lines, Earthing Wire, Shut Off Valve								24
20	Battery Installation & Isolator Switch								29
21	Bilge Pump System & Bailor								31
22	Engine Cut Off & Connection Cord								19
23	Compass (Magnetic)								39
24	Compass (Electronic/GPS)								39
25	Navigation Lights (Port/STBD & Stern)								16
26	Strobe Light (300 and P12855)								42
27	Buoyancy								32
28	Seating, Mouldings, All Rails								22
29	All Sharp Edges or protrusions								17
30	Anchor & Line (50m Minimum)								38
31	Painter/Towing Line (2 off)								15
32	Orange Flag (Both classes)								42
33	Fog Horn								44
34	Fire Extinguisher (2kg <u>NOT</u> 2 x 1kg)								36
35	Flares for Boat								41
36	Personal Flares								41
37	Whistle, First Aid Compress, Knife (Per person)								40
38	Protective Clothing								46
39	Radio (M2, P4, 16) Facility +TEST+								37
40	Propeller Number								33
41	Weight								35

A

After the event	326
Age limit	203.04 - 702.1
Altering course	313.10
Anchoring (during a race)	317
Appeal (board)	402.03 - 405.01
Appeals (protests and)	405
Approaches to the starting line	305
Arbitration	409
Assistance (outside)	319
Assistance (accidents)	206
Authority (change of National)	120.02
Automatic penalties	406
Automatic throttle cut-off device	706
Awarding of prizes	204.01.16

B

Base (course)	300 - 604
Boat (ownership)	203.05
Boats (equipment)	503
Boats (safety)	205.08

C

Calculation of speed	605
Calendar	101
Cancellation of licence	120.06
Cancelled (race)	101.04 - 309

Canopies	508.03 - 703.8.1
Carbon-monoxide alarm	508.24
Cards (yellow)	406.03
Certificate (measurement)	501
Certificates (performance)	614
Championship (World)	800
Championship (Continental)	800
Championship (World Class 1)	860
Change of National Authority	120.02
Circulation of the result (record)	615.02
Circulation of the result (championship)	800.14
Class, hull and engine rules (Class 3)	730 - 735
Classes (International)	500
Classes eligible for records	601
Cockpits	508 - 703.9
Commissioner (UIM)	201.08
Committee (organising)	201.01
Committee (powers of the race)	201.01
Committee of appeal	402.03
Communication officials	201.02.10
Consumption of alcohol	205.02.01
Control (boats by race committee)	701.09
Cost of measurement	501.01/09/10
Course Officials	201.02.5
Course (minimum depth)	800.13
Cranage	209 - 703.7.1/2

Crew safety	508
Crew	702
Cut-off device	706.1

D

Deadline dates	101.03
Deadlines for penalising	406.02
Deck	703.1 - 703.2
Deputy Officer of the Day	201.02.2
Diesel (definition of)	504.03
Disqualification	406.04
Distance record	611
Doping test	205.02.02
Drivers meetings	204.2
Drivers representative	120.08
Duties of the Course Officials	201.02.5
Duties of the Safety Officer	201.02.2
Duties of the Technical Officers	201.02.4
Duties of the Timekeepers	201.02.7

E

Engines	502.04 - 704 - 705
Engine compartments - well	704.3 - 707
Engines class 3 outboards	734 - 735
Engine cut-off	706
Entries	202.02.11 - 203
Equipment of boats	503

Event (after the)	326
Exclusion	406.07
Exhausts	704.1

F

Fee (calendar)	101.01
Fee (entry fee)	203.03
Finishing a race	320 - 800.7.1/2/3
Finishing line	320
Fire extinguishers	508.24 - 704.4 - 715.06
Floatation	501.12 - 733
Fouling competing boats	315
Fuel	504.04/...17
Fuel test procedure	504.06
Fuelling	205.14

G

Gearcase	735.16 - 735.21 - 735.30
Guard rails	703.4
Gyros	709

H

Hand signals	206.02
Helmet	205.07
Homologation of engines	505
Homologation of records	615
Hour records	612
Hull measurement	501.11

Hull specifications	703
Hulls (inspection of)	324 - 502 - 607.04
Hulls (dimensions and minimum weights - class 3)	730 - 731 - 732

I

Immersion testing	508.01
Inboard engines	704 - 705
Injured drivers	205.05
Inspection of engines and hulls	324 - 502 - 607
Insurance	205.09
Interested parties	405.02
International offshore rules	700
International races recognised by the UIM	103 - 104 - 105
Interpretation of the rules	200.02

J

Jurisdiction	400
--------------	-----

L

Length (minimum/maximum class 3)	501.08 - 730.1 - 731 - 732
Licences (list of)	120.02
Licences (rules for racing licences)	120
Licences (validity of)	120.03

M

Marathon racing class rules	1200
Margin required (records)	606
Marks (rounding and fouling)	313.09 - 314

Marks of nationality	711
Marks of the course (definition)	303
Measurement certificate	501
Medical and rescue officials	201.02.9
Medical examination	205.02 - 205.03 - 205.04
Medical Officer	201.02.9
Minimum age	203.04 - 702.1
Minimum requirements (venues)	855
Modifications allowed (class 3)	734 - 735
Monetary penalties	200.03
Monohull definition	502.03
Mooring cleat	703.5

N

National Authority (change of)	120.02
National flag	711
Nationality (foreign)	203.02
Nationality of the drivers	203.05
Non approved races	104
Number (race)	710

O

Obstacle (definition)	300.01 - 313.08
Officer of the Day	201.02.1
Organisation of races	200
Organisation of records	600 - 608
Organising body	201

Outside assistance	319
Overlap	313.07
Ownership of boats	203.05

P

Penalties	406
Petrol (definition of)	504.18
Performance certificates	614
Posting of the results	323
Postponed races	310
Procedure (protest)	403
Programme (race)	204
Propulsion	318 - 708
Protecting helmet	205.07
Protection (technical)	205.10
Protective clothing	205.13
Protest fee	403.02
Protest form (appendix to)	400
Protest (and appeals)	403
Protests (timing)	403.04
ProVee	960
Pseudonym	120.07
Publicity (on boats)	203.06
Pit officials	201.02.8

Q

Qualification	306
---------------	-----

R

Race (finishing a)	320
Race (stopping of a)	312 - 320.02
Race cancelled	309
Race curtailed	312.01
Race number	710
Race postponed	310
Race programme	204
Race Secretary	201.02.3
Race shortened	311
Race start	305
Races recognised by the UIM	103 - 104 - 105
Racing instructions	204
Racing licences (rules)	120
Racing rules	300
Racing vest	205.06 - 508.25
Record Officials	609
Record permit	608
Record results (circulation)	615.07
Records (rules)	600
Reinforced cockpit area and Canopy	508.03 - 730.2
Reports (after the event)	326
Restraints (class 3)	508.29 - 508.30
Results (posting of)	323
RIB racing class rules	1100
Rounding marks	314

Rules (interpretation of the)	200.02
Rules of the road	313
Running aground	316
Running production changes	507.01

S

Safety boats	205.08
Safety equipment	205.12 - 715
Safety of the crew	508
Safety Officer	201.02.2
Safety rules	205
Scoring system	321 - 800.10
Shaft guards	704.2
Smoking (prohibited)	205.11
Spectator areas	303.02
Speed records (calculation)	605
Speed records	610
Sprint power slalom	330
Start	305
Starting (boat starting before time)	305.16
Starting officials	201.02.6
Steering position	703.3
Stock rules	1300
Stopping of a race	312 - 320.02
Suspension (prolonged)	406.06
Suspension (temporary)	406.05

T

Team Representative	203.07
Technical Officials	201.02.4
Technical protection	205.10
Technical rules, classes	500
Time (of protest)	403.04
Timekeepers and timing devices	201.02.7 - 301
Transmission (class 3)	708.04

V

V24 Class Rules	950
-----------------	-----

W

Water pick-up (class 3)	734.6 - 734.12 - 735.16 - 735.30
Weight (minimum class 3)	730.1 - 731
Windshield	703.8.2
Wings	502.02
World championship	102 - 800 - 855

Y

Yellow cards	406.05
--------------	--------

THE RACING RULES OF OFFSHORE POWERBOATING CLASS 1

2014

CONTENTS

Introduction	377
Part 1 Fundamental Rules	378
Part 2 General Application	379
Part 3 Event Rules	383
Appendix 1 Recommended Safety Procedures and Guidelines	465
Appendix 2 On-Water Procedures and Guidelines	473
Appendix 3 On-Land Procedures and Guidelines	481
Appendix 4 Turn Buoys Clarification Drawings	488
Appendix 5 Start Procedure drawings	492
Appendix 6 Technical Clarifications	494
Appendix 7 Digital Boat and Event Logbook	535

INTRODUCTION

The Racing Rules of Class1 include three main sections. The first, Parts 1–3, contains rules that affect all competitors. The second Part provides details of rules, rules that apply to Class Promoter racing which affect only competitors or officials.

The third Part Appendices 1–6, provides details of rules and Guidelines required for the events.

No expressed or implied warranty of safety shall result from publications of, or compliance with these rules and/or regulations. They are intended as a guide for

the conduct of the sport and are in no way a guarantee against injury or death to participants, spectators, or others.

The racing rules are revised and published every year by the (UIM), the world governing body for the sport.

The situations that the Blue pages do not cover will rely on the White page rules. In the case of a conflict between the White pages and the Blue pages, the Blue pages prevail.

PART 1

FUNDAMENTAL RULES

01. FAIR RACING

02. ACCEPTANCE OF THE RULES

03. DECISION TO RACE

04. BANNED SUBSTANCES AND METHODS

01. FAIR RACING

A driver and team owner shall compete in compliance with recognized principles of sportsmanship and fair play. A driver may be penalized under this rule only if it is clearly established that these principles have been violated. A disqualification under this rule shall not be excluded from the boat's series score.

02. ACCEPTANCE OF THE RULES

By participating in a race conducted under these racing rules, each driver and team owner agrees

- (a) to be governed by the rules;
- (b) to accept the penalties imposed and other action taken under the rules, subject to the appeal and review procedures provided there in, as the final determination of any matter arising under the rules; and
- (c) with respect to such determination, not to resort to any court or other tribunal not provided in the rules.

03. DECISION TO RACE

The responsibility for a driver's decision to participate in a race or to continue racing is his alone.

04. BANNED SUBSTANCES AND METHODS

A driver shall neither take a substance nor use a method banned by the Olympic Movement Anti-Doping Code or the World Anti-Doping Agency, unless a T.U.E. has been granted.

PART 2 **GENERAL APPLICATION**

SECTION A

01. GENERAL APPLICATION TO ALL EVENTS
02. GENERAL PRE-RACE REQUIREMENTS FOR CONDUCT OF EVENTS
03. COMPLIANCE WITH RULES AND REGULATIONS
04. INTERPRETATION OF THE RULES
05. AWARDS AND PRIZE MONEY
06. PAYMENT OF FINES
07. CONSUMPTION OF ALCOHOL
08. ANTI-DOPING REGULATIONS

SECTION B

09. INDIVIDUAL REQUIREMENTS FOR PARTICIPATION IN AN EVENT

SECTION A

01. GENERAL APPLICATION TO ALL EVENTS

Unless otherwise specified the following General Racing Rules apply to all UIM Races/events, and Special Events and time/speed trials.

A Race is defined as from start of Registration time on Day 1 to end of Final Day closure of Race Administration, unless stated otherwise within the UIM Rules.

All stated numbers of days for any post race correspondence is considered to be in accordance with the International dateline and not the race/event local time.

02. GENERAL PRE-RACE REQUIREMENTS FOR CONDUCT OF EVENTS

Event Participation - Teams are encouraged to participate in all media opportunities, autograph sessions, sponsor parties, parades, etc.

03. COMPLIANCE WITH UIM RULES AND REGULATIONS

Race Teams must comply with all UIM rules and regulations established for each particular Event, including speed zones, testing times, and testing only in designated areas, if applicable. The teams shall be solely responsible for checking with the appropriate UIM Officials at the Event during registration as to the applicable rules and regulations for testing at the event. The team must obtain the Race Instructions.

Any telemetry/locator GPS/Data or TV/Video recorder(s) required by the UIM to be fitted to the race boats must remain in place and removed or its content reviewed only with the permission of the Chief Technical Commissioner.

Violation of this rule may result in penalties including, but not limited to, fines, time penalties, disqualification and suspensions of not less than 30 days.

Ignorance of the rules is no excuse for not complying with the rules.

04. INTERPRETATION OF THE RULES

In all cases not foreseen by the international rules or seemingly inaccurately defined, any decision has to be taken bearing in mind the spirit of the rules. Decisions on interpretation will be taken by the UIM Commissioner and /or the Race Jury or the UIM.

Any telemetry/datalogging device or GPS transponding and/or TV/Video equipment required by the UIM to be fitted to all competing race boats is deemed to be the official technical recording of the race. Team owned devices are not accepted as evidence.

Television/Video evidence may be used for either the correction of a race decision, or implementing a change to provisional results up to 5 days after the posting of results.

05. AWARDS AND PRIZE MONEY

No prize money shall be distributed to any driver until protests have been completed and the official order of finish is posted by the race committee in that class.

Any boat which receives prize/start money and has an outstanding debt with UIM/Event Promoter will have that debt taken out of the prize/start money that is due them. If fees are paid by cheque at the race site, prize/start money may not be paid until the cheque clears.

The promoter may designate special awards for outstanding performance.

A minimum prize fund may be established and announced in the Race Instructions.

Trophies will be awarded to the first three finishers. It is the responsibility of the competitors to accept their trophy at the awards ceremony or have a representative accepting on their behalf. UIM will not be responsible for the trophy after the event.

All rules in the rule book, and all requirements of the promoter must be met, in order to participate in the prize fund or start monies.

06. PAYMENT OF FINES

All fines must be paid to the UIM by the end of the race. If the fine has been issued in conjunction with penalties qualifying for appeal, then the appeal must be written and properly filed within the specified time limit for appeals. Fines that remain unpaid after 14 days shall be subject to additional penalties. Post race/event issued fines must be paid within 14 days of notice being issued or will become subject to additional penalties.

07. CONSUMPTION OF ALCOHOL

No contestant shall participate in a race or practice, nor shall any official serve in an official capacity, while under the influence of intoxicants, or any dangerous drug, which shall include narcotics, depressants, stimulants, or hallucinogenic drugs. The race officials (Officer of the Day or the UIM Commissioner) shall suspend for minimum of six (6) months any competitor or official whom he believes to be under

the influence of intoxicants or dangerous drugs as defined above or during the time the competitor arrives at the race site, on race day, until the competitor has been cleared through post-race inspection and for one hour after the race is over. No alcoholic beverages are to be consumed until one (1) hour after the unofficial results are displayed. No contestant or official participating at a UIM event may evade this rule by withdrawing his entry or by resigning his duties while at the event. Ceremonial champagne or equivalent usage in the winner's circle shall be permitted provided that only minor consumption occurs.

A competitor shall, if requested by an authorized officer, submit to an alcohol test. Written notification of selection for testing will be handed to a competitor by an authorized official. Refusal or failure to do so may be taken as if a positive test had been ordered and dealt with accordingly.

Testing when carried out shall be using an Alcometer.

For International Races, any competitor found to have more than BAK 0,10 0/00 (0,10 g/L blood // AAK 0,020 mg/L air) 1 hour before and during testing or race shall be immediately suspended and disqualified from the whole event. A formal report shall be forward to the N.A. of the competitor.

If in an organizing country, the legal requirements of the above numbers are lower, then these numbers have to be announced in the advance regulations and will prevail.

08. ANTI-DOPING REGULATIONS

The Anti-doping regulations are in line with the World anti-doping code as published in the UIM Offshore Rulebook.

SECTION B

09. INDIVIDUAL REQUIREMENTS FOR PARTICIPATION IN AN EVENT

Releases - All participants in each event must execute a liability release to UIM, prior to racing. No owner, driver, contestant or his representatives or any race official shall hold any other owner, driver, competitor or his representatives or any race official liable for any personal injuries or damage resulting from an accident occurring during a sanctioned race, except as the result of a deliberate collision or premeditated act of unsportsmanlike conduct. The interpretation of this shall rest with the UIM Commissioner subject to review by the UIM.

Suspension, Expulsion - No individual who has been expelled from the UIM or its events or who is currently under suspension by UIM shall be permitted to officiate, serve as a race committee member, act in any capacity of assistance, or participate as a boat owner, driver or crew member in any sanctioned racing event. No boat belonging to that individual shall be allowed to compete, nor shall it be chartered or otherwise transferred except by final sale to render it eligible for a sanctioned race. The UIM also reserves the right to deny future membership to anyone who has previously been expelled or suspended from any category of UIM.

Medical / Rescue, Tow and Patrol Boat Coverage - LIFE JACKETS/ BUOYANCY SUITS - During all official times, all assistance / safety / rescue boat teams and all teams in support boats must wear life jackets.

The race organiser may or may not provide medical and rescue coverage for testing prior to the race event. If medical and rescue coverage is to be provided, a schedule will be posted at Race Registration detailing times and specific location of covered testing.

PART 3 **EVENT RULES**

SECTION A

01. RACE ORGANISATION AND SANCTION
 - 01.01. ELIGIBILITY
 - 01.02. RACE DATES
 - 01.03. SANCTION/EVENT HOSTING FEES
 - 01.04. REQUIREMENTS
02. TITLE CHAMPIONSHIP
03. CHAMPIONSHIP POINTS
04. CHAMPIONSHIP
 - 04.01. CHAMPIONSHIP TIES
05. CREW/BOAT COMBINATION
 - 05.01. NATIONALITY

SECTION B

LOCAL ADMINISTRATION/ ORGANIZATION

- 06. RACE COMMITTEE MEMBERS AND ROLES
 - 06.01. THE OFFICER OF THE DAY (O.O.D.)
 - 06.02. LOCAL SAFETY OFFICER
 - 06.03. JURY BOAT COORDINATOR AND COURSE OFFICIALS
 - 06.04. PIT OFFICER
 - 06.05. EVENT SECRETARY

07. EVENT INSURANCE

SECTION C

RACE ADMINISTRATION

- 08. RACE REQUIREMENTS
 - 08.01. PIT REQUIREMENTS
- 09. ADVANCE NOTICE
- 10. RACE INSTRUCTIONS

SECTION D

- 11. RACE MANAGEMENT AND UIM OFFICIALS
 - 11.01. UIM COMMISSIONER
 - 11.02. SAFETY COORDINATOR
 - 11.03. ASSISTANT UIM COMMISSIONER
 - 11.04. CHIEF TECHNICAL COMMISSIONER
 - 11.05. RACE ADMINISTRATOR
 - 11.06. OFFICIAL RACE TIMING
 - 11.07. O.P.C. TASK FORCE REPRESENTATIVE
 - 11.08. PROTEST JUDGE
- 12. RACE REGISTRATION (ON-SITE)
- 13. PRE-RACE INSPECTIONS
 - 13.01. REQUIREMENTS
- 14. PRE-RACE TECHNICAL RULES VIOLATIONS
 - 14.01. NOTICE TO OWNER
 - 14.02. CORRECTION OF VIOLATIONS
 - 14.03. PROTEST AND APPEALS
 - 14.04. GENERAL INSPECTION
- 15. TEAMS REPRESENTATIVE
- 16. DRIVERS REPRESENTATIVE

17. TEAM MANAGERS

SECTION E

- 18. PILOTS' BRIEFINGS
 - 18.01. ATTENDANCE
 - 18.02. PILOTS' BRIEFING TIMES
 - 18.03. ALCOHOLIC BEVERAGES
 - 18.04. UNIFORMS

SECTION F

- 19. UIM LICENCE
 - 19.01. ISSUE OF SUPERLICENCE
 - 19.02. TEST DRIVER LICENCE
 - 19.03. COCKPIT EVACUATION / IMMERSION TESTING
- 20. PARTICIPATION RULES

SECTION G

CLASS 1 SAFETY RULES

GENERAL APPLICATION

- 21. REQUIRED SAFETY FACILITIES
 - 21.01. MEDICAL/RESCUE, TOW AND PATROL BOAT COVERAGE
 - 21.02. SEA SAFETY
- 22. FUELLING
- 23. SMOKING
- 24. RACE OVERALLS AND PERSONAL SAFETY WEAR
- 25. RADIO COMMUNICATION EVENT - PRACTICE / POLE POSITION / RACE

SECTION H

GENERAL REQUIREMENTS, PRACTICES AND PROCEDURES FOR CONDUCTING RACES

- 26. BOAT PARADE
- 27. TESTING/PRACTICE
- 28. POLE POSITION

- 29. RACE COURSE
 - 29.01. RACE COURSE LENGTH AND RACE COURSE DESIGN
 - 29.02. RACE LENGTH
 - 29.03. LONG LAP
 - 29.04. BAD WEATHER
 - 29.05. START CHUTE
 - 29.06. TURN MARKS
 - 29.07. TURN MARKS AND/OR ABSENT OR NON-CONFORMING TURN
BOAT OR BUOY
 - 29.08. RIGHT OF WAY
 - 29.09. SPECTATOR AREAS
- 30. LAUNCHING
- 31. CRANING
- 32. PIT STOPS
 - 32.01. DEFINITIONS
 - 32.02. PIT STOP RULES
- 33. OUTSIDE ASSISTANCE
- 34. START PROCEDURE
- 35. FINISHING PROCEDURE
 - 35.01. STOPPING THE RACE
 - 35.02. CURTAILING THE RACE
 - 35.03. STOPPED / CURTAILED RACE
 - 35.04. FINISHING PROCEDURE
 - 35.05. 70% RULE
 - 35.06. TIME LIMIT
 - 35.07. NO FINISHERS
 - 35.08. RETURNING TO THE PITS
 - 35.09. RETIREMENTS
 - 35.10. POST RACE
- 36. POST RACE TECHNICAL INSPECTIONS
 - 36.01. ALL BOATS SUBJECT TO
 - 36.02. CHIEF TECHNICAL COMMISSIONER'S DISCRETION
 - 36.03. FINAL RESULTS
 - 36.04. ENGINE INSPECTIONS
 - 36.05. REFUSAL OF INSPECTION
- 37. POSTPONEMENTS
- 38. PRIZE GIVING

39. LAY DAYS

40. RESCHEDULING OF RACES

SECTION I

BOAT RULES

41. REGISTERED BOAT

41.01. NATIONAL FLAG

41.02. RECOVERY OF DAMAGED / SUNKEN RACE BOATS

42. SPONSOR DECALS

42.01. SERIES DECALS

42.02. UNAPPROVED DECALS

42.03. IDENTIFICATION (NAME AND NUMBER) & HULL

43. RACE NUMBER DIMENSIONS

43.01. SIMILAR BOAT IDENTIFICATION

43.02. REGISTERED HULLS

44. HULL DIMENSIONS AND MINIMUM WEIGHTS

44.01. MEASUREMENT CERTIFICATE

44.02. NEW BOAT CONSTRUCTION

44.03. ESCAPE HATCH

44.04. SUBSTITUTE HULLS (New Boats)

44.05. NEW NON-SUBSTITUTE OR REPLACEMENT HULLS

44.06. CANOPY RULES AND CREW

SECTION J

TECHNICAL RULES REQUIREMENTS

45. ENGINE SPECIFICATIONS FOR REGISTRATION

45.01. CAPACITY AND TYPE RESTRICTIONS

45.02. COMPRESSION RATIO MAXIMUM ALLOWABLE

46. RESTRICTOR, BLOW-OFF VALVE & AIR BOX SIZES
AND SPECIFICATIONS

47. ENGINE QUANTITY & POSITION

48. FUEL & INJECTION

49. ENGINE RESTRICTIONS (non compliance will result in Disqualification)

50. EXHAUSTS

50.01 MAXIMUM PERMITTED NOISE LEVELS

51. TRANSMISSION RESTRICTIONS

52. PROPELLERS

53. CREW CONTROL AND SYSTEMS

54. DATA TELEMETRY

55. ENGINE BONUS POINTS

EVENT RULES

The following General Racing Rules apply to all UIM-sanctioned Class 1 Events.

The Rules and/or Regulations set forth herein are designed to provide for the orderly conduct of racing events and to establish minimum acceptable requirements for such events. These rules shall govern the condition of all Class 1 events and by participating in the events, all UIM members are agreed to abide by these rules. No expressed or implied warranty of safety shall result from publications of, or compliance with these rules and/or regulations. They are intended as a guide for the conduct of the sport and are in no way a guarantee against injury or death to participants, spectators, or others.

All Race event rules and criteria/requirements may be subject to venue specific requirements and National Authority or geographic conditions that result in changes to suit as contained in race instructions, bulletins or changes on safety concerns.

A digital Event Logbook approved during 86th UIM G.A. must be implemented as from 2014; the web program is based on a modern Internet technology, logging all information available including the mandatory by- rules. The digital Event Logbook will be cloud-based with direct access from the UIM website, providing secure access through use of dedicated user credentials and all traffic will be SSL encrypted. (See details on APPENDIX 7)

SECTION A

01. RACE ORGANISATION AND SANCTION

01.01. ELIGIBILITY

In order to be eligible to organize a Class 1 event, a Local Organizing Committee must be sanctioned by the Promoter and recognized by the UIM. The National

Authority or the Local Organising Committee must be recognised by the UIM, which must authorise the event, ensuring that the minimum requirements established in mutual agreement with the promoter are in place.

01.02. RACE DATES

Candidatures for any Class 1 World Championship event must be addressed to the Promoter. Event dates will be agreed between the UIM and the Class 1 Promoter, which must present the annual calendar in accordance with a timescale to be agreed with the UIM. Final approval for Race events must be obtained from the UIM.

01.03. SANCTION/EVENT HOSTING FEES

The Promoter is responsible for the definition of all aspects relating to the contract with the Local Organising Committee. The financial conditions for staging the Class 1 event will be established between the Promoter and the Local Organising Committee. The Promoter and the UIM will establish, on an event-by-event basis, the fee due to the UIM, which will not include the event insurance required or the cost of UIM officials assigned to the event.

01.04. REQUIREMENTS

The Local Organising Committee contracted by the Class 1 Promoter must follow the requirements and criteria established in these rules with regard the organisational and safety aspects of the event.

02. TITLE

The title of the Championship will be known as the:
“**UIM Class 1 World Powerboat Championship**”.

CHAMPIONSHIP

03. CHAMPIONSHIP POINTS

In any of the races, the points will be awarded to the first and second named pilots only. The first and second named pilots must be the helmsman, throttleman or navigator. The Navigator cannot be the first named pilot.

Points scored in the races and Pole Position are as follows.

Classification	Points
1 st	20
2 nd	15
3 rd	12
4 th	9
5 th	7
6 th	5
7 th	4
8 th	3
9 th	2
10 th	1

04. CHAMPIONSHIP

The Class 1 World Powerboat Championship will be determined from points accrued from all the Class 1 World Powerboat Championship races. All races carry points with no discards. Of the planned races, at least 50% must be held in order to award the Class 1 World Championship title.

If any Class 1 Championship titles include some or all of the Class 1 races, then the rules and points awarded will be as per the UIM rules.

04.01. CHAMPIONSHIP TIES

In the case of a tie in the overall Championship and/or Pole Position Championship, the number of first places shall be considered, then the number of second places, etc.

In the case of the Championship still being a tie, the boat with the fastest average speed in any of the Championship races will be deemed the Champion. In the case of the Pole Position Championship still being a tie, then the position will be shared.

05. CREW/BOAT COMBINATION

The points and penalties for UIM Class1 World Championship will be awarded to the 1st Driver/Boat number combination designated as the first driver on the entry form and who shall be aboard throughout the races.

The first driver is the person responsible for the boat. The second member of the crew may replace the first driver in case of physical problem of a temporary nature, or once in a whole season for any other problem. In these cases, all points

obtained from the boat will be assigned to the 1st Driver/Boat number originally entered in the Championship.

If the first driver changes boat and / or team, he may transfer his accumulated points in the championship only if he moves into the new boat and also uses his original boat number (once accepted). See also rule 44.05.

In the event that drivers will be penalised for Doping and after all appeals foreseen by rules, they will not have any right to prize monies/trophies/titles on World, Continental and National Championship and he/she will have to return any prize monies/trophies/titles received from UIM and/or Promoter during the race season in question.

05.01. NATIONALITY

The nationality of the entry is that determined by the Team as declared at the first race.

The nationality of Pilot No. 1 or Pilot No. 2 is as declared at the first race and whose nationality is determined by the Racing License of that pilot.

SECTION B

LOCAL ADMINISTRATION/ORGANIZATION

06. RACE COMMITTEE MEMBERS AND ROLES

The Local Organising Committee (LOC) appoints a Race Committee which is composed of:

- THE OFFICER OF THE DAY (O.O.D)
- Local Safety Officer
- Buoy observers' coordinator
- Course Officials
- Local Pit Officer
- Event Secretary

06.01. THE OFFICER OF THE DAY (O.O.D)

The Officer of the Day must supervise the organisation of the races on behalf of the Organising Club. He must maintain order on the course, ascertain that all services work properly, enforce the Racing Rules according to the Programme, gather written reports of the Officers and Timekeepers and any other documents that will enable the results to be compiled.

For the C1 Championship events, the O.O.D must meet the following minimum standards:

Have had previous experience in controlling at least 3 important races.

Fluent written and spoken English and good knowledge of the UIM Rules, those relating to Class 1 are obligatory.

Be nominated by his/her National Authority as "Officer of the Day".

National Authorities must keep a list of their O.O.D.'s up to date and notify the UIM upon request accordingly.

National Authorities are strongly encouraged to arrange for their Officers to attend training sessions.

06.02. LOCAL SAFETY OFFICER

The Local Safety Officer reports directly to the THE OFFICER OF THE DAY (O.O.D). He/She coordinates with the Class 1 Safety Coordinator, in accordance with the UIM Safety Procedures and Guidelines (Appendix 1), for all safety and rescue activities on land, on the water and in the air, including the preparation necessary for these activities.

During all official race activities the Local Safety Officer must be present at Race Control.

06.03. BUOY OBSERVERS' COORDINATOR AND COURSE OFFICIALS

This group is composed of one Coordinator located at Race Control, plus a minimum of two observers for each turn - buoy. The group must attend training by a qualified UIM Official at least two hours prior to Pole Position.

At the end of the race, they draw up and sign a statement certifying that all participants' conduct during the race and at the turn marks was in accordance with the UIM On-Water Procedures and Guidelines (Appendix 2), UIM Turn buoys

clarification drawings (Appendix 4) and with the Race Instructions. This statement is handed to the UIM UIM Commissioner, who will apply the UIM rules in the case of infringements. The Course Officials monitoring turn marks must have a clear, unobstructed view.

06.04. LOCAL PIT OFFICER

The Local Pit Officer must guarantee security of the areas and all the services as detailed in the PIT REQUIREMENTS under item 08.01 and the UIM Class On-Land Procedures and Guidelines - Appendix 3.

06.05. LOCAL EVENT SECRETARY

The Local Event Secretary is responsible for the general local organisation of the event. He/She must ensure that all the various local officials know their respective duties and that they have the necessary documents and equipment to fulfill their duties. He/She must collate all documents following the race.

07. EVENT INSURANCE

The Organising Committee must have an Insurance Policy which covers all Race and Non-Race activities for both Competitor and Organiser liability. It must include event third party insurance which is to cover all racing and non-racing activities and, in particular, any damage resulting from towing or launching of the boats and full third party cover for all race officials and associated race helpers.

The Organiser's insurance will be available from the Organising Committee during Race Administration.

The competitors are strongly recommended to have a racing third party insurance. The amounts must not be inferior to those required by the law of the countries where the races will be held. In addition to any other insurance as required by the National Authority or local organizer, Fee to be paid by the team.

All insurance must cover the whole period of the Practice, Pole Position and Races from beginning to end of the event.

SECTION C

RACE ADMINISTRATION

08. RACE REQUIREMENTS

The latest version of all UIM Procedures and Guidelines, as listed below, must be strictly adhered to:

- APPENDIX 1 - Safety Procedures and Guidelines
- APPENDIX 2 - On-water Procedures and Guidelines
- APPENDIX 3 - On Land Procedures and Guidelines
- APPENDIX 4 - Turn Buoys Clarification Drawings
- APPENDIX 5 – Start procedure drawing & clarifications

Failure to comply with the indications given therein will result in a penalty to be applied at the discretion of the UIM Commissioner.

08.01. PIT REQUIREMENTS

At all Events the Local Organiser must establish clearly defined, readily identifiable areas for Dry Pits, Wet Pits, and Crane/Launch Areas in accordance with the outline given below.

In all cases these areas must be set apart from the general public by being fenced off or some other means, except that the public may be admitted to the Dry and Wet Pit Areas during designated times. Only contestants, crews, officials, approved event staff or current members of the UIM or the Class 1 Promoter shall be admitted to the Launch Area. The sale to the public of pit admission tickets or passes to any pit area is strictly prohibited. These areas require the execution of Insurance Waiver/Indemnity by all participants (racers, crews, family members, officials and event staff - not spectators) for admission.

Alcohol use within any of the designated pit areas by any UIM/Class 1 Promoter members or a signee of the insurance Waiver is strictly prohibited and may result in the imposition of a penalty at the discretion of the UIM events have three mandatory pit area classifications with the following restrictions:

Crane and/or Launch area and/or Fuelling area: shall be defined as an adequate area around launching cranes, fuel storage and fuelling area. Entry into these pit areas requires UIM or Class 1 Promoter Racing, Crew or Official Membership. Except for cases of emergency, the maximum number of boat crew in the lift and

drop areas are restricted to 6 support crew and 2 competitors. Team guests are not to enter this area.

Fuelling of boats shall be allowed only in the approved fuelling area(s). Fuelling or de-fuelling of race boats in the water is specifically prohibited. Fuelling at commercial marina pumps on the water or commercial land-based filling stations is allowed, providing they have met the fuel requirements; it is also mandatory to use any fuel supplier provided.

Enclosed Footwear is mandatory in the Craning Areas.

Waste oil and fuel must be disposed of in a responsible manner by all race teams.

Signs shall be posted around all working pit areas designating:

“No Smoking or Drinking of Alcoholic Beverages.”

All UIM race events are required to provide a crane service. All boats are required to provide their own single point lifting harness for lifting/weighing purposes.

The crane area shall be highly restricted and fenced off from all other areas. No persons shall be permitted to the crane area except specifically authorised personnel.

Wet Pits – The Wet Pits shall be defined as the docking areas, as crew working areas and, except in cases of emergency, the maximum number of boat crew in wet pit areas is restricted to 6 support crew and 2 competitors. Team guests are not to enter this area. Access to this area may be available by controlled admission pass dependent on pontoon capability.

Dry Pits – The Dry Pits shall be defined as those areas where race boats are on display prior to launch. Race Officials have the right to remove a race team or race boat from this area for disruptive conduct. Teams may be subject to other disciplinary actions.

09. ADVANCE NOTICE

The Advance Notice must be received by the UIM and, after the approval of both the Promoter and the UIM, it must be sent to the Promoter's Office for distribution at least 30 days before the Race.

The Advance Notice shall contain the following information:

- Local Organising Committee contact details
- Provisional timetable

- Race course and distances
- Map of Pit Areas
- Accommodation, travel and transport details for teams and equipment
- Details of insurance, fuel and social events, if this information is available

10. RACE INSTRUCTIONS

The information contained within the Race Instructions is to be considered as a clarification of the UIM Rule Book or additions to take account of any special local conditions.

These Race Instructions shall contain all of the following information:

- Official timetable and locations.
- Official race course and details.
- Local conditions, such as any special instructions and/or information pertinent to the race, e.g. special radio channels required, nautical chart numbers, tides and any venue-specific Authorities' instructions.
- Terms and conditions of insurance for personal and material damage to third parties, competitors, to be included if available.
- The minimum limit of insurance cover required.
- The type of risks to be covered by the insurance.
- The amount of insurance fees to be paid at the race site.

No course, timetable or race site logistics can be published without direct approval of the UIM and the Promoter.

SECTION D

11. RACE MANAGEMENT AND UIM OFFICIALS

The following officials will be present at all Events to assist in the management and conduct of the Event:

- UIM Commissioner (UIM Comm.)
- Assistant UIM Commissioner
- Safety Coordinator

- Chief Technical Commissioner
- Race Administrator
- O.P.C. Task Force Representative
- Protest Judge

Each of the above may have an assistant, to be appointed from the pool of UIM-approved personnel.

11.01. UIM COMMISSIONER (UIM Comm.)

The overall on water management of the race is the responsibility of the O.O.D. who is the ultimate decision maker on the overall conduct of the event assisted by the UIM Comm. who liaises with his assistant and the Safety Coordinator).

The UIM Comm. keeps an overview of the race. He shall be consulted when judging if the weather conditions are suitable for racing. The UIM Comm. also supervises the onshore aspects of race management.

The UIM shall appoint a UIM Commissioner who shall serve at the discretion of the UIM. The UIM Comm. shall be responsible for the supervision of the race and the correct application of the UIM rules. He/She shall perform such other duties as are requested from time to time by the UIM. The UIM Commissioner must be included in the race/event approval process and receive all course and race instruction details for approval.

Presence at Events - Each UIM-sanctioned racing event must have a UIM Commissioner present. Should an officially designated UIM Commissioner be unable to attend any sanctioned racing event, the UIM may appoint an alternate to serve at that racing event.

DUTIES AND RESPONSIBILITIES

Driver's Briefings - The UIM Commissioner shall run all drivers' briefings. He/She shall see that all questions relative to the racing rules are properly answered and that the conduct of the race itself is in accordance with all the racing rules.

ENFORCEMENT OF RULES

Purpose - The UIM Commissioner shall see that all UIM racing and technical rules are enforced throughout the event. All possible rules infractions shall be reported only to the UIM Commissioner by the appropriate UIM official in writing. It shall be

the sole responsibility of the UIM Commissioner to communicate with any team member or other party who has been charged with the violation of any rule, as well as the proposed action that the UIM shall take as a result.

Multiple Roles - Unless there is prior agreement and approval by the UIM, the UIM Commissioner shall not serve in any other official capacity such as Timer, Chief Technical Commissioner, etc., nor shall he act as a member of any committee charged with the actual management of the racing event or any other racing organisation that directly competes with the UIM while also serving as the UIM Commissioner.

Interpretation of Rules - He/She, along with the Assistant UIM Commissioner shall be responsible for overseeing all sporting matters or penalties of a Championship / Race nature outside the remit of the race jury. The UIM Commissioner will additionally chair and coordinate event meetings with the O.O.D between race officials, promoter and local authority's representatives for the event. Should any questions regarding the interpretation of racing rules arise, the UIM Commissioner shall rule on these matters after consultation with all contestants, witnesses and other UIM and local officials involved. In the event that the Race Committee or another UIM Official makes an erroneous decision or interpretation, the UIM Commissioner is authorised to overrule that decision.

Training - Members desiring to become a UIM official must complete a training programme by the UIM.

Other - The UIM Commissioner of a sanctioned event also must ensure that:

- All proper officials shall be present and function correctly during the event to the relevant UIM standard.
- The approved course is provided (courses must be pre-approved by the UIM before the sanctioned testing or racing begins).
- At Race sites, the UIM safety and rescue requirements are met.
- He prepares all bulletins and results, and official race documentation to be signed by the THE OFFICER OF THE DAY (O.O.D)

Post Race Reports – The UIM Commissioner must file a report in writing to the UIM detailing all of the events, the conduct of the races, any accidents or other incidents such as disciplinary actions, technical Disqualifications, weather conditions at race and test times, course infractions, and penalties assessed interviews with competitors or witnesses to infractions, etc. The UIM Commissioner shall report ALL accidents, even when no injuries are involved, to the UIM. This does not stop local authorities from reporting those accidents, as required by any

local authority law or otherwise to be reported. Race Officials will assist local authorities in completing their reports where necessary.

11.02. SAFETY COORDINATOR

The Safety Coordinator, appointed by the Class 1 Promoter and recognized by UIM as an Official, is responsible for safety and rescue operations. He must be familiar with the race venue, with the characteristics of the competing powerboats and any applicable governmental or similar rules. The cooperation with local or private non-profit Rescue organizations is highly recommended.

The Safety Coordinator must be familiar with the safety regulations under which the race is being organized by the LOC, that is to say the safety requirements of the National Authority, of the Class 1 rules, of the Race Instructions and of any authority over the race water such as the local harbour master/board.

It is highly desirable that any possible conflict between these be resolved before the race and that the Race Instructions give the final word, including the resolution of any conflict.

11.03. ASSISTANT UIM COMMISSIONER

The Assistant UIM Commissioner will assist the role of UIM Commissioner where necessary in his/her absence. The assistant acts with the director in the delivery and coordination of race rules and the local services.

The Assistant is also a course race official with regard to race rules, and may be acting as Director within race control, or as start or course judge.

11.04. CHIEF TECHNICAL COMMISSIONER

The UIM OPC shall appoint a Chief Technical Commissioner who shall serve at each event. The Chief Technical Commissioner shall be responsible for developing and implementing the Technical Inspection plans, procedures and protocols (including branding\ marketing requirements) for each event, and managing the Technical Scrutineers appointed by the UIM OPC (if any) and their implementation of those practices, procedures and protocols.

DUTIES AND RESPONSIBILITIES

Compliance with Technical and Safety Rules: The Chief Technical Commissioner's main responsibility is to inspect the competitors' racing equipment for compliance

with the applicable technical, equipment and safety rules and document their findings on each piece of equipment inspected at an Event.

Unsafe Equipment: The Chief Technical Commissioner shall also have the right to refuse any entry at any time prior to the start that in his/her opinion is not designed, built and equipped to the standards of seaworthiness and safety required for any adverse weather and water conditions during the race. The UIM Commissioner must formalize any such action.

Further Technical Responsibilities: It is also the Technical Commissioner's responsibility to check all the activities relating to technical areas (craning, fuel, etc.) and to report his findings to the UIM Commissioner.

Reporting Infringements: The Chief Technical Commissioner inspects racing equipment for compliance with the technical and safety rules and if he finds what he believes to be a violation of any of those rules, then he shall report the infraction to the UIM Commissioner for the event. The Chief Technical Commissioner does not discuss his findings with anyone including any member of the relevant team, until the matter has been reported to the UIM Commissioner and the UIM Commissioner has authorized same.

Extra Duties in the Case of Accident: In the case of accident, the Chief Technical Commissioner must check all race boats involved for seaworthiness and report his findings to the UIM Commissioner.

11.05. RACE ADMINISTRATOR

The UIM shall appoint a Race Administrator. He/She is responsible for the general sports-related administration and organisation connected to the event, and shall also perform such other duties as requested from time to time by the UIM. The Race Administrator shall be responsible for managing the assistant administrator (if any).

DUTIES AND RESPONSIBILITIES

Membership, Registration at the Event - The Race Administrator shall be responsible for collecting and accounting for all registration details along with the respective forms.

Insurance Disclaimer - It shall also be the duty of the Race Administrator to have all members signing the insurance Disclaimer at each race site.

General documents and Race Bulletins - The Race Administrator will supervise the Local Event Secretary in organising and drafting all general racing documents and

bulletins as required. The Race Administrator will also organise any briefings/daily meetings as required.

Signature and Posting of Results - The Race Administrator shall be responsible for posting the arrival order in a pre-determined and announced location, and communicating those results to the designated media representative following the final race of the day.

The Race Administrator shall also be responsible for arranging for the final results to be signed and for posting them on the Official Notice Board.

11.06. OFFICIAL RACE TIMING

An Official timekeeping system and operator will be appointed by the UIM and is the official race timekeeping system. In Class 1 the system and operator will be provided by the Class 1 promoter. Official timekeeping system may be linked to the mandatory transponder equipment required by the UIM. The mandatory transponder/virtual system may be used as either the principle timing system or the reserve system and operated by the UIM and/or its contractors.

The UIM race officials will provide to the operator the necessary entry list details and the official course distance calculations and information.

The UIM Commissioner and his assistants will provide information for the operator regarding race information, penalties, communications etc. as necessary during the course of each session and race.

At the end of each on water timed session the operator will make available to the race administration the system results.

Race Administration will publish the official timed and signed result.

These results are all provisional and subject to post race technical scrutineering, post race/pole position reports, protests etc.

11.07. OPC TECHNICAL TASK FORCE REPRESENTATIVE

The UIM OPC shall appoint a Member of the OPC Technical Task Force to attend the races. He performs duties as requested from time to time by the Committee and particularly:

To help the Chief Technical Commissioner to train new scrutineers, refine the inspection procedure, clarify any doubt on Technical rules via OPC Technical Task Force Official Releases, and oversee the performance/parity results, reporting to the OPC any required action.

With the Technical Task Force, he is charged to research future technical rules and requirements, and is empowered to liaise with manufacturers to facilitate the future technical advancements.

11.08. PROTEST JUDGE

In Class 1, one qualified Protest Judge will be nominated by the UIM for each event and will adjudicate all protests and will be named in the advance program.

12. RACE REGISTRATION (ON-SITE)

Registration will take place at the Race Administration office.

Race Registration times will be as per the Advance Program. All Race Registration requirements (including payment of insurance) must be completed by the time Race Registration closes for signing in to the event.

Failure to do so will result in non-participation in the racing event.

On the registration form for each Class 1 Race, the names of the Team Manager, first and second pilots, together with reserve and test pilots, must be specified. Also the name and boat number and a copy of the current valid measurement certificate with registration number must be supplied.

On signing in at Race Registration, a Scrutineering/Technical inspection slip will be issued, which will enable competitors to present their boats for Scrutineering/Technical inspection.

All Official Race Information, Bulletins and Results will be posted on the Official Notice Board at the Race Registration Office.

Competitors will be provided with identification tags, if required, which must be worn at all times during practice, Pole Position and racing.

Any person other than a racing crew member wishing to embark on the boat for practice will only be authorised under the condition that they are a UIM-licensed reserve/test pilot and have registered as such during Race Administration or as otherwise agreed by the UIM UIM Commissioner.

Changes to crew must be notified to race control a minimum of one hour prior to testing. PENALTY Euro 600 for non-conformity.

He must sign up with the Organiser's Insurance Agent with either the Local Organiser or the Class Promoters' representative.

He must be in possession of all current documents required by the UIM and organising committee.

All race numbers will be between 1 and 99 and will be allocated by the UIM after the entry has been accepted. Number 1 will be allocated to the previous year's World Champion, unless otherwise requested. The replaced number of the World Champion's boat will be reserved for this boat to resume when it is no longer the World Champion. Race Bulletins will be issued and signed for at Race Briefings and/or the Race Administration Office.

Restraint Indemnity - UIM rules mandate the signing of the National Indemnity form regarding restraints. In view of the International nature of the Class 1 series and the fact that all boats use restraint systems, in applying for the licence, the competitor confirms that he has read and understood the UIM, and his National Authority's rules regarding restraint systems and confirms that the boat entered for the event in which he is taking part has been constructed to and conforms with these rules. He further undertakes not to hold the UIM, his National Authority, the Organising Club, or any of the servants or agents of the foregoing, nor any other person connected with the organisation of an event, responsible for any personal injury incurred by his wearing this restraint system.

13. PRE-RACE INSPECTIONS

13.01. REQUIREMENTS

The Chief Technical Commissioner may also at any time during the event pre testing, pole, and/or race, require random inspection and testing of equipment for compliance to the rules.

All race boats entered in a sanctioned race are subject to a pre-race inspection by a UIM-approved Chief Technical Commissioner

The time and place of such inspection shall be published in the Advance Program for the Event.

No race boat may be considered a bona fide entrant in a UIM race until such time as the Chief Technical Commissioner has passed and signed the official pre-race technical inspection form.

It is the responsibility of the Team Owner and the Manager to submit his team's equipment to the Chief Technical Commissioner for his inspection. If, in the judgment of the Chief Technical Commissioner, a boat and/or safety equipment is unseaworthy, unsafe, or unmanageable, the non-compliance must be brought to the attention of the UIM Commissioner. If the UIM Commissioner determines that

the condition cannot be rectified prior to the start of the race, then he shall have the right to prohibit the boat from competing.

The Chief Technical Commissioner shall examine each entry for compliance with all safety requirements and shall also visually inspect hull, propulsion and engine for compliance with Class 1 technical rules. The spirit as well as the letter of these rules shall be enforced equitably to all entrants.

The Chief Technical Commissioner may also at any time during the event pre testing, pole, and/or race require random inspection and testing of equipment for compliance to the rules.

14. PRE-RACE TECHNICAL RULES VIOLATIONS

14.01. NOTICE TO OWNER

In the event that a pre-race inspection finds a technical rule violation, the UIM Commissioner or Chief Technical Commissioner must notify the Team Owner of the boat or the Team Manager of the specific nature of the alleged violation as soon as practical, but in no instance later than the Final Drivers' Meeting. Depending upon the nature of the violation, and whether this is a first or multiple offence, the UIM may issue notice with a range of penalties from fines to immediate Disqualification from the event.

14.02. CORRECTION OF VIOLATIONS

After a race boat owner has been notified of the violation, he shall have until 1 hour prior to Pole Position to correct the violation, unless he has been disqualified. Should he elect to correct the violation then it shall be his responsibility to have the boat re-inspected and cleared for competition.

14.03. PROTESTS and APPEALS (Refer to Article 400 of the UIM Rule Book white pages and any amendments as described in the Race Instruction **and to SECTION D 11.08**)

14.04. GENERAL INSPECTION

During this Inspection the UIM shall also check the team for compliance with the marketing requirements in force for the race series or event venue. Teams shall comply with the marketing requirements as set out in their contract with the Series Promoter.

15. TEAMS' REPRESENTATIVE

A Teams' representative must be elected by the Class 1 Teams Owners in the presence of a UIM Official and candidates must be teams actively participating in Class 1 racing.

The teams' Representative is elected at the first race of the season for a one-year term.

The Teams Representative has a seat in OPC with voting right.

16. DRIVERS' REPRESENTATIVE

A Drivers' representative must be elected by the Class 1 Drivers in the presence of a UIM Official.

Candidates must be drivers actively participating in Class 1 racing.

To be eligible, they must have competed in a minimum of two Class 1 World Powerboat Championships in the last five years.

The Drivers' Representative is elected at the first race of the season for a one-year term. He should be available to attend meetings called by the Promoter at least annually, to report and advise on the consensus recommendations and safety requests from the drivers.

He may be invited to attend all UIM - OPC meetings and may receive all relevant UIM communications.

17. TEAM MANAGERS

Each boat must designate its Team Manager at each race venue for contact in case of an accident or Force Majeure, this representative must be present and registered at Race Administration.

Each boat must have its representative present at the Team Managers' Tent throughout Official Practice sessions, Pole Position and Grand Prix races for all official communications - PENALTY Euro 500 - and should be in permanent radio contact with Race Control through an efficiently working radio system - PENALTY Euro 500.

Team Managers' briefings must be attended by the Team Manager. It is his responsibility to ensure that all team and crew members are informed about all local authority requirements both on land and on water. He must also ensure that

the crew is fully conversant with all testing, pole position and race course areas and all relevant safety requirements.

SECTION E

18. PILOTS' BRIEFINGS

18.01. ATTENDANCE

Pilots' briefings shall be confined to crew members, Team Owners / Managers and concerned officials only. Unless otherwise provided in these rules attendance at these meetings is mandatory for every boat crew. All pilots and crew members attending any pilots meeting must wear a team uniform.

Racing Team members who fail to wear a uniform as specified above will be subject to a fine of Euro 200.

18.02. PILOTS' BRIEFING TIMES

It is mandatory for one pilot from each team to attend the Teams Course inspection at all venues. Time to be announced in race instructions.

It is mandatory for both first and second pilots to be present at the main briefing and 1 of the 2 nominated pilots for the weather briefing (if held).

Signing in for any briefing will begin 20 minutes before scheduled pilots' briefing time. It is mandatory for all competing crew members to attend and sign in at the briefing. All signing in must be completed prior to the pilots' briefing time. Penalty for non-attendance: Euro 1.000,00 fine per person and/or disqualification. Pilots who are late for a briefing: Penalty Euro 500,00.

It is essential that all competitors read and are totally aware of the contents of the Race Instructions and / or any Race Bulletins before attending the Pilots' Briefing.

It is the responsibility of all pilots to bring the Race Instructions and any bulletins to all Briefings.

Event Race Instructions may contain one mandatory Official Reception that competitors must attend. Penalty Euro 1.000,00.

The use of mobile phones during briefing is strictly forbidden Penalty Euro 200,00.

18.03. ALCOHOLIC BEVERAGES

Sale or consumption of alcoholic beverages is strictly prohibited at all pilots' briefings.

18.04. UNIFORMS

All racing crew and team members are required to be in team uniform at Pilots' Briefings, race/event functions and on race day, unless other requirements are published in the Race Instructions / programme. Penalty Euro 200,00.

SECTION F

19. UIM SUPER LICENCE

In order to participate in Class 1, every crew member of the boat must hold a UIM Class 1 Superlicence.

All UIM licences are valid from the date of issue to 31st December of the same year.

In the event of any accident either within the sport or beyond it's jurisdiction that prevents an individual from racing on medical grounds, a new post injury medical certificate must be submitted prior to re issue of the individuals licence.

Additionally in the case of a race incident resulting in medical care the competitor must be signed fit to return to racing by the race/event doctor. If this is not obtained, a signed declaration from a Doctor of fitness to return must be submitted to the UIM.

The event insurance must be complied with.

19.01. ISSUE OF SUPERLICENCE

A UIM Class 1 Superlicence can be obtained either following a request by the pilot's National Authority to the UIM.

The application for a UIM Class 1 Superlicence must be lodged with the UIM four weeks prior to the first event in which the pilot wants to participate is held.

The price of the UIM Class 1 Superlicence will be Euro 1.000,00 -

All required documents must be completed and the following additional documentation, or proof of such, must be provided to the UIM Secretariat:

- The sports career of the pilot, including the results obtained in the previous two seasons of offshore racing.
- Evidence of having raced during the previous 2 years in Class 1 or Class 3 Closed Canopies, or equivalent as pilot or throttleman in at least 7 races. (Other classes may be taken into consideration as agreed by the UIM).
- Current immersion certificate.
- A medical certificate providing all the data required by the UIM Official medical form.
- Two photographs (passport size).
- The fees fixed by and paid to the UIM.
- The UIM Class 1 Superlicence is valid from the date of issue to 31st December of the same year.

The UIM may issue the UIM Class 1 Superlicence on the basis of the information supplied by the pilot's National Authority. The UIM may withdraw a UIM Class 1 Superlicence on the recommendation of the relevant pilot's National Authority and the agreement of the Class 1 Promoter.

Any pilot who has passed their 50th birthday before the date of application for a UIM Class 1 Superlicence will be obliged to submit a medical report from their own doctor, and also a report of an examination by a doctor nominated by the UIM.

The minimum age for a pilot to qualify to hold a UIM Class 1 Superlicence and to compete in a Class 1 race is 18 years unless otherwise agreed by the UIM.

The application and nomination by the pilot's National Authority will be deemed to be the National Authority's consent to represent their country for the duration of the UIM Class 1 Superlicence. Should the NA not wish a UIM Class 1 Superlicence holder to represent his country at any time, the NA must inform the UIM in writing.

The UIM may authorize a provisional Licence for the second Driver who may not have the above - mentioned sports career to participate in the competitions provided that:

- The First Driver is well – experienced, has attended the last two Class 1 Championships and takes the responsibility of the Second Driver's behavior in the race, the first driver must also sign the UIM letter of agreement regarding responsibility ;
- He should have an Offshore Licence issued by his relevant National Authority.

- The application should be authorized by his N.A.

19.02. TEST DRIVER LICENCE

A UIM 'Test Driver' licence will be issued by the UIM to use during event 'official practice times'. To qualify for this licence, the following conditions must be complied with:

- Current immersion test certificate.
- A medical certificate providing all the data required by the UIM Official Medical Form.
- Two photographs (passport size).
- The fees fixed by and paid to the UIM.
- The application must be authorised by his NA.

The fee for a test drivers licence is Euro 250, if upgraded in the same year to a full super licence Euro 750 fee is applied.

19.03. COCKPIT EVACUATION / IMMERSION TESTING

Before racing in a craft with restraint systems, all crews must have passed in the last fourteen months, an immersion test in a restraint system to ensure that they can exit a reinforced cockpit crew compartment successfully.

Prior to taking the immersion test, all crews must have a valid scuba certificate or have received suitable training. This alternative training should be approved by the National Authority.

An Immersion Certificate to certify the passed test, showing the expiry date, must be delivered by experts recognised by a National Authority.

All riding crew members using restraints must sign the National indemnity form prior to competing in any race or practice.

UIM Cockpit Evacuation training and testing is available at the Class 1 races.

Regardless of any current immersion training certificates held by competitors the UIM training and testing is mandatory.

Immersion testing is to include training for all crews in both self extraction and full partner rescue training utilizing on board air systems. All crew members must be

capable of full use of on board air systems to the satisfaction of the appointed UIM testing facility.

The UIM will also request mandatory random testing throughout each racing season for all crews, where possible this will be integrated with the local event rescue crew training.

20. PARTICIPATION RULES

The UIM and its National Authorities and organising clubs reserve the right of entry of any team or competitor and has the right to accept or reject entry/eligibility at the discretion of the UIM on the grounds of the unsuitability of any entrant for reasons such as and not limited to unsportsmanlike conduct, bringing the sport into disrepute.

Eligibility - The Team entries for the Championship must be received by the UIM via the Class 1 Promoter and notified to the relevant recognised NA and must include at least the Owner's name, the names of the First and Second Pilots and the race number of the boat. The name of the two pilots must be stated including any reserve/test pilots.

It is mandatory for each entrant to participate in all Class 1 races to accumulate points unless otherwise agreed in writing by the UIM.

The UIM will decide the penalty which may be a fine of min. Euro 10.000,00 max. Euro 50.000,00 and/or alternative penalty for contravention of the above.

In the case of accident damage to a boat, the pilot may apply to the UIM for dispensation not to compete in the next races until the damage is repaired. The Chief Technical Commissioner must verify and certificate the damage and report to the UIM. If the dispensation is requested for damage outside the races, the Chief Technical Commissioner must verify the boat where it is situated. The applicant must pay all the costs.

New series entries arriving after the first round will be eligible for Championship points. Late entries are at the discretion of the promoters and the UIM only.

Inscription Fee & Prize Money - as per contracted promoter's agreement.

SECTION G

SAFETY RULES

GENERAL APPLICATION

The following Safety Rules apply to all Class 1 UIM Series and Championship Events and are in addition to all safety related rules set forth above in the UIM General Racing Rules.

21. REQUIRED SAFETY FACILITIES

In General - All International-Series Event Medical and Rescue facilities must be pre-approved by the UIM at least 30 days in advance of the Event. It shall be the Local Organising Committee's responsibility to communicate with the UIM and perform all safety functions and secure all necessary safety facilities in accordance with these rules and the UIM Safety Procedures and Guidelines Requirements. Each Local Race Promoter shall be required to provide all personnel, materials and services required by the UIM Safety Procedures and Guidelines.

It will be the responsibility of the Local Race Promoter to provide, to complete and return the UIM Class 1 Pre-Race Safety Questionnaire Sheet to the UIM office no later than 30 days prior to the event.

21.01. MEDICAL/RESCUE, TOW AND PATROL BOAT COVERAGE

For detail of Safety/Medical cover requirements, see APPENDIX 1.

21.02. SEA SAFETY

Safety Equipment & Sea Safety - Whenever a registered race boat bearing UIM numbers or branding is being operated at a UIM event, including while testing, or at any other times while the craft is on plane, or while off-plane under its own power and within 250 feet of the outside of an active race course, the competitors must wear complete approved and tested personal safety equipment including helmet and life jacket. This rule shall be in effect at a race site from the time of arrival and until one hour after the chequered flag of the last event of the day. Whenever such registered equipment is being operated at times other than UIM events, such operation shall be conducted in a careful and prudent manner according to local rules and regulations with regard to safety, so as not to endanger life or property and so as not to bring powerboat racing into disrepute.

Violation of this rule shall result in a fine or Disqualification from that event or a subsequent event at the discretion of the UIM Race.

Pilots must have the full Safety Equipment for all Practice, Pole Position and Races.

It shall be the pilot's sole and ultimate responsibility to ensure the fitness of his boat and to decide whether or not to start or to continue in the race once the boat has passed scrutineering. The Organisers wish to inform competitors that all safety and patrol boats will be instructed that their first priority at any incident is saving human life. If possible, although no guarantee can be given, assistance may be given to boats.

In case of an incident or accident on the racecourse, a yellow flag will be flown or waved by any "Official" boat. Race boats should keep a distance of at least 50 metres away from any boat displaying this flag and the accident. A minimum penalty of a Yellow Card for infringement of this rule will be enforced.

Harbour restrictions. All boats must respect the local rules with reference to the maximum speed in the harbour. A Euro 500 penalty for infringement of this rule may be enforced. Crews may also be subject to Local Authority conditions and possible legal actions.

It is the Team's sole and ultimate responsibility to recover damaged and/or sunken participating race boats.

22. FUELLING

Fuelling or decanting of fuel may be made until 24.00 hours (or as announced in the relevant Race Instructions) on the day prior to the race. In the case of Force Majeure, an Official Bulletin may authorise the refuelling on the race day.

Penalty - Disqualification.

Any team which dispenses fuel either in or out of the boat other than in the designated fuelling area, or without prior approval of the Chief Technical Commissioner, will be subject to a fine of:

- First time of the season: Penalty - Euro 1.000,00
- Any future offence: Penalty - Euro 5.000,00 and/or Disqualification.

Fuelling times will be notified in the Race Instructions and/or Bulletin, posted on the Official Notice Board.

The only fuel approved for use in UIM Events, will be that supplied by the local organiser or series supplier, as the case may be. The Unleaded fuel supplied will have a minimum of 95 Octane. Unless otherwise notified in the Race Instructions.

- No modifications or additives are permitted to the fuel.
- Fuel samples showing any characteristics other than the fuel issued at the event, will be deemed illegal. The penalty is Disqualification from the event for a first infringement, and more serious penalty for a second offence.
- The UIM inspector can at any time take samples from boats to ensure compliance.

Any team using fuel subsequently deemed to be illegal will incur the cost of testing and be disqualified.

23. SMOKING

“All smoking of any type is prohibited in pit areas”. The deemed area for non-smoking is in the dry pits (excluding inside workshops), craning area and in the wet pits.

Competitors are reminded that it is their responsibility to ensure that the rule is strictly adhered to by all their team members. Any contravention of this rule shall incur a penalty of Euro 100.

24. RACE OVERALLS AND PERSONAL SAFETY WEAR

Any person aboard any boat partaking in Races, Pole Position and Practice must always wear an adequate race vest as per UIM rules. Life jackets/Buoyancy suits must be worn during all races, Pole Position and practice. Life jackets/Buoyancy suits may only be removed when effecting repairs and with the boat at very slow speed (less than 10 knots). The choice and efficiency of Life jackets/Buoyancy suits is the sole responsibility of the wearer.

Any person aboard any boat partaking in Races, Pole Positions and Practice must always wear an adequate helmet of predominantly orange colour. Helmets must be worn during all races, Pole Position and Practice. Helmets may only be removed when effecting repairs. The wearer is entirely responsible for the choice and the efficiency of his helmet. Helmets may be removed when returning to pits at less than planning speed.

Each crew member's helmet must be identified with their boat race number. This number shall be placed on the top surface of the helmet and shall be read from the rear. The numbers shall have a minimum height of 7.5 cm and shall be of the same

style as the boat race number. These numbers shall be painted in waterproof black paint or be made of black coloured self-adhesive material of adequate strength. The wearer is entirely responsible for the choice of his helmet. The organisers are requested to repeat this important rule in their particular rules, as well as in their Advance Programmes and Race Instructions. Each National Authority may require that their nationals wear a helmet of a type laid down in their own National rules.

At all times during Practice, Pole Position and the Race, competitors or test pilots must wear suitable protective clothing that covers the torso and all limbs to the wrists and ankles. Protective clothing used must be durable enough to provide bodily protection and it is recommended to be fire retardant.

This protective clothing must have the name of the competitor clearly visible on the front chest section or waist band and the competitor must apply to Race Control for a wristband. The inclusion of Blood Group detail added to the race overalls is recommended but not mandatory.

Contravention of any of the above clothing rules will result in a minimum of a Euro 500 fine. It is strongly recommended that neck protection is worn.

25. RADIO COMMUNICATION EVENT - PRACTICE/POLE POSITION/RACE

Radio communications with the Race Boats and Team Managers will be maintained on the designated VHF channel as detailed in the Race Instructions and/or Bulletins and/or Briefings.

The Team Manager is responsible for ensuring that all communications to be made to the Race Boats have been received by the pilots on board.

Communications via VHF radio are allowed excluding Official Race Control Channels and Channel 16. Competitors must be fully conversant with, and at all times, must comply with, the International Regulations for the Prevention of Collisions at Sea and such other safety measures promulgated in connection with this event.

Competitors must at all times use the Race Number of their boats as their call sign.

Competitors are required to communicate with Race Control before leaving and returning to the Wet Pits at all times. A team representative must be present in the timing area during all on water activity - no boats may leave the Pit area without the presence of the team representative.

No boat will be allowed to leave the wet pit before its radio communications system, as well as that of its Team Manager, has been checked and approved by the UIM Technical Commissioner.

It is mandatory to have at all times during on water activities an efficiently working radio system, and to stay in permanent radio contact with Race Control. PENALTY Euro 500,00.

Communications - Radio, cell phone or other electronic communication from any aircraft, boat, or shore crew, to any race boat shall be permitted provided that those communications are not used to violate or assist in the violation any of the rules contained herein, and provided further that any team using such communication must provide UIM live access to said communications through assigned radio frequencies or otherwise so that UIM Officials can monitor those communications when requested by UIM. Such communication is prohibited on official Race Control channels.

SECTION H

GENERAL REQUIREMENTS, PRACTICES AND PROCEDURES FOR CONDUCTING RACES

26. BOAT PARADE

All boats must take part in the Boat Parade unless previously agreed by the UIM Commissioner

27. TESTING/PRACTICE

Inspections and Registration - Race boats must have a pre-race technical and safety inspection, the boat and all riding crewmembers must complete all registration requirements prior to any on water testing. The owner and all riding crew members must sign the Mandatory Insurance Waiver/Indemnity prior to any on water testing.

Testing should always take place on the race course where possible.

Any boats wishing to practice during the official practice period, must communicate with Race Control on the designated event channel for approval before proceeding out on the course and on safe arrival back in the harbour, in accordance with the procedures contained in the Race Instructions. Penalty Euro 500.

Practice times will be detailed in the Race Instructions and/or Bulletins. Any practicing outside of these times must be agreed by the UIM Commissioner. Boats

must not cut across the practice course except in the case of Force Majeure and with extreme caution. Penalty for non-compliance - Euro 500 per boat for first offence, Euro 1.000 for subsequent offences in the event and/or the championship.

Official practice will only take place after Administration and Scrutineering have been completed satisfactorily.

Penalty for non-compliance will result in a penalty of Euro 1.200 .

28. POLE POSITION

A Pole Position for both races 1 and 2 will be held on the race course with options to be held the day of the race or the day before each race, unless otherwise stated in the Race Instructions or posted by Race Bulletin as a change due to unforeseen local conditions or weather. Only the results (points) of the Race 1 Pole Position event will be counted towards the official "Pole Position Championship."

The UIM will approve all rules, procedures and guidelines for the official Pole Position.

All communication will be on the event official VHF channel, as announced in the Race

Instructions, via the Team Manager and/or Race Boat.

Boats may run in accordance with the official "On-Water Procedures and Guidelines". As

per race instructions.

PROCEDURE - APPENDIX 2 – POINT 01

29. RACE COURSE

29.01. RACE COURSE LENGTH AND RACE COURSE DESIGN

The UIM must approve all Class 1 series race courses. Courses must be submitted by the Local Organisers at least 60 days prior to event.

The promoter and/or the UIM will correspond with the drivers representative with the draft race course for comments and observations in writing for consideration by the UIM and within 48 hours of the notification to the representative.

29.02. RACE LENGTH

The race must be programmed to be multi-lap with a maximum time of 35 minutes for race 1 and 45 minutes for race 2, unless otherwise agreed by the OPC. Race laps must be repeated and be at least 4 nautical miles in length. The length of any Class 1 race shall be a maximum of 1 hour, unless stated in the race instructions or race bulletin.

Under no circumstance during the event may any boats practice, Pole Position or race outside of the designated area. Failure to comply will result in a fine of up to Euro 1.000 .

Failure to complete the course correctly as described in the Race Instructions and/or at Race Briefing and/or Race Bulletins, will result in Disqualification.

29.03. LONG/SHORT LAP

It is mandatory for all racing boats to complete the Long Lap(s), as specified in the Race Instructions, at any time from the start of the second lap; the first long lap is to be completed before the end of the 50% lap and the second by the end of the 70% lap. Failure to complete the Long Laps will result in time added or lap penalties, as follows:

- Failure to do the first long lap by the end of the 50% lap - Penalty 1 Long Lap.
- Failure to do either of the 2 long laps - Penalty Disqualification.
- It is recommended that the Long lap should be approximately 20 and 30 seconds longer than the course race lap.

01 Execution of Long Lap penalties with “long lap course in place”

- The penalty given during the race **MUST** be run within **TWO** laps following the UIM Commissioner’s official communication. This communication is done via radio and/or timing screen;
- If the penalty is not run within the two laps, an extra Penalty Lap is applied;
- If the penalty is not run at all, there will be a **DISQUALIFICATION**;
- If the penalty is not run due to force majeure reasons (race stoppage, breakdown or other), the “penalty time” is applied. This time **MUST BE** the boat’s long lap time (from the total time, the best long lap time of the boat replaces its worst normal lap time);
- If the penalty is a **POST RACE** penalty, the above “penalty time” is applied.

02 Execution of Long lap penalty with “No long lap course in place”

- The penalty, during and post-race, is applied in terms of time; in all the various cases, 25 seconds will be added at the total boat time, for each penalty given.

As an alternative to the Long Lap it is possible to have the “Short Lap.” It is mandatory for all race boats to complete the Short Lap(s) as specified in the race Instruction.

The Short Lap/s shall be completed at any time before the 70% Lap as indicated in the Race Instructions.

It is recommended that the short lap should be approximately 20 and 30 seconds shorter than the course race lap.

03 Execution of penalties with Short Lap course:

- Each penalty will result in a reduction of one short lap from the total of the short laps established for the Race
- If the penalty is not carried out, 1 regular Penalty lap is applied post-race, if imposed after the completion of all planned Short Laps, or there are penalties remaining after removing the foreseen Short Lap/s, a “penalty time” of 25 second is applied POST RACE for each remaining penalty

04 Execution of Penalty Time without Short Lap course:

- In case the Race course does not include Long or Short Laps, the Penalty, during and post- race, is applied in terms of time; in all the various cases, 25 seconds will be added to the total boat time, for each penalty given.

29.04. BAD WEATHER

If it is necessary to run a Bad Weather course and/or bring the Race start time forward, full points will be given provided:

- The length must be a minimum of 50% of the original programmed number of laps.
- The Teams’ and Pilots’ Representatives, UIM Commissioner, Promoter’s Representative, Safety Coordinator and THE OFFICER OF THE DAY (O.O.D) must agree the bad weather course and the number of laps, and inform the competitors, in writing, at least 30 minutes before the launching procedure as per rule 30.

29.05. START CHUTE

Minimum Width - The recommended minimum starting chute width shall be 30 metres per race boat.

Minimum Length - The recommended starting chute length shall be 1.5 nautical miles before the first turn mark.

If the starting chute length is less than 1,5 nautical miles, the first turn must not be more than 90° to the second buoy of the turn.

Other Requirements - The chute must be maintained in a straight line so that the boats can maintain their position. The start cannot be set up with a turn before the end of the chute.

The start run must allow for the pole position boat to have the shortest distance to the first turn mark.

The start chute may be clearly marked, buoys or fixed points on either side of the course. All competing boats must pass through the start chute between the markers.

The finish boat shall fly a C1 Flag or a specifically-designated flag, and chequered flagging will be done from the official finish boat.

29.06. TURN MARKS

All turn marks on the course, approved by the UIM, will be published in the Race Instructions.

When a turn is less than 90 degrees, it should be designated by a minimum of 3 mark buoys a minimum of 100 metres apart.

Above 90 degree turns should be designated by a minimum of 2 mark buoys a minimum of 100 metres apart.

Any buoy used to designate race marks should be a minimum of 1.5 metres tall, and made of vinyl or plastic material capable of withstanding conditions encountered in powerboat racing.

It is recommended that where turn marks are located in the vicinity of permanent navigation marks, the turn mark must be laid to the outside of the permanent mark at a minimum distance of 10 metres.

Each turning point must be marked by a buoy in accordance with the relevant guidelines and controlled by the Course Officials. Different colours must be used to indicate whether the boat must pass inside or outside the buoy.

29.07. TURN MARKS AND/OR ABSENT OR NON-CONFORMING TURN BOAT OR BUOY

The procedures for passing buoys are included in the UIM On-water Procedures and Guidelines (Appendix 2) with related illustrations of the various scenarios in the Turn Buoys Clarification Drawings (Appendix 4)

All marks are laid in the approximate positions referred to in the Race Instructions and/or

Drivers' Briefing and/or Race Bulletins , or as directed by the UIM Commissioner.

It is the competitor's responsibility to drive close enough to the course marks to ensure that he has been seen to pass correctly and to satisfy himself to this effect.

Unless otherwise specified in the Race Instructions and/or Drivers' Briefing and/or Race

Bulletins, the color of the Course Buoys must be as follows:

All Outside Course Marks YELLOW

All Inside Course Marks ORANGE and/or GREEN

Should a buoy go 'missing', the pilot must pass through the approximate geographical position referred to in the Race Instructions, and/or Drivers' Briefing and/or Race Bulletins. Penalty will be applied as described in the Race Instructions for not passing through the geographical position -

If a boat hits a buoy on the correct side, no action will be taken. If a boat goes over a buoy, demolishes and/or dislodges a buoy, Penalty will be applied as described in the Race Instructions.

In any case, if the buoy is also demolished, the team will be fined Euro1.500,00 to refund the cost of the buoy. See APPENDIX 4

The repetition of a missed buoy is not allowed – Penalty: Disqualification and Yellow Card

Any buoy passed on the wrong side during practice may result in a Euro 500 fine, and if safety or other boats are compromised a Yellow card.

Course in and out procedures and mark buoys must be followed at all times during practice, pole position and races.

Failure will result in Euro 500 fine.

Any buoy passed on the wrong side will result in a Penalty specified in the table below: (See also APPENDIX 4)

Turn Mark Comprising of 1 buoys		
1 Buoy missed	=	DISQUALIFICATION
Turn Mark Comprising of 2 buoys		
1 Buoy missed	=	Penalty will be applied as described in the Race Instructions
2 Buoys missed	=	DISQUALIFICATION
Turn Mark Comprising of 3 buoys		
1 Buoy missed	=	Penalty will be applied as described in the Race Instructions
2 Buoys missed	=	Double Penalty will be applied as described in the Race Instructions
3 Buoys missed	=	DISQUALIFICATION
Repetition		
Repetition of a missed buoy	=	DISQUALIFICATION
Other marks of course		
Passing marks on wrong side	=	As stipulated in the race instructions

29.08. RIGHT OF WAY

Overtaking - When two race boats are on the same course or approximately the same course on a straight, and one boat is passing or attempting to pass another boat, the passing boat shall keep clear of the boat being passed, and in passing, shall allow at least 2 boat-lengths of clear water between its transom and the bow

of the boat being passed, before altering its helm so as to assume the same or essentially the same line or lane as that of the overtaken boat.

Passing in a Turn (Formerly “Overlap”) - If two or more boats are approaching a turn mark side by side, the boat on the inside of the turn has priority and the outer boat must keep clear.

Any overtaking boat must still give way to the overtaken boat as per overtaking above. Priority must be given to the race boats on the race lap.

Long/Short Lap - Boats returning from their long/Short lap to the race lap must give priority and give way to the boats on the race lap unless they are clearly ahead as overtaking above. The return to race lap line is at any point when returning from Long Lap/Short mark buoys and rejoining the race line at any point.

Yellow Flag - On seeing the Yellow flag signal from a safety boat, race boats must slow down, acknowledge the signal, proceed with caution and keep clear of the danger area. No overtaking is permitted under this caution. Any competitor who ignores the Yellow flag signal will be penalized and/or issued with a Yellow card and minimum Euro 1000 Fine.

Red Flag - All boats must safely slow from racing and make their way under safe speed as described in Race instructions or drivers briefing to the muster/milling area.

29.09. SPECTATOR AREAS

Organisers should designate spectator areas. If a race boat finds itself in a spectator area, it must reduce speed to less than 12 knots immediately and proceed at less than 12 knots with caution until back on the race course.

Penalty Yellow Card.

30. LAUNCHING

Launching and recovery facilities are available to competitors during official races, Pole Positions and practice. The operational hours are indicated in the Race Timetable. No facilities will be available to competitors until they have signed in and completed Administration and Technical Inspection.

All boats' movements, launching and recovery instructions will be under the responsibility of the Chief Technical Commissioner. Once signed in, no boat may leave the compound for any reason unless agreed by the UIM Commissioner.

At the end of the Pole Position and the Grand Prix race, unless otherwise advised by the Chief Technical Commissioner boats are obliged to return to the crane from which they were launched.

In all cases when a crane is used, it is forbidden for any person to be on or in the race boat while the boat is being lifted. The penalty for non-compliance will be a Euro 1.000 penalty and/or Disqualification from the event. Exception is rule 32.02 Pit Stops.

The order of launching for the race and the choice of the crane and launch time will be decided by the classification order of the Pole Position, or based on the previous race result if no Pole Position takes place.

If the race boat is not ready to be launched at its designated time:

- The team may be fined Euro 500
- The boat will be launched at a time to be decided solely by the Chief Technical Commissioner

No boat will be recovered from the water once launching has commenced unless with the permission of the the Chief Technical Commissioner If a race boat, having been launched needs to use the crane, it may only do so after all race boats have been launched and with the permission of the Chief Technical Commissioner.

No propeller or ratio changes will be permitted until the start has taken place and the change will be deemed a pit stop.

31. CRANING

Craning time is fixed at a maximum of ten minutes. A boat can enter the craning area only when it is ready to be hoisted. The time starts from when the boat is attached to the crane by its straps and ends when the straps are released from the crane.

Boats exceeding the craning time during pole position or race will not be permitted to continue in the Pole Position or race.

Practice session time infringement Penalty Euro 500.

32. PIT STOPS

32.01. DEFINITIONS

Pit Stops - visit to the designated Pit Stop area for assistance during the Race and/or Pole Position i.e. from the time of the race Start until the time the last boat crosses the finish line. The Pit Stop area is the Wet Pit unless otherwise defined in the Race Instructions.

32.02. PIT STOP RULES

The Pit Stop procedure is under the responsibility of the Chief Technical Commissioner.

No-one is permitted in the water during pit stops.

Whilst on the crane pilots may remain harnessed for prop changes (safety issue exiting and entering the boat). In this case, 1 x support crew to check with race crew that BATTERY SWITCHES ARE OFF for duration of change. No Crew are allowed on deck – Penalty Euro 1.000,00 .

PROCEDURE - APPENDIX 2-02

33. OUTSIDE ASSISTANCE

Refuelling is not authorised for any race boat whilst competing in the Pole Position or Grand Prix race.

Outside assistance may only be provided in the officially designated Pit Stop Area.

Changes - No change is allowed during Pole Position and Grand Prix Race, except if conforming to Pit Stop Rules. Otherwise the Penalty is Disqualification.

The UIM Commissioner has the right to decide if there was illegal outside assistance, which will result in Disqualification.

34. START PROCEDURE

Start/Pace Boat - The Start/Pace Boat Procedure is as described in the latest version of the Appendix 2 UIM On-water Procedures and Guidelines and/or the Race Instructions or race bulletins and must be followed at all times.

Description - The description of the Start/Pace Boat will be given in the Race Instructions.

Safety equipment -

- Life Jackets to be worn by all onboard personnel.
- Radio VHF marine system.

Start Procedure - The Start Procedure is described in full in APPENDIX 2-03.

35. FINISHING PROCEDURE

35.01. STOPPING THE RACE

Emergency Race Stoppage Procedures/Restarts (Force Majeure).

The procedure is included in APPENDIX 2-04.

The UIM Commissioner in agreement with O.O.D shall have the authority to stop an event.

Red Flag race stopped procedure (see Pace Boat Procedure Appendix 2-04).

The severity of the incident and race stop decision is taken only by the UIM Commissioner with information from his race officials.

If the race is stopped after completion of 70% of the race laps, then the boats will be classified based upon their position when crossing the previous lap timing line. In this case, a restart will not be implemented.

35.02. CURTAILING THE RACE

In the case of Force Majeure or accident, the UIM Commissioner in agreement with O.O.D may curtail (shorten) the race by the waving the Finishing (Chequered) Flag. Boats will be classified on completion of their current lap, based on their position at the Finishing line.

35.03. STOPPED / CURTAILED RACE

In the event of a Stopped or Curtailed Race, if the 'winning boat' has completed:

- A minimum of 50 % of the actual programmed number of laps: full points will be awarded.
- Between 25% but less than 50 % of the actual programmed number of laps: half points will be awarded.

- Less than 1 lap: the race may be restarted using the normal procedures.

Any Engine Bonus points are awarded following a race start if stopped by this procedure.

In the event of a Stopped or Curtailed race, boats must have completed a minimum of 70% of the laps completed by the 'winning boat' to be classified a finisher.

LONG LAP CALCULATION “Stopped/Pace boat Procedure”

- If a race is stopped before the completion of 70% and the Pace Boat is used, the mandatory long laps can be done after the restart at any time before the end of the race.
- If the race finishes while the Pace Boat is running 1 hour (time Limit), the order of the boats will be used for the final classification. Time compensation will apply provided that all the boats have done at least one Long Lap. In this case, the boats that have done a Long Lap will be compensated replacing their long lap time with the time of their best race lap (excluding the first/start lap).
- If the race finishes by the 50% lap, one Long Lap will be counted.
- If the race finishes by the 70% Lap, then two Long Laps will be counted.

LONG LAP CALCULATION “Stopped/Curtailed”

- If the race is curtailed between the beginning of lap 2 and the 50% lap and not all the boats have done at least one mandatory Long Lap, none of the long laps will be counted. The boats that have done a Long Lap will be compensated with the time of their best race lap (excluding the first lap/start lap).
- If the race is curtailed between the 50% and the 70% laps, two long laps will be counted. Time compensation will be applied to the boats that have done only one mandatory Long Lap, replacing their fastest Race Lap with the time of their previous mandatory Long Lap.

35.04. FINISHING PROCEDURE

White Flag - When the leading boat crosses the finish/timing line to commence its last lap, a white flag will be raised on the Finish Boat.

The white flag will also be announced via radio and timing computer.

Chequered Flag - When the leading boat completes the course and crosses the finishing line, the chequered flag will be waved. It designates the 'winning boat' for the Race and the start of the finish procedure.

The chequered flag will also be announced via timing computer.

The full finishing procedure is included in APPENDIX 2-05.

- The Winner, 2nd and 3rd place will be awarded for the Grand Prix.
- In order to be classified as an official finisher, a race boat must make a legal start. The Driver and Throttleman must remain in the boat during the entire race, except during Pit Stop procedure.
- After crossing the finish line, a race boat shall not interfere with any other boat still in the race so as to affect the time of such boat at the finish or endanger its crew.
- Official race results shall be posted on the Official Notice Board and on the UIM and Class 1 website.

A boat is timed when the bow crosses the finishing line.

A boat that finishes a race must follow the procedure described in the UIM On-Water Procedures and Guidelines and Race Instructions and/or Bulletins and/or Pilots' Briefing. Penalty Euro 1000 and Yellow Card.

The Team support crew cannot under any circumstance open any engine, cockpit or any other hatch on the race boat until they receive the permission of the Chief Technical Commissioner. Penalty Disqualification.

Posting of Results - The arrival order will be posted at a designated location, and may be announced on radio after the race. A racer or team cannot badger any officials for their decision which they are ruling on.

For any race that is subject to Post-Race Dyno Testing, the finish order is provisional pending report from Dyno Test Facility.

As soon as the results have been officially confirmed by the UIM Commissioner the final official results are posted. Once the official posting of the results has taken place, there is a period of one hour in duration during which protests can be made. These results are deemed to be provisional for the purposes of post race dyno and/or engine inspections testing and subject to any video evidence of race infractions.

35.05. 70% RULE

CALCULATIONS (PERCENTAGE/FRACTION)

Throughout the % rules, where a percentage/fraction needs to produce a whole number, the number will be rounded up or down to the nearest whole number. Where the percentage/fraction results in .5 (1/2) or less, the number will be rounded down unless specifically stated in the rule.

For example:

- 70 % of 7 laps is 4.9, 5 laps would count.
- 70 % of 5 laps is 3.5, 3 laps would count.

The minimum number of laps to qualify as a finisher may also be announced at the relevant Pilots' Briefing and/or bulletin. This should equate to 70% of the laps of the race.

Any boat crossing the finishing line after the 'winning boat', will be classified based on the number of laps completed and its time, provided it has covered the minimum number of laps.

Any boat that fails to cross the finishing line after the winning boat will be classified based on the number of laps completed and its time, provided it has covered the minimum number of laps.

The Course will close 15 minutes after the 'winning boat' has finished unless otherwise stated in the Race Instructions, Race Briefing and/or Bulletin. Any Lap completed after this time will not be included in the final results.

35.06. TIME LIMIT

All Grand Prix races will have a maximum time limit of 1 hour. The chequered flag will be waved when the lead boat crosses the line after the 1 hour maximum time limit, and all boats will finish as they complete that lap. The number of laps completed by the 'winning boat' will determine the number of laps for the 70% rule. Alternate distance may be set by Race Instruction.

35.07. NO FINISHERS

Should no boats finish, each boat will be classified based on the number of laps completed and its time, provided it has covered the minimum number of laps of the 70% rule, as announced at the relevant Pilots' Briefing and/or Bulletin.

35.08. RETURNING TO THE PITS

When a race boat retires, the strobe light must be on until in the wet pit. A Euro 600 penalty for infringement of this rule may be enforced.

35.09. RETIREMENTS

Boats, when retired, must report to Race Control on the VHF channel detailed in the Race Instructions. If a boat has announced its retirement to Race Control, the timing will be stopped at the moment of the announcement. Retired boats are forbidden to proceed on the course and must return to the wet pits, with assistance if necessary.

35.10. POST RACE

After the end of the race, to be classified as a finisher competitors must not receive any outside assistance, go alongside, collect other persons aboard, allow access to any deck/engine hatches or remove or place in electronic equipment etc., before reporting to the official Post-Race Scrutineering area and being cleared by the Chief Technical Commissioner. Disregard for this rule will result in Disqualification, with the exception of medical or mechanical emergency by permission of Race Control.

36. POST RACE TECHNICAL INSPECTIONS

36.01. All boats are subject to a post-race technical inspection conducted by a Technical Commissioner. The 1st, 2nd, 3rd and 4th placed boats will have all hatches fully sealed after the removal of the data logger memory card and will then be cleared by the TC after Torque Sensor recorded data review.

A list of approved inspectors shall be issued annually by the UIM O.P.C.

36.02. UIM Chief Technical Commissioner Discretion

The Chief Technical Commissioner has the discretion to develop and implement a Technical Inspection Plan for each Event, and may vary the items and boats to be inspected at each Event. The UIM Commissioner may also direct the Chief Technical Commissioner to conduct an inspection of any competing race boat.

36.03. FINAL RESULTS

No points shall be awarded until the legality is confirmed by the Chief Technical Commissioner to the UIM Commissioner, but still remain provisional subject to post race dyno and/or engine inspection and any video or virtual/Telemetry evidence regarding race infringements.

36.04. ENGINE INSPECTIONS

At the discretion of the UIM Commissioner or Chief Technical Commissioner-a dyno test may be required as a part of the post-race scrutineering.

The Chief Technical Commissioner, at his discretion, will:

1. Seal other parts of the engines,
2. Put his signature on critical removal parts,
3. Take pictures of critical parts,
4. Collect engine oil samples,
5. Keep the ECU, under UIM jurisdiction,
6. Require the engines and equipment to be sealed in the presence of a UIM official in shipping crates or boxes supplied by the team,
7. Agree with the Team representative on follow-up issues.

36.05. REFUSAL OF INSPECTION

Any entrant to UIM events refusing an inspection after having been notified by the UIM Commissioner or Chief Technical Commissioner that equipment within his control must be checked for conformity with safety and/or technical specifications shall be disqualified from that event and suspended for sixty (60) days.

Violations - In addition to the penalties provided in above clauses of Rule 36, the following procedures and penalties shall apply to post race technical inspections and violations.

First Offence - A first offence for a technical, weight or safety nature at any time during that racing year may result in the forfeiture of monies for the event, a deduction in position for that event - Fine Euro 1.000,00

This is applicable unless a penalty is established in any other specific rule.

Second Offence - A second offence of the same nature at any time, during that racing year will result in a mandatory disqualification from the event.

Subsequent Offence(s) - A subsequent offence(s) of the same nature at any time during that racing year will result in mandatory Disqualification from the event and the current racing year.

Team Responsibility - The owner shall be responsible for the condition of the engine and hull as raced. Errors on the part of the manufacturer, boat builder, engine builder, mechanic or previous owner shall not excuse non-compliance with the rules. The Owner is also responsible for their team's conduct at an event.

Additional Responsible Parties - The party responsible for making illegal modifications to engines shall be subject to up to one year suspension from UIM events and /or a Euro 50,000 fine and if a commercial homologated vendor, possible barring of all equipment prepared by them for competition in UIM Events.

37. POSTPONEMENTS

If a race cannot be run due to inclement weather or any other unforeseen circumstances, it can be rescheduled to run on the day following the scheduled date (if previously announced in the Advance notice), or a storm course may be substituted. This decision shall be made by Teams' and Pilots' Representatives, UIM Commissioner, Promoter's Representative, Safety Coordinator and THE OFFICER OF THE DAY (O.O.D)

Hourly postponements will be permitted when weather or other conditions are such as to make it unsafe to start a race at its designated time but there is a reasonable chance that conditions will improve. In no case shall a race be started if it will place race boats on the course within one (1) hour before sunset.

If a race cannot be run due to inclement weather or any other unforeseen circumstances, and cannot be rescheduled, the entry fee is not refunded.

Changes to the Event - Any race postponement or substitution of an alternate storm course shall be announced by Race Bulletin. The Race Committee shall adhere to the schedule set forth in the Race Instructions unless some emergency shall dictate otherwise.

In the case of any such changes, the UIM Race Officials must notify the teams in writing at least 30 minutes before the launching procedure as per rule 30.

38. PRIZE GIVING

1st, 2nd and 3rd place will be awarded for the Grand Prix.

Additional prizes may be awarded at the discretion of the Organisers.

The No.1 and No.2 pilots of the first three classified boats must appear at the prize-giving (and at the following press conference if any) at each Class 1 heat, unless agreed in writing by the Class 1 Promoter. The prize-giving (and the press conference if any) will be held within one hour after the end of the race. The pilots must wear their own race overalls.

Penalty for either of the above: Euro 1.000 fine.

39. LAY DAYS

Lay days are allowed if previously agreed with the UIM and announced in the Advance Program.

40. RESCHEDULING OF RACES

Should it be impossible to hold a scheduled race due to Force Majeure, the UIM may reschedule the race at a different time and place, the commitment of the teams to participate remaining unaffected, provided a minimum of 28 days' advance notice is given prior to the date of the original event that is being cancelled. Unless agreed by the promoter and the UIM.

If insufficient notice is given, the race may not be rescheduled and may be lost.

Reserve dates/venues will be fixed by the promoter in agreement with the UIM.

SECTION I

BOAT RULES

The following Rules apply to all boats participating in Class 1.

The technical rules follow for the construction of Class 1 boats and relating to the engines to be used in Class 1.

41. REGISTERED BOAT

41.01. NATIONAL FLAG

All boats are to fly the appropriate national flag of the designated driver as determined by the entry, with a minimum dimension 0.45 m x 0.30 m, throughout the race.

Alternatively, the flag must be painted, or glued, on a panel of not less than 0.45 m x 0.30 m on both sides of the hull.

41.02. RECOVERY OF DAMAGED / SUNKEN RACE BOATS

It is the Team/pilot's sole and ultimate responsibility to recover damaged/sunken race boats. The Local Organiser must assist where possible.

42. SPONSOR DECALS

Definitions - Sponsor Decals shall be defined as follows:

Series Decals (Compulsory) - Shall be defined as those specifically identified as such by the Class 1 Promoter. These are Decals for overall series sponsors.

Event Sponsor Decals - (Compulsory Single Event) Shall be defined as those specifically identified as such by the Class 1 Promoter. These are Decals for individual Class 1 Promoter Series Events. Event Sponsor decals are intended for single event display only.

Associated Sponsor Decals - (Fuel suppliers or other products, etc) Shall be defined as those specifically identified as such by the Class 1 Promoter. These are Decals provided by suppliers/service providers as sponsors in conjunction with the Class 1 Promoter.

DECAL PLACEMENT (GENERAL)

42.01. SERIES DECALS

The following Decals are hereby designated as Series Decals:

- UIM Logo - Decals shall be located, as per the handbook regulations.
- UIM Class 1 Identification Logo - It is the responsibility of each team to see that the required Decals are on the boat prior to entering Technical Inspection.

Logo Placement – The Promoter reserves the right to require their logo, and any Series Sponsor logo or patch to be present on the racing and dress uniforms in an appropriate positioning.

42.02. UNAPPROVED DECALS

Competitors should not display on their boat, uniforms or transporters, the logo of any unapproved Decals or any other mark identifying or otherwise referring to any other racing body. The UIM reserves the right to require the removal of any such logos or marks as a condition of entry into an Event.

42.03. IDENTIFICATION (NAME AND NUMBER) & HULL REGISTRATION NAMES OF BOATS

Any race boat may carry a boat name selected by the owner, but the name shall be subject to the approval of the UIM. Lewd, suggestive or vulgar names and/or graphics will not be permitted on race boats. Should such approval be withheld and until such time as UIM approval is forthcoming, that boat shall not be eligible to compete in any UIM event. The decision of UIM is final on such matters.

43. RACE NUMBER DIMENSIONS

The Race number has to be contained in a white square of 60 x 50cm for numbers 1 to 9, a white square of 70 x 50cm for numbers 10 to 19 and two digit numbers containing a 1, and a white square of 80 x 50cm for numbers 20 to 90 except two digit numbers containing a 1.

Race numbers must be black in colour, plain and upright and use the Arial font or similar. The number 1 in any number will not be Arial font but a plane upright single line white background must be surrounded by a 2cm thick black line with 12.5cm radius corners.

They shall conform to the following minimum dimensions: Height: 40cm Width: 30cm Thickness: 7.5cm Spacing:10cm (see diagrams). The numbers shall be positioned as far forward as possible.

The distance between the Race boat number area and any advertising will be 150mm.

Diagram numbers 1 to 9
(White cm 60x50)

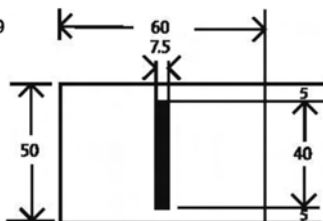


Diagram numbers 10 to 19
an 21 (White cm 70x50)

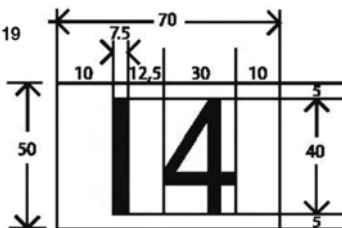


Diagram numbers 20 to 30
except 21 (White cm 80x50)



43.01. SIMILAR BOAT IDENTIFICATION

If a number of boats are competing in Class 1 of similar colour, it is requested that, in liaison with the contracted promoter, the team provide a differing feature to each which is easily recognisable from a distance.

43.02. REGISTERED HULLS

In Class 1 a separate record of points shall be maintained for each registered hull as tabulated by the UIM. Each hull registered shall be given a separate racing

number to be used for that season. The number will be utilised in all results to maintain a record of points earned.

Effective Dates - Registrations and assignments of racing numbers are for the duration of the current championship racing year, unless rescinded or surrendered. When requested, the numbers of the previous membership year may be reassigned if the application is received prior to start of current race calendar.

44. HULL DIMENSION, MINIMUM WEIGHTS AND EQUIPMENT

To be eligible to compete in Class 1, hulls must comply with the following criteria:

1. A price list of Hull and all major machinery/components for each boat are to be submitted to the OPC, following the instructions in ANNEX 6. 05. All equipment is to be available commercially, to teams or potential teams. The OPC may investigate any hull or component considered to be excessive in price or not as per the original, and take the necessary action to either ban or suspend its use.
2. Any existing hull built prior to 1st January 2001, measuring 12.0/14.63m (39ft.4in/48ft.0in) and weighing a minimum of 4950kg after race completion, will be grandfathered indefinitely.
3. Any hull built after 1st January 2001, measuring 12.19/13.41m (40/44 feet) and weighing a minimum of 4950kg after race completion.
4. Any hull measuring 11.5/13m using the 775+2%hp Naturally aspirated engines and weighing 4500kg after race completion.
5. Any Race boat found to be 50kg or more below weight will be disqualified, less than 50kg will be given a one minute time penalty.
6. Other hull/engine combinations may be permitted to compete in Class 1 through specific dispensation from the UIM OPC.
7. Any hull changes must not alter the total length or width; if the total length or width is changed the hull must be remeasured.
8. The boat weight may be checked at the end of the Pole Positions and/or Race. The weight will be an all-up weight including engines, residual fuel, drives and safety equipment as stated in the racing rules.
9. Weighing procedure will be defined as the scale of the day.

10. It is mandatory that fully dimensioned drawings of all liquid containers and/or tanks are given by each team before the first race to the OPC Technical Inspector. This drawing shall include measurements from the nearest reference point e.g. a bulkhead to show its location. Methods of filling and emptying must also be shown and the tanks must be able to be checked if empty or not. All onboard liquid ballasting must be coloured with a permanent dye.
11. Liquid Ballasting is not permitted to be pumped in from the sea or pumped overboard during Race or Pole Position.
12. Compression Flaps if fitted must be fixed and remain in the 1 position, they must follow in a straight line from the upper side of the tunnel with no stepping down or sloping of the trailing edge.
13. Compression flaps are not permitted on any boats measured after 17th October 2003.
14. A Hull, Machinery, Component or System must be constructed only of conventional materials. The only exotic metals allowed are aluminium and stainless steel. No other exotic metals (as listed in Annex 6.07) are allowed. The only exception is the use of titanium and Inconel as permitted in Rule 45.01 CAPACITY, TYPE AND RESTRICTION
15. Any existing hull as per 44.2.-3.-4. Modified in length, beam, and/or tunnel width must be registered as new boat and consequently for boats built prior to 1st January 2001, will lose the grandfather status granted in rule 44.2.

44.01. MEASUREMENT CERTIFICATE

A boat is not allowed to take part in a local, National or International race without a measurement certificate, issued by a National Authority, made up in the native language and in English according to the official UIM model. The certificate is valid for one year. It can be renewed without re-measuring, provided the rules for the relevant class have not been changed, or the boat modified.

Each National Authority stipulates the cost of measurement and the duration of the validity of the certificate. If no National Authority is available the UIM can issue a measurement certificate to confirm conformity to the rules.

The request to obtain a measurement certificate must be accompanied by the homologation file of the motor, if such is requested for the devices being used.

A - REINFORCED COCKPIT AREA AND CANOPY MANDATORY REQUIREMENTS

1. One single air supply (not oxygen) and a bottle will be provided for each riding crew member. The air supply must be securely fixed adjacent to, or on, each one of them. It is recommended that sufficient air be provided in each individual bottle for ten minutes.
2. Air bottles must have a pressure gauge fitted for visual checking at pre-race scrutineering. This gauge should be filled with liquid and be at least 5 cm in diameter for easy reading.
3. Air supply bottles shall be "Turned On" before starting a race or taking part in practice and/or testing. All air hoses to be secured and mouthpieces to be placed within reach of the pilots and mounted on retainers
4. Diving Masks for each crew member, stowed securely and accessible.
5. A minimum of 1 belt cutter or knife is to be secured in the cockpit.
6. Stop buttons/switches located in the cockpit area, immediately accessible to the crew and rescue officers. The stop buttons/switches must be identified by a fluorescent colour. These switches must shut off all fuel pumps as well as the ignition circuit.
7. A White High Intensity Strobe Light fitted to indicate "coming off the plane." The strobe light must be able to be operated by both crewman, and should be operated if a problem occurs, to enable any following race boats to take avoiding action. The strobe light shall be mounted on the top rear of the canopy. This strobe light may also be used as a substitute for the orange retirement flag when returning to port under reduced power.
8. A quick release steering wheel must be fitted on all boats but all pilots must be able to exit the cockpit without removing the steering wheel.
9. Rear view mirrors are mandatory, as well as a method of cleaning the canopy whilst underway.
10. Racing Vests - the efficiency of the racing vest is a matter of the exclusive responsibility of the wearer. Every crew member whilst on board must wear a racing vest during the practice runs and throughout the race. Racing vests must be colored high visibility orange or yellow. The racing vest must have epaulets/handles to help extract crew from the boat. The racing vest must have crutch straps or a method of ensuring that the vest does not "ride up".

B - OTHER REQUIREMENTS

1. Intercoms - Locking boat intercom plugs connecting helmets to racing craft are expressly prohibited. Plugs must disconnect easily, placing no additional stress on competitors' necks. Inflexible metal or plastic intercom microphone booms are strongly discouraged.
2. Radios - A licensed ship-to-shore radiotelephone must be permanently installed in an unexposed location with a minimum power output of twenty-five watts with international marine VHF channels available and any channel designated by the Race Committee as being essential for race communications.
3. Bilge Pumps - Two (2) bilge pumps, at least one of which it is recommended shall be hand-operated. Electric bilge pumps are to be automatic and wired so as to maintain operation independent of the battery cut-off switches.
4. It is mandatory that two approved fully charged powder fire extinguishers, each a minimum of 2kg, with metal pull rings and an indication gauge showing amount of charge secured in a position readily accessible to the crew, and mandatory to have a fully automatic fire extinguisher system. A standard Race Car Fire extinguisher Decal (Round Stickers with E in red circle) must be applied on top of the deck in correspondence with the extinguishers installation. All crew containment areas of inboard engine canopied boats must be fitted with a carbon-monoxide alarm.
5. Cleats - All cleats and other deck hardware must be recessed or protected.
6. Non-Skid - It is recommended that non-skid material be installed on the deck surface surrounding the cockpit to the satisfaction of the Chief Safety Inspector. (Clear non-skid is available). When the boat is sitting on its trailer, it is mandatory for mechanics, who are either walking on the deck or working in the cockpit and in the engine areas, to have three sides of protection to prevent either a mechanic or crew member from falling.

C - NOTES

1. "Cool Suits" - may contain any proven safe fluid or gas cooling agent other than Freon.
2. Eye protections - must be constructed of shatterproof material. Eyeglasses shall not be accepted as eye protection.
3. The UIM Commissioner - or Chief Technical Commissioner may prohibit use of any equipment he deems unfit for service.

4. Violation - of safety rules will result in a penalty assessed by the UIM Commissioner.

44.02. NEW BOAT CONSTRUCTION

Any constructor of a new build boat must complete a constructors/designer declaration form with its accompanying information and detail.

44.03. ESCAPE HATCH

All new boats measured after May 1999 must have a secondary escape hatch in the tunnel.

- A floor escape hatch in the tunnel must be fitted as the secondary means of escape.
- Any floor escape hatch fitted must remain operable during the entirety of the race and practice.

Refer also to Appendix 4 Technical Note 03, 05.

44.04. SUBSTITUTE HULLS (New Boats)

If a new boat, ordered and promised for delivery in writing by the builder, is unavoidably delayed in its delivery due to circumstances beyond the owner's control, documented in writing by the builder, a boat of like kind may be substituted until the new boat is delivered with the substitute hull's points being transferable up to Race 4 of the current racing year. UIM shall have the authority to grant extensions beyond the deadline in extenuating circumstances.

44.05. NEW NON-SUBSTITUTE OR REPLACEMENT HULLS REGISTERED BOAT

- Pilots may run different boats but points may not be accumulated or transferred from one boat to another boat, except in cases where the first driver carries with him the boat number (once accepted) and also in the following circumstances.
- For point allocation in the final classification, the registered boat is deemed to be the hull, engine, 1st driver and boat number used for the first race of the season.
- Points can be transferred in the following cases:

1. Should the registered hull become totally lost or totally destroyed (as certified UIM Officials and/or approved marine surveyors, in writing, at the expense of the owner), it may be replaced by like kind, for completion of the racing season, or if seriously damaged, until restoration of the original hull. (This replacement hull then becomes the registered hull during its period of use). This replacement hull may have engines with a different registration certificate. If the replacement hull is a boat already in the championship, it may re enter the championship under its original race number.
2. A 1st driver may also change his hull, with the same boat number, once during the season, provided this new hull is used for all the remaining races. In this case it is not possible for this designated driver/hull/number combination to use the previous hull any more in that racing season unless the replacement hull becomes seriously damaged. This replacement hull then becomes the registered hull.
3. A 1st driver may also change to engines with a different homologation certificate once during the season.

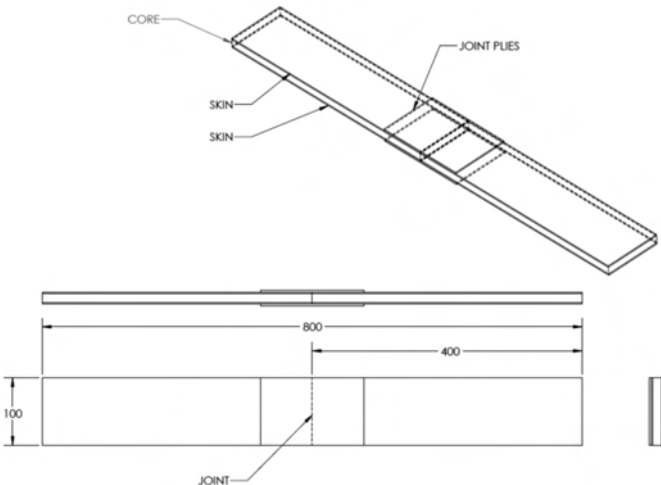
44.06. CANOPY RULES AND CREW

1. All boats must be equipped with a Reinforced Canopy/Cockpit, it is defined as a containment area for the crew and should be constructed as an integral part of the boat or suitably attached to boat if a drop in cell is fitted this should be complete with reinforced shield/crunch zones. This Reinforced Cockpit Area must be designed and constructed to a specification capable of withstanding the forces of a water impact when running at the highest design speed of the boat. A logbook for the boat is to be maintained and available to officials in order to provide a record of modifications, changes, damage and configuration of the boat.
2. A digital Event Logbook approved during 86th UIM G.A. must be implemented as from 2014; the web program is based on a modern Internet technology, logging all information available including the mandatory by- rules. The digital Event Logbook will be cloud-based with direct access from the UIM website, providing secure access through use of dedicated user credentials and all traffic will be SSL encrypted. (See details on APPENDIX 7)
3. All canopies in Class 1 boats manufactured after January 1, 2014 and all replacement canopies manufactured or installed after January 1, 2014 must be built by registered UIM cockpit builders. Constructors wishing to be registered must submit panels for test according to the following standards. Constructors meeting these standards will be registered as UIM registered cockpit builders.

UIM CLASS 1 COCKPIT/CANOPY TEST STANDARD

Sample Construction Requirements

- i. Sample quantity must be 3, one of which must include a sample of the joint/bonding of the cockpit parts, transverse to the length of the sample.



- j. Trimmed sample size must be $100 \text{ mm} \pm 1.0 \text{ mm}$ wide x $800 \text{ mm} \pm 5.0 \text{ mm}$ long, with the width being parallel.
- k. For fibre orientation the 800 mm length is to be parallel with the centerline of the boat.
- l. Sample must be laminated on a flat surface using the same manufacturing process, materials, and fibre orientations as the intended homologated cockpit construction.
- m. The sample must have a uniform thickness with no core crushing along any edges.
- n. The sample must have one moulded face and the other face being unmoulded, the moulded face will be taken as being the external surface of the cockpit during testing.

- o. The sample must be representative of the thinnest lay-up of the cockpit/canopy.
- p. The sample and cockpit must be manufactured using balanced or unbiased materials.

Sample Test Method

- d. The sample will be supported across the full width perpendicular to the 800 mm edges by two parallel 25 mm Steel bars at a distance of 500 mm apart. The load will be applied equally through two 25 mm Steel bars, each a distance of 167 mm parallel from each support.
- e. The molded face of the sample will have the load applied and the unmoulded face will support the sample.
- f. The load will be applied at 0.4 mm/sec and the deflection will be measured at the two 25 mm Steel bars applying the load within 2 minutes

Sample Test Requirements

- d. The sample when loaded with a force of 20,000 Newtons must have no more than a maximum deflection of 25 mm without the sample failing.
- e. The sample weight in gm/sq m will be calculated, skin thickness and sample thickness will be measured to enable inspection and comparison of damaged homologated cockpits/canopies.
- f. Further non-destructive test analysis methods may be used to compare test cockpits during the life of each cockpit/canopy.

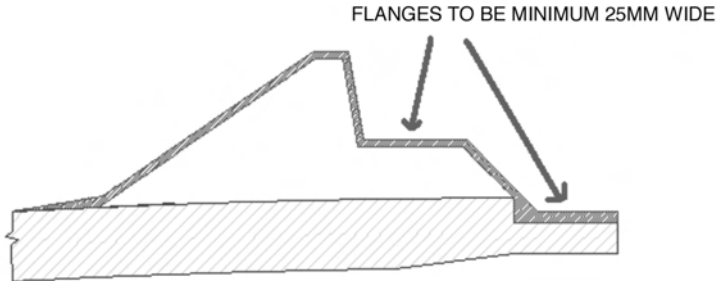
Sample Manufacturing Information Requirements

- j. Ply laminating sequence (stating which ply is the moulded face).
- k. Ply materials.
- l. Ply weavestyles
- m. Ply material weight in gm/sq m (dry weight ie. Without resin)
- n. Ply orientation (where 0degree is parallel with the 800 mm edges).
- o. Core material and density in lbs/cu ft or kg/cu m.
- p. Manufacturing method (stating vacuum, pressure, and temperature).

- q. A 100 mm x 100 mm sample of all materials used (resin samples not required) Samples as per sample construction requirements must be sent to the UIM appointed person.
3. All new boats built after January 2010 must conform to the 2010 rules. All current boats to be assessed if crunch zones and side water deflectors can be fitted. Any boat failing to carry out the Technical recommendations will not be eligible to race. All other canopy rules apply to the current boats.
4. New build manufacturers should contact the UIM for any planned rule updates due to the development program of canopy structure and safety designs.
5. Minimum weight to be increased to 4950kg to allow for the extra structure.
6. For design and construction an impact pressure of 250 tonnes per square meter shall be met or exceeded when applied to the main hatch and surrounding structure. An impact pressure of 100 tonnes shall be met or exceeded by the above the deck side of cockpit structure. The highest importance is given to protecting the crew in the event of severe accident. To this end, load analysis and definition will continue using advanced computational tools such as CFD and FEA. Any specification or safety device over and above the requirements will be assessed and supported as required.
7. The extremity of a Reinforced Cockpit comprises of a canopy, forward and rear bulkheads, sides, floor, and water deflectors. Additionally the cockpit is to be protected by a reinforced shield fitted into the boat surrounding the cockpit with a cavity between to form a 'crunch zone'.
8. Mandatory Water impact deflectors surrounding the cockpit, designed with a shape and structure so that the water is diverted around the cockpit and away from the crew in the event of the boat being breached to be fitted. There shall be no openings within the exterior confines of the impact shield/crunch zone allowing the forced ingress of water.
9. A continuous fitted structural framework inside the cockpit must be installed reducing the unsupported panel area sizes of the cockpit cell lining. The framework will incorporate roll bars fore and aft of the hatch with extensions to support the screen aperture divisions, the rear bulkhead directly behind each seat and the top escape hatch flange. The flange is to be a minimum of 25mm width (New build boats must have a minimum of 50mm) measured across the flange recess, with the hatch opening measuring 0.55m by 0.825m at the widest points. The canopy apertures should be cut with all corners having a radius of minimum 25mm. The radius should be constant and have a smooth finish to relieve stress. The canopy aperture must have a 20 mm wide (minimum) fluorescent orange band around the opening. All boats are

recommended to use the double flange method of installation as per the graphic below. All new build boats 2010 onwards must use the double flange installation.

10. There must be a minimum of 1 compression strut installed to support the canopy.



11. The main hatch being directly above the crew's helmets and its supporting flange must have further reinforcement to maintain its shape under stress and be able to withstand the impact of the water and retain its ability to function. The hatch thickness must be maintained throughout and not reduced at the flange where possible. The hatch should be protected by water deflectors incorporating internal and external method of prizing open the hatch caused by water pressure or binding to assist in emergency underwater rescue and escape.
12. The hatch shall be fitted with a catch which has a positive open and positive close mechanism and should hold the hatch against lateral forces. These catches shall be able to be opened from both inside and outside the cockpit and must have a second emergency mechanism to allow the rescue team to easily remove the hatch from outside if necessary. The hatch should be fitted with hinges with large bases and backing plates to spread the load and short release pins. This is important, because long pins invariably bind the hinge. It should be considered that the hinge centers be as far apart as possible. The release mechanism or hinges must not encroach within the canopy aperture area, and must not in any way hinder the exiting of crew members when fully race fitted. It is recommended that the catch have at least 2 locking points as far apart as possible, whilst still maintaining head clearance. It should be operated by a single handle with the addition of individual mechanisms internally in case of jamming. The hatch must be removable when the quick

release hinge pins are removed. The locking mechanism must protect against both forward and aft stuffing

13. Mandatory secondary tunnel escape hatch, it is strongly recommended that polycarbonate windows are applied to the tunnel hatch to help illuminate the cockpit for ease of escape. A waterproof light which is activated by water, and/or being inverted, with its own power source, **is mandatory as per offshore rule 508.26.**
14. Canopy hatches and release handles, must be painted fluorescent orange or have a fluorescent orange background panel to identify them, with directional arrows to indicate the method of opening must be provided both inside and out.
15. There should be one or more divers' grab handles fitted to the outside of the top hatch.
16. The clearance between and around the crews helmet and the canopy must be a minimum of 120mm.
17. It is mandatory to close the canopy hatch, and for the hatch to remain closed during all racing and practice.
18. Raised sections above deck level, including hatches and openings, connected to the canopy, at least to the extent of the mandatory water deflection zones must be structural and comply.
19. With the 'Reinforced cockpit area' rule. Any hatch within this area must have the same strength as the main cockpit hatch.
20. Cockpit seating must be side by side seating only.
21. The Cockpit should have flood tubes or other means of flooding the cockpit to equalize the pressure quickly in an accident. The floor of the cockpit should be as airtight as possible when in an upturned position. It is mandatory that sufficient buoyancy is provided in the boat, or in the material used for its construction, to ensure that the boat floats if capsized or holed, the hull should float as parallel with the surface of the water as is practical, to help in rescue accessibility. If extra buoyancy is needed, the buoyancy system described by the designer should be verified by the Measurer. This added buoyancy must be in at least four separate flotation units.
22. The various components that constitute the Reinforced Cockpit shall be properly maintained to ensure reliable operation of all components, with emphasis being placed on the canopy release mechanisms, emergency air supply and restraint systems, in accordance to the latest cockpit guidelines.

Windscreens

Polycarbonate (only) areas are strongly recommended to be as small as possible, while still maintaining that the pilot and co-pilot have clear, safe and undisturbed visibility ahead at sea level whilst racing. **Refer to rule C1 44.06.5.**

1. The combined visibility the pilot and co-pilot must be through a horizontal arc of 225 degrees (112.5 degrees either side of the centre line of the boat).
2. These polycarbonate panels are to be recessed into the composite structure and should be bonded using a suitable bonding agent, and/or “bobbins” (Bonding the Windscreen is the recommended method of fitment. Boats built after January 2010 must use bonding only, to fit the windscreen no bobbins will be allowed).
 - a. If the boat is requested, for any reason, to change the windscreen from 1/01/2013, it must use bonding only.
3. It is highly recommended that there is also a through bolted outer flange for the fitting of the polycarbonate panels.
4. Screen flanges should be a minimum of 50 mm, if bobbins are to be used they should be fastened every 100 mm it is recommended to use metal bobbins” with heads, as opposed to the recessed plastic type.
5. The outer polycarbonate area of the flange fitting must not be painted, so that the measurer/ scrutineer may monitor any discrepancies. A removable covering tape or other easily removable means can be applied to protect from UV rays.

Seats and Seatbelts Side of head protection

1. Cockpits must be fitted with **rear and side** of head protection for each crew member. This must be an integral part of the seat, which must be attached directly to the structure of the Restraint Compartment. The head protection must be a minimum of 0.2m wide and extend at least 80% of the height of the safety helmet as worn by the crew whilst in the normal seating position. There must be a minimum of 0.12m vertical and lateral clearance between the canopy and each of the crewmembers when in the normal seating position.
2. The Restraint System must consist of minimum of 6 point harness and should utilize a minimum 50 mm lap belt, a minimum 50 mm strap over the shoulder harness rated at 4,100kg (9,000 lb.) and protected to prevent chafing or cutting of the belt.

Harness straps must be attached directly to the cockpit structure. Those straps close behind the driver's head and neck must be 100 mm to 150 mm apart at point of attachment. The shoulder harness should be installed at 90 degrees to the spine at shoulder line to minimize compression injuries under high "G" loading. All straps must be free to run through intermediate loops or clamps/buckles. All anchor point bolts must be fitted with backing plates of 10cm minimum width. The driver harness attachment bolts in reinforced cockpits must consist of minimum grade EN8 bolts, with an 8 x 1.25 mm thread and locked nuts. There must be a spacer and plain washers on each bolt. The spacers must be glued to the cockpit structure. Intention of these spacers is to prevent buckling of surface material near bolts. This always leads to local delamination which easily spreads out over cockpit structure, when it is under stress.

3. On the sides of the structure, which has to take up the force on the attachment bolts, there must be a stainless steel plate (washer of minimum 3 mm thickness and 100 cm² area).
4. When using seats with suspension, and therefore not using a bulkhead restraint anchorage, drawings must be lodged with the National Authority of the measurer and approved prior to boat measurement.
5. All restraint systems must have a common method of release. The single lever method (sometimes called the NASCAR type) or rotary type, are both acceptable restraint release systems. Both types of restraint release must be examined for satisfactory operation by the scrutineer before every race.
6. The shoulder harness should be installed 90 degrees to the spine at shoulder line to minimize compression injuries and the high "G" loading. 75 mm minimum/maximum to Centre line of Lap Belt at Seat Back, Seat Bottom junction. Lap Belt should continue in straight line to anchorage.
7. The seatbelts must be Supplier certified for longevity time at the first installation, and annually checked and certified for the following 12 months. The original certification must be presented to the Technical Scrutineer

DRAWINGS AND MEASUREMENT

Drawings of the Reinforced Crew Canopy/Cockpit to be supplied will include the following:

1. The type of Canopy, Plan, side and elevation.
2. the Bulkheads and buoyancy system

3. Restraint System anchorage Points
4. Canopy aperture dimensions
5. The type and method and construction of release devices.
6. Material specification of the transparent areas.
7. Water impact deflectors and crunch zones
8. Material mechanical property data of the canopy, hatch, cockpit and crunch zones, running surfaces should be provided in support of the drawings to establish a database for future assessment and analysis.

Prior to Boat Measurement the drawing and material specifications shall be sent to the Measurer requested to measure the boat. On completion of measurement, the drawings and material specifications called for by the designer shall be lodged with the measurers National Authority before they issue a certificate of compliance and measurement. Drawings must also be submitted to the UIM for all entries to UIM races prior to racing.

The construction, strength safety and conformity to meet and exceed the recommended requirements is the sole responsibility of the boat owner and or boat builder. All points of impact incidence to the canopy, hatch and screens, (as being a primary impact area) cockpit and crunch zone extremities should be afforded the diligence deserved in protecting the crew. Any scrutineering or measurement by the UIM officials is not condition/construction survey.

Non-compliance - The Chief Technical Commissioner has the authority to deny entry to any race boat subject to these rules that has non-compliant cockpit safety systems. The Chief Technical Commissioners also has the authority to allow a non-compliant boat to compete, provided that after consultation with the Chief Technical Commissioner, the O.P.C Technical Task Force determines that the intent of the guidelines has been met and that the safety of the riding crew and fellow competitors is not jeopardized.

SECTION J

TECHNICAL RULES REQUIREMENTS

45. UIM CLASS 1 ENGINE SPECIFICATIONS FOR REGISTRATION AND RACING 850 HP +2% TOLERANCE

1. Deliberate breach of any engine/Drivetrain rules either written or implied at any time will result in a minimum of Disqualification. The Technical inspector may remove or exchange any part of the engine or Drivetrain and replace it with a substitute, so as to inspect the component or part for adherence to the rules.
2. Registration of new engines entering UIM Class 1 must be received no later than 10th February for appraisal. The OPC may accept registrations after 10th February, upon acceptance engines would be eligible for race entry and points:

For engine appraisal the following information/declaration must be sent (use the form in APPENDIX 6.02):

- a. Natural aspirated or Compressed (please for: Natural Aspirated 775 and 850V8 see specific rules)
 - b. Number of cylinders
 - c. Number of valves per cylinder
 - d. Maximum displacement
 - e. Stroke and bore dimensions
 - f. Compression ratio
 - g. Number of fuel injectors
 - h. Any and all components made of exotic materials.
 - i. OPC will welcome other gasoline forced induction engine configuration. Manufacturer must present the product details to OPC to commence the above appraisal process prior to approval to start the homologation procedure.
3. To ensure a form of parity between new engines entering and the current engines, currently running in Class 1. Each manufacturer/Tuner is to submit Dyno results using the air restrictor, PCM or Carburettor as approved by the

UIM OPC no later than 20th February consisting of information on Dyno test result, detailed every 250RPM (dyno sheets)

4. The OPC may request that the Torque sensor be fitted for the Dyno test. Engines that do not have near the maximum horsepower restricted using the air restrictor, may only use OPC approved electronic control devices on the engine.
5. Engine Dynamometer (Dyno Test facilities, and/or Torque Sensor) correction method ISO 3046-1 is to be used (the old 88/195/CEE method is not to be used)
6. The mandatory Torque Sensor with inbuilt data logger must be purchased by the Promoter and leased to the Teams following the protocol in APPENDIX 6.01. The torque sensor(s) will measure the actual engine power output during the entire official race weekend by continuously recording torque and RPM and computing the instantaneous horsepower being transmitted through the respective drive shaft after the “crash box” and ahead of the drive system.

The horsepower thus measured is the total sum of the engine's(s') output delivered to the boat's drive system(s) through the drive shaft(s). The power losses from any additional device/s such as a geared drop box, hydraulic oil or water pump/s, electrical generator/s, etc., that are not part of the engine's original support system, and which are installed between engine and torque sensor, will be added to the power measured by the torque sensor during the race to give a total engine power output that must, at all times during a race event, remain below the mandated power limit. Teams may be requested to duplicate race-recorded data on the Dyno, if their engine is chosen and sent for testing during the season, with the dedicated torque sensor from the race, if both the dyno and torque sensor readings match within 1% tolerance the data recorded during the race is confirmed. In the event of torque sensor failure during the dyno test a replacement torque sensor will be installed. The Chief Technical Commissioner will inform the team, after the free test on day one of the event, if their RPM and /or Torque and Power are out of legal range. Data taken from engines during pole or race may be judged by the technical commissioner, as to whether they are actual breaches of the rules or simply running data spikes. Breaches will receive a penalty from the following list, added post-race:

- a. RPM infringement:
 1. Up to five: 30 second penalty each.
 2. Multiple deliberate infringements above five: disqualification

- b. Power and torque infringement
 1. Not complying during Pole and Race: Disqualification
 2. If a new engine is installed after free test, replacing the registered one, during the same event weekend and has no previous record with Torque sensor: five minute penalty
7. Engine must be registered with UIM OPC, with the form in APPENDIX 6.03. to be eligible to race.
8. Engine vendors and teams must make their engines available for any inspection which may include Dyno testing, complete stripping or any other inspection deemed necessary at the Vendor's or team's cost to ensure legality of engines and components at all times.
9. Each engine manufacturer's request for registration will be reviewed positively, but full consideration will be given to the satisfaction of the spirit of these rules as well as the written specifications.
10. The engines MUST be based on automobile, marine, or industrial cylinder blocks and heads. Specialty non production heads and blocks will not be accepted, Engines must be commercially available.

The follow information/declarations are required in the appraisal file (APPENDIX 6-02):

1. Certificate of origin for the heads and block.
2. Declaration that the engine will be available for any kind of use, to any entity, including competing race teams; and that they will be available within a reasonable time frame and at a commercially realistic price, estimated in € (time frame and final price to be agreed in writing with the UIM/OPC).
11. When Engines are presented for scrutineering, they must have the sealing and lock wires already twisted in place, ready for the fitment of the numbering and sealing tags. The precise method of carrying this out is shown in technical notes supplied at engine registration.
12. The seals will prevent the following operations: Splitting of the crankcase for access to crank and bearings. Removal of cylinder heads for piston, rod, and liner withdrawal. Removal of camshafts, valves, and valve springs, pushrod engines may use split rocker covers that allow the adjustment of tappets only, without breaking the seals.

13. For any race that is subject to Post-Race Engine Dyno Testing, the finish order is provisional pending the report from the Dyno Test Facility. Teams must declare the type and brand of engine oil used for the race in question.
14. Costs for Dyno testing engines and UIM Commissioners involved that are protested against will be paid by the losing party.
15. Engines that are chosen to be dyno-tested, have the choice of the following with no loss of engine bonus points:
 - a) If the engine is returned before the next race - refit the engine
 - b) If the engine is not returned in time for the next race, it can be fitted at a later date during the season with no penalty and points will be counted as if they had been awarded for consecutive races. The engine must remain sealed.
 - c) If the engine fails or encounters technical problems on the Dyno, the engine may be repaired and fitted at a later date during the season with no loss of points.
 - d) If an engine is sent for testing on the second-to-last race of the season and cannot be fitted for the last race due to points b) and c) above, the new engine fitted will act as a substitute, and points will be given.
16. All 850 hp+2% engine rules apply to the 775hp+2% Naturally aspirated pushrod engines by substituting 850hp with 775 in the text.
17. All engines will be fitted with loom and sensors, that will record to the torque sensor logger, the rpm, max boost pressure, and air depression inside the air box;
18. SINGLE MAP ECUs REQUIRED

Only ECUs with a single map “built in” are allowed, carryovers of old ECU's are accepted only if modified and certified by their original manufacturer. Protocol for updating and checking software. on APPENDIX 6 09.

ECU's are subject to random testing at the discretion of the Chief Technical Commissioner to confirm conformity to the rules.

Only ECUs with a single map “built in” are allowed, carryovers of old ECU's are accepted only if modified and certified by their original manufacturer. Protocol for updating and checking software will be issued by the OPC.

ECU's are subject to random testing at the discretion of the Chief Technical Commissioner to confirm conformity to the rules.

19. THROTTLE BY WIRE.

Throttle by wire is allowed once the homologation file on the existing engine is updated, and displayed on the new engine homologation with the following conditions:

1. Engine manufacturer declaration specifying that the points on Appendix 6.06.1-2 and 3 are respected, specifying no strategy of Traction Control (53.50)
2. Engine manufacturer declaration specifying the potentiometer in the cockpit that manages the throttle via ECU, must not be connected to any other device, as per rule 53.9 and following statements.
3. Engine manufacturer declaration stating that in any case of malfunction the Throttle goes into a closed recovery position to guarantee maximum security.
4. To ensure the above, the Engine manufacturer must supply to UIM the following :
 - a. Construction drawing of the throttle by wire looming installation to be updated with running changes, if any, on the following sections:
 - i. Pin to Pin between Potentiometer throttle command and device that receives the signal.
 - ii. Pin to Pin between Engine ECU and throttle by wire on the engine
 - b. Provide updated engine map, to be used in conjunction with the Engine manufacturer as a comparison at any time requested by the UIM Technical Commissioner, particularly on Throttle by wire management.
 - c. Provide at free cost, equipment and software to record during race events in actual time (maximum delay 1 second) the following data:
 - i. Engine RPM
 - ii. Position of Throttle by wire (butterfly on engine)
 - iii. Input from potentiometer connected to the Throttle inside the cockpit

45.01. CAPACITY, TYPE AND RESTRICTIONS (non-compliance will result in Disqualification)

1. Max 4 camshafts per engine.
2. OEM camshaft position cannot be changed.
3. OEM position of lifter bores cannot be changed.

4. Pistons to be produced from automotive piston alloys.
5. Heads and blocks to be manufactured in cast Aluminium or Iron only.
6. Max 4 valves per cylinder.
7. No exotic material as per APPENDIX 6.07. with the exception of Inconel for Headers on Exhaust System, Titanium on cylinder head valves and spring retainers, no Titanium coatings are allowed.
8. No Ceramic components or coatings with the exception of spark plugs, seals and bearings.
9. Any Diamond or hardened coatings.
10. Valves must open and close using conventional mechanical methods.
11. No variable length tracts or ducts.
12. Original registered quantity of spark plugs to be retained.
13. Mercury 850 engine with standard factory ECU, type to be approved by the OPC, No blue printing or Modifications of any kind Allowed, maximum 6250 Rpm random cylinder cut out, 6350 full cut out as per Mercury standard,. Other supercharged engines may be accepted by the OPC for racing subject to approval, the Mercury 850hp torque curve printed on ANNEX 6. 04 is not to be exceeded.
14. 6100cc-8200cc overhead cam V12 Naturally aspirated engines, 60mm restrictor, maximum RPM 7600.
15. 8111cc-9521cc(495 to 581 cu inch) pushrod engines, single camshaft, maximum RPM 7600, (see specific OPC engine data sheet for restrictions and specifications)
16. No non-piston or unconventional engine are allowed - engines must be based on standard production engines
17. Any form of traction control managed by the engines' ECUs with any kind of slip sensor are forbidden; management of the engines' RPM limiter related only to the engine's acceleration rate is allowed, the protocol of rpm management is specified on Appendix 6.06

45.02. COMPRESSION RATIO MAXIMUM ALLOWABLE

- Natural aspirated 10.0:1
- Pressure charged 8.5:1

- Pushrod engines 10:8:1

46. AIR BOXES AND AIR INTAKES

1. Internal or external spraying of any substance into the airstream except fuel through the engines injection system is forbidden
 - All Single Camshaft Pushrod engines
2. The air track running from an external opening to the flame arrestor/Air Cleaner may not be completely sealed. Likewise, sealing or pressurizing of the engine compartment or engine air intake is prohibited. Fresh air ducting is not allowed to be closer than 50mm in any direction to the engine and/or flame arrestor/ Air Cleaner.
 - V12 Naturally aspirated engines with restrictor
3. The maximum dimensions of the Plenum Chamber must be contained within an imaginary straight-sided square-edged box. The volume of this box must be no more than 1 cubic meter.
4. The Air box dimensional schematic showing all components including anything passing through or placed inside the air box must be supplied to the UIM OPC
5. A single air restrictor is to be situated at the first point of air entry into the mandatory sealed plenum chamber/Air box. This air restrictor will be issued by the UIM OPC (at a cost of Euro 650 each) and will be engraved with a specific serial no. The air restrictor will be anodized to provide ease of inspection, a go and no go mandrel will be used to check the restrictor size has not been modified. No other restrictor will be accepted
6. The air restrictor must be placed in an accessible position for easy visual and dimensional checking. If required it is permitted to be forward facing above the boat's deck.
7. The air restrictor must be fitted in such a way that it can be removed and substituted for another UIM OPC official air restrictor at any time.
8. All air feeding the engine must pass through the air restrictor. The air box must remain airtight at all times and is subject to post race scrutineering.
9. Conformity of the air box tract seal will be checked using the official UIM OPC "vacuum gauge" applied to a running engine. The check will be performed at post race Scrutineering The engine speed during the check is set at approx 2200rpm. A minimum reading of -100 millibars must be reached to confirm

100% seal. Should the reading fail to reach -100 millibars, the induction tract seal will be considered illegal.

- Reading from -100 to -85,00 millibars time penalty 20" from classification time
- Reading from -84,90 – 70,00 millibars time penalty 50" from classification time
- Below -70,00 millibars disqualification.

47. ENGINE QUANTITY & POSITION

- Maximum of 2 engines per boat.
- Inboard engines only.

48. FUEL & INJECTION

- Gasoline powered engines only.
- Max 2 fuel injectors per cylinder.
- The only fuel approved for use in UIM Events, will be that supplied by the local organiser or series supplier, as the case may be. The Unleaded fuel supplied will have a minimum of 95 Octane. Unless otherwise notified in the Race Instructions.
- No modifications or additives are permitted to the fuel.
- Fuel samples showing any characteristics other than the fuel issued at the event, will be deemed illegal. The penalty is Disqualification from the event for a first infringement, and more serious penalty for a second offence.
- The UIM inspector can at any time take samples from boats to ensure compliance.

49. ENGINE RULE FOR 775 AND 850 NATURAL ASPIRATED V8 (NON-COMPLIANCE WILL RESULT IN DISQUALIFICATION)

1. Displacement, maximum 581 CID

2. Displacement, minimum 495 CID
3. Compression ratio Maximum 10.8:1, no minimum – Measured by Whistler at race site – before or after race.
4. Number of valves, two valves per cylinder operated by pushrods. One camshaft located in the block, OEM location.
5. Intake valves, allowed: Stainless steel, or titanium
6. Exhaust valves, allowed: Stainless steel, or Inconel
7. Valve stem diameter, minimum 11/32"
8. Variable cam timing, Not allowed
9. Camshaft Must remain in OEM location
10. Valve guides Not to be cut down, shortened, tapered, or reshaped
11. Valve angle As manufactured OEM + - 2 deg
12. Valve head diameter, maximum Intake 2.450", exhaust 1.950"
13. Valve spring cooling Allowed with internal engine oil
14. Spring Retainers Any material and design may be used
15. Rocker Arms Except as stated above, any type or design legal
16. Rocker stud girdle Legal
17. Engine Blocks: Cast iron – painting block is allowed, No aluminium blocks allowed, GM Blocks with 9.8" or 10.2" deck heights approved, GM Blocks manufactured by General Motors, World Products (Merlin), Dart, approved. GM part #, approved 10185049, 10134367, 24502502, 24502500, 12370834, 14044808, 12370833, 10051106, 10237292, Merlin part #, approved 080100, 080110, 081100, 081110, 081102, 081112 and 081120, 085010, 085110, Dart part numbers, approved 31263344, 31263354, 31263444, and 31263454.
18. Cylinder bore centers Must remain in OEM spacing
19. Internal Components: Materials allowed, Crankshaft, connecting rods, pushrods and wrist pins must be made of steel. Wrist pins may be hard coated, Materials, not allowed Titanium or any other materials
20. Main and rod journals, Not to deviate more than .040" in diameter from OEM specifications

21. Connecting rods Big end width may not be reduced from OEM specifications. Width of rod big end not to be less than .980"
22. Wrist pins Minimum .990" inch diameter
23. Pistons, allowed, Forged aluminium only
24. Thermal barrier coatings, No thermal barrier coatings allowed on any engine parts
25. NO Single plane crankshafts Crankshaft throws must be timed according to OEM specifications
26. Angle Milling, Not allowed
27. Cylinder Heads: Must remain in OEM location. Intake and exhaust ports must be in conventional OEM location and spacing (NO BIG CHIEF STYLE CYL HEADS). 1.Allowed:CNC machining, Blending, Grinding and Polishing, Resizing of ports, 2.Cylinder head repairs welding to repair cylinder head must not change original manufacturers shape
28. Head bolts Additional head bolts legal. Any type legal.
29. Intake Manifold: Approved intake manifolds, Commercially available 4bbl cast intake manifolds only. 1.Welding Not allowed. 2. Porting Allowed
30. Carburettor spacers Maximum 2.5"
31. Intake Manifold, The angle between deck surface and intake manifold surface not to be altered.
32. Induction System: Only one carburettor or 4bbl throttle body, if injection used Approved type, Holley dominator bolt spacing
33. Fuel injection Maximum 8 injectors only
34. Blueprinting Allowed
35. Exhaust System: C.M.I. 2.25 E-TOP 1075 HEADER #13595 ~~⊖~~. No modifications allowed. Alternative suppliers may be accepted by the OPC, in writing, for reliability/durability reasons only.
36. Cooling, Exhaust system from engine outlet to point of exit from hull or deck must be water-cooled by water jackets. "Cool Collar" design water jacket exhaust cooling is allowed.
37. Lubrication: Wet sump, Wet sumping and internal oil pumps allowed

38. Dry Sump External oil pumps / dry sumping allowed. Not more than three scavenging pump sections allowed. Not more than three hoses (maximum size –12AN) shall be connected from the oil pan to the scavenging sections of oil the pump. Oil may only be scavenged from the oil pan.
39. Crankcase vacuum, Not allowed. Engines must be vented to the atmosphere
40. Crankshaft oil seals, Conventional style only
41. Filter screens, Allowed. May not restrict the return of oil to the crankcase. Flush mounted only.
42. Intake valley, As cast only. No damming, no raised sections, no alterations whatsoever.
43. Ignition: Distributor type only, Spark distribution Distributor rotor / cap only
44. Electronic ignition Allowed, Crank triggered ignition allowed, Belt drive ignition allowed.
45. Coils, Limit one ignition coil per ignition system (primary and back up). Back-up ignitionAre allowed. Illegal if any combination is capable to exceed specified rev limits.
46. Rev limiter. The maximum RPM allowed is 7,600 RPM with no tolerance above 7,600 RPM. Engines which have ignition systems exceeding 7,600 RPM are illegal as determined by OPC test equipment.

50. EXHAUSTS

- Exhausts to be adequately insulated as to prevent the possibility of fires or burns.

50.01 MAXIMUM PERMITTED NOISE LEVELS

- The maximum permitted noise levels from any individual competing boat in a UIM Class 1 race is 100 Decibels +5%.
- Testing may be instigated by the Technical commissioner for noise readings during the race weekend or upon request at any time.
- Readings are taken at 50 metres distance, at max RPM+/- 2% with no other race boat in the vicinity.
- Readings are only accepted as official when taken with the official UIM calibrated sound meter.

- Race Venues that do not require a limit on engine noise levels, must first obtain approval from their National Authority. Upon receiving written notice from their National Authority, the OPC Task Force will give its final approval and which will be written into the Advance Notice of the stated event. Only in such a case are teams free (not mandatory) to use the noise free equipment without altering the engine performance as per rules.

51. TRANSMISSION RESTRICTIONS

- No method of shift able gear ratio selection is permitted in the driveline between the engine crankshaft and the propeller, other than neutral facility. C1 boats are not required to have astern propulsion ability.
- No forms of variable speed transmission or torque biasing devices are permitted.
- During the event, all ratio changes (if made on the launch jetty) must only take place under the strict control and agreement of the Technical Commissioner, and be achieved by mechanical parts replacement only.

52. PROPELLERS

Propeller registering, marking and measuring procedure.

- There will be 1 supplier of propellers for Class1,
- Only Cast propellers are allowed
- One specified (regular) blade area only is allowed.
- The Propellers thickness cannot be altered, and will be specified by the manufacturer and or the UIM, disqualification will result for modified propellers.
- Only the propeller cup may be altered; however, this will void any warranty claim that may arise. The manufacturer will specify the areas that can be modified. No superficial treatments are allowed unless requested by the supplier to improve the durability, and then this must be applied to all propellers racing in the boat fleet by the same supplier.
- The propellers will be produced from the manufactures C1 specified material only.
- 3 different pairs of propellers per boat will be allowed. Propellers can only be changed for the identical type as those initially registered due to damage or wear. In that event it must be reported to the Technical Commissioner, It may

then be replaced and the registered number will be cancelled from the eligibility records, and the new number submitted.

- New propellers will be stamped at the end of the first World Championship, Pole or Race in which they were first used, and will be registered by the Technical Commissioner.
- Propellers will be allocated to a boat, for use on that boat only. For instance a two boat team may not swap registered propellers from one boat to the other. However a two boat team with less than 4 sets of registered propellers may use them on either boat.
- Replacement warranty of Propellers, is only for material failure and doesn't include misuse or damage
- Running life at each race venue of each set will be recorded by the UIM Technical Officer. During any use of the registered propellers in private testing, the running time must be reported to the UIM Technical Officer at the next event to account for the total running life. Failure to report will result in void warranties.
- It shall be the responsibility of the competitors to ensure that the propellers are protected while operating on land, and during gear ratio changes in the wet pits. Penalty for non-compliance will be a fine of Euro 1000.

53. CREW CONTROL AND SYSTEMS

Technical restrictions for all boats.

1. Steering must be by either drives or rudders not both. Steerable drives to be homologated before acceptance for racing.
2. Rudders must only be allowed to move on one axis (No retractable rudders). (To be investigated for 2012)
3. Drives must not steer independently of each other (no toe in or out underway). Additionally the tie bar is be fixed mechanical.
4. No Auto gyros.
5. Any form of traction or acceleration control.
6. No presets of any kind including trim and steering.
7. Potentiometers can be used for monitoring only.

8. All systems must be homologated following the instructions in ANNEX 6.05 and fully inspected before installation, including all drawings and details of components. The system must meet the rule and the spirit of the rule in all aspects. The scrutineer's decision to not allow any or all components in the system is entirely at his discretion and cannot be appealed.
9. The system cannot be improved, changed or modified in any way without prior written approval.

Rule – “All control inputs affecting trimming and steering of the boat must be directly provided by the crew. It is not permitted to interface the operation of the control surfaces, drives, ballast tanks, fuel tanks or throttles with any devices that provide additional control input, whether electronic, mechanical, hydraulic, pneumatic, or any other source.

Crew derived control inputs may be amplified, in force and/or displacement, using suitable power assistance systems, but the response of the control device must remain a constant and direct function of the crew input.

Any additional sensing devices, such as, but not exclusively, those used in a data acquisition system, must be demonstrably physically independent of any individual control system.”

54. DATA TELEMETRY

Teams may not upload or download data whilst the vessel is moving during any race weekend. Data loggers may be used.

55. ENGINE BONUS POINTS

All eventualities within the Engine Bonus points Rule will be judged according to the following text together with the published Bonus Points scenarios. Any unforeseen scenarios will be judged by the OPC Technical Task Force within the spirit of the engine longevity requirements.

1. Bonus Points will only be allocated for the World Championship (i.e. they do not apply to Continental Championships).
2. Allocation of points is to a boat / engine combination, and they are not transferable with the engine to another boat.
3. Engines presented for scrutineering with broken or no seals, will not be eligible for points that race weekend, this includes the first race of the season. Engines presented with previous seasons unbroken seals will be treated as a new engine that requires seals and not be eligible for points that race weekend

4. Race weekend completed with seals unbroken and or engine replaced will get 2 points per engine per race weekend after the first completed race weekend. The boat must at least leave the jetty for the start of race two of the race weekend to be eligible for the bonus points. Race 2 remains the defining changing and completion point for the bonus points.

YOU LOSE POINTS IF YOU CHANGE AT ANY TIME											
RACE WEEKEND 1			RACE WEEKEND 2			RACE WEEKEND 3			RACE WEEKEND 4		
PR	R1	R2	PR	R1	R2	PR	R1	R2	PR	R1	R2
		0 POINTS			2 POINTS			2 POINTS			2 POINTS
NO POINTS FOR THE THE FIRST COMPLETED R2											
PR	R1	R2	PR	R1	R2	PR	R1	R2	PR	R1	R2
		0 POINTS			0 POINTS			0 POINTS			2 POINTS
					DNS OR DNF R2			ENGINE SEALS UNCHANGED			
PR	R1	R2	PR	R1	R2	PR	R1	R2	PR	R1	R2
		0 POINTS			0 POINTS			2 POINTS			2 POINTS
			SUBSTITUTE ENGINE FITTED DURING RACE WEEKEND SEALED ENGINE FITTED RACE 2								
PR	R1	R2	PR	R1	R2	PR	R1	R2	PR	R1	R2
		0 POINTS			0 POINTS			2 POINTS			2 POINTS
			ENGINE CHANGED BOAT COMPLETED RACE 2 WITH NEW ENGINE								
PR	R1	R2	PR	R1	R2	PR	R1	R2	PR	R1	R2
		0 POINTS			2 POINTS			0 POINTS			2 POINTS
						NEW ENGINE FITTED AND SEALED DURING SCRUTINEERING					

APPENDIX 1

SAFETY PROCEDURES AND GUIDELINES

SAFETY PROCEDURES

01. GENERAL REQUIREMENTS
02. TECHNICAL RECOMMENDATIONS FOR SAFETY BOATS
03. INTERVENTION PROCEDURE

SAFETY GUIDELINES

SAFETY

LAND SERVICES

- A. DRY PIT AND WET PIT AREAS
- B. FUELLING AREA
- C. HELICOPTER AREA (IF REQUIRED)
- D. HOSPITALITY AREA

ON WATER OFFICIAL TIME LAND SERVICES

SEA SERVICES

- E. RESCUE BOAT
- F. TOW BOAT
- G. FIRE BOAT
- H. COURSE PATROL BOATS
- I. FINISH BOAT
- J. COURSE CONTROL BOAT
- K. CONTROL BOAT

ACCESSORIES AND SPECIAL EQUIPMENT

SPECIAL INFORMATION TO BE PROVIDED BY THE LOC

RECOMMENDED SAFETY PROCEDURES

Minimum requirements for the realisation of a Class 1 Grand Prix

01. GENERAL REQUIREMENTS

- 01.01. the recommended minimum depth of the water for the race course area, in normal condition, is 4 meters in the turn mark; this depth can be reconsidered by the Safety Coordinator, in case of bad sea weather condition and or in case of particular geographical local condition;

- 01.02. the logistics point on land where safety boats and ambulances are located must not be further than 2 nautical miles from the furthest point of the race course;
- 01.03. the mooring post for the rescue boats has to allow for a parking area for ambulances;

02. TECHNICAL RECOMMENDATIONS FOR RESCUE BOATS

- 02.01. The rescue boats should be:
- a minimum of 7.5 metres long with free bow in order to have enough space for stretchers;
 - sides and/or stern can allow an easy boarding of stretchers and/or must have a system for the easy loading of stretchers;
 - cannot transport on board minors (18 years) and/or persons who do not belong to the crew or to the rescue group;

- 02.02. Considering that race courses have a maximum lap length of six nautical miles, the safety plan, coordinated by the Safety Coordinator, according to the Local Safety Officer, with the assistance of the Local Medical Coordinator, envisages a minimum number of six rescue boats. This plan is created according to the grid method in order to be able to patrol an area of 1 mile for each rescue boat.

Following this method, the rescue boats will be implemented if the race lap is longer than 6 miles and, in any case under the Safety Coordinator approval;

The Safety Coordinator, according to the Local Safety Officer, is responsible for the repositioning of the safety Plan, according to the interventions and in order to cover the whole race course.

- 02.03. Each rescue boat is assigned a Tow boat and they form the safety group positioned in the grid plan. The rescue boat is the group leader.

The group has to take its position according to the safety plan and it has to maintain the position by maneuvering the engine (avoiding the creation of waves), at a minimum distance from the course line of 200 metres and, in any case, in a safe position. Every group has to be constantly in contact and has to listen to Race Control on the assigned radio channel. The driver's assistant is assigned to carry out this function.

03. INTERVENTION PROCEDURE

- 03.01. Every safety group, composed of one Rescue boat and one Tow boat, has to be in the assigned position at least 30 minutes before the official start time for practice, pole position & race.
- 03.02. Every group has to check how the practice is going, reporting every anomaly, accident, etc. to Race Control.
- 03.03. It cannot act unless authorised by Race Control.
- 03.04. Once the group has received the authorisation from Race Control, and if towing is not specifically requested - in which case, only the Tow boat would intervene - the group moves towards the point requiring intervention. While the rescue boat approaches the accident location, the Tow boat, with the yellow flag raised, places itself about 100 metres from the intervention point, on the course line, in a safe and visible position, signaling the danger to the other race boats, and protecting the intervention by the rescue boat.

If it is a medical intervention, the operation is controlled by the doctor and/or paramedic on board the Rescue boat, who will take care of rescuing the patient(s) and report to Race Control regarding what further action needs to be taken.

Once the rescue has been completed, the rescue boat will return to the established mooring post following the re-entry procedure, while the Tow boat will control the accident area and clear the water of any floating debris.

At the end of its intervention, with the consequent report having been made to Race Control, the group will take up its original position for normal service.

- 03.05. During the operation, Race Control will reposition the inactive groups in order to cover the whole race course.
- 03.06. If, during an intervention, the operation commander requires the aid of other rescue boats, Race Control will send the appropriate group(s), including the fire service.
- 03.07. In the case of an inverted race boat, the ***UIM Commissioner***, via Race Control, will stop the practice, pole position and/or race. In this case, the order will be given to raise the red flag, thus initiating the Race Stopped/Pace Boat procedure.

03.08. If simultaneous incidents require the use of more than 50% of the active groups envisaged in the safety plan, it will be the decision of the **UIM Commissioner**, via Race Control, to stop the practice, pole position and/or race. In this case, the order will be given to raise the red flag, thus initiating the Race Stopped/Pace Boat procedure.

SAFETY GUIDELINES

SAFETY

The Safety and Rescue Plan includes both land and sea services.

All services will be carried out by Local Committee crews together with relevant class and Police / Military crews.

The Safety and Rescue system will be co-ordinated by the Race Control.

LAND SERVICES

The Land Safety System includes the medical, fire prevention, helicopter and hospitality area services.

The Land area is divided into four separate zones:

- Dry Pit and Wet Pit area;
- Fuelling area;
- Helicopter area;
- Hospitality area

The areas indicated above must be set up and equipped as follows:

A. DRY PIT AND WET PIT AREAS

A.01. Medical Services

The medical services will be carried out under the control of the Head of the Medical Committee, with:

- A.01.01. N.1 Ambulance, with doctor and paramedical crew, used in place of first aid and patient transport.

This ambulance will be positioned close to Dry Pit, free to move, from 08.00 to 20.00 from arrival to departure of all vehicles.

A.02. Fire Prevention Services

- A.02.01. 3 Fire points, marked with signposts, each containing 6 Extinguishers of 10 Kg, in separate locations in the pit area.
- A.02.02. N.2 fire extinguisher operators.
- A.02.03. N.2 water pumps, positioned in crane area (one at each crane).

B. FUELLING AREA**B.01. Medical Services**

- B.01.01 N.1 Ambulance, fully equipped and manned by reanimation doctor and reanimation paramedical crew.

This ambulance will be positioned close to Fuelling area, free to move, one hour before and until one hour after the official fuelling times.
- B.01.02 N.1 Fire engine with a minimum of 2 cm extinguishing capacity, fully equipped and manned by the proper crew

During fuelling operations, the area will be closed and controlled by the security crews.

C. HELICOPTER AREA (IF REQUIRED)**C.01. Medical Services**

According to the local Law.

C.02. Fire Prevention Services

According to the local Law

D. HOSPITALITY AREA**D.01. Fire Prevention Services**

- D.01.01. 2 Fire points, marked with signposts, each containing 5 Extinguishers of 10 Kg, in separate locations.

ON WATER OFFICIAL TIME LAND SERVICES

- N.2 Reanimation Ambulance (“A” type), fully equipped and manned by reanimation doctor and reanimation paramedical crew.

These ambulances will be positioned free to move one hour before and until one hour after the official race, practice and pole position times.

Note: The LOC can also use the fuel ambulance during the official race times.

SEA SERVICES

Safety and Rescue Services on Racecourse.

E. RESCUE BOAT

(Mandatory for Practice, Pole Position & Races)

- E.01. Two Rescue Boats, specification as per point 02.01, with only driver and helper, which will be located with the Class 1 Official Rescue Group.
- E.02. A minimum of n. 5 Rescue Boats, specification as per point 02.01, with crew, n. 1 reanimation doctor or specialist paramedic and n. 2 divers for each boat, fully equipped (see following details).

F. TOW BOAT

(Mandatory for Practice, Pole Position & Races)

- F.01. N.6 Boats with crew, equipped for towing in order to tow up to 5 tons (see following details).

G. FIRE BOAT

(Mandatory for Practice, Pole Position & Races)

- G.01. N.2 Boats with crew, equipped with anti fire system with foams and water pump.

H. COURSE PATROL BOATS

(Mandatory for Practice, Pole Position & Race)

- H.01. N.6 / 7 Military boats for external course patrol.

I. COURSE OBSERVERS BOAT

(Mandatory for Pole Position & Races)

- I.01. N.1 Boat (minimum 7 mt) each turn point with crew and n. 2 Course Officials.

J. OFFICIAL CLASS 1 PHOTO Boat (Mandatory for practices, pole position and races)

J.01. N. 1 Boat for Class 1 Official Photo crew during whole period, available with driver at disposal of the Class 1 (minimum 7m - high speed).

ACCESSORIES AND SPECIAL EQUIPMENT

• LOC DIVERS' EQUIPMENT

The only special request for the divers is the supply of max 2 liters cylinders.

• LOC TOW BOAT

- Class One flag;
- Nautical radio system;
- 1 Yellow flag;
- 5 lengths of rope (15m each);
- 2 Blankets;
- 2 Marker buoys.

• LOC COURSE OBSERVERS BOAT

- Class One flag;
- Nautical radio system;
- 1 Yellow flag;
- 1 Red flags;

• LOC RESCUE BOAT

CREW

- Driver;
- Assistant;
- 1 Reanimation Doctor or Specialist Paramedic;
- 2 Paramedic Scuba Divers

LOC RESCUE BOAT EQUIPMENT

- Class One flag;
- Nautical radio system;
- 1 Yellow flag;
- 1 Red flags
- Oxygen
- 2 Collars
- C35
- Several Splints
- Pair of Scissors
- Floating Stretcher

- Thermal Blanket
- Emergency Medical Bag
- SUGGESTED BAG'S CONTENTS IN DETAIL (FOLLOWING THE NATIONAL LAWS)

Venflon 14 g	Ipnovel 15mg/3ml	Effortil vials 10mg/1ml
Venflon 16 g	Cortop vials 500mg/5ml vials	Ebrantil vials 50mg/10ml vials
Venflon 18 g	Gluc 33%/10ml	Sodium bicarbonate 8.4%/100ml
Butterfly 19 g	Mgso4 vials 1g/10ml vials	Emagel 500ml/flac
Butterfly 21 g	Normal saline solution 0.9%/10ml bottle	Sodium chloride 0.9%/500ml
Arterial compressors with 19g Needles	Calcium chloride fl 1g/10ml	Corrugated tube
Silk bandage 2.5	Esmeron vials	Endotracheal tube 7.5
Syringes 5 millilitre	Silk bandage 2.5	Endotracheal tube 7
Syringes 10 millilitre	Lubricant	Endotracheal tube 6.5
Administration set with flow regulator	Laryngoscope	Endotracheal tube 6
3-way taps	Blades for laryngoscope Mac 3-4	Endotracheal tube 5.5
Mini tracheal set	Magil forceps	Disposable gloves, size M
Adult laryngeal tubes	Mayo cannulas in several measures	Adrenaline 1mg/ vials
Adult Ambu bag	Intubation stylet	Midarine (succinylcholine) vials
Adult hematometer	Atropine sulphate 0,5mg/1ml vials	
Adult ventilation mask	Xilocaina 2% 50ml flac	
Ventolin spray flc	Cordarone 150mg/10ml	
Diprivan 10mg/millilitre bottle	Revivan vials 200mg/	

SPECIAL INFORMATION TO BE PROVIDED BY THE LOC

- 1) Name of pre-warned hospital(s) with contact names of doctors and telephone numbers;
- 2) Notification of location of specialized units at hospital(s) (emergency surgery; radiology with tac; resuscitation; etc.);
- 3) Time to cover distances from the pits to the designated hospital(s) via road;
- 4) Time to cover distances from the pits to the designated hospital(s) via helicopter;
- 5) Further emergency berthing points (if any);
- 6) Possibility of security presence at such berthing points;
- 7) Possibility of resuscitation ambulance at such berthing points.

APPENDIX 2

ON-WATER PROCEDURES AND GUIDELINES

01. POLE POSITION PROCEDURE
 - 01.01. GROUPING
 - 01.02. POLE POSITION PROCEDURE
02. PIT STOP PROCEDURES
03. START PROCEDURE
04. RACE STOPPED/PACE BOAT PROCEDURE
 - 04.01. RACE STOPPED
 - 04.02. PACE BOAT PROCEDURE
05. FINISHING PROCEDURE

01. POLE POSITION PROCEDURE

01.01. GROUPING

All Race boats must be positioned on the wet pit pontoon 1 hour before the scheduled start time for Pole Position, unless otherwise stated in Race instructions or race bulletin. Boats to be positioned in Championship order. In the case of equal standing, we will follow the race number order.

01.02. POLE POSITION PROCEDURE

Ten minutes prior to start of pole position, a white flag will be shown at the wet pit area

As soon as the green flag is displayed and the course is open, race control will call in championship order, each boat to be given permission to enter the course. After this first call any race boat is entitled to request permission from Race Control on the designated VHF channel to leave the Race wet pit pontoon and enter the race course.

There is no maximum number of race boats on the course.

Each boat is entitled to run a minimum of 1 timed lap; there are not laps limits.

Completion of 1 timed lap only will qualify for Pole Position. To qualify for any Pole Position prize funding (at the discretion of the class 1 promoter) race boats must have completed a minimum of 2 timed laps.

Race boats are also entitled to use the crane for a maximum of 10 minutes during the pole position round, when available and/or possible and under the direction of the Chief Technical Commissioner.

Any boat entering or leaving the race course must request explicit permission from Race Control via radio on the designated VHF channel and follow the In & Out procedure as given in the Race Instructions and/or Bulletin and/or Pilots' Briefing.

The course will be closed at the Race Instruction specified time min 45 minutes to 90 minutes maximum, the chequered flag will be raised and all boats on the course must exit the course following the In & Out procedure.

As a consequence of the strict time limitation, all race boats must commence a timed lap before the allotted minutes have passed so as to be entitled to finish this timed lap. Each boat must then proceed to the wet pit area, respecting the In & Out procedure.

The final Pole Position results will determine the starting order for all participating boats in the

Grand Prix Race 1 only.

It is at the discretion of the UIM Commissioner to establish whether, when entering the race course, the departure point will be the Wet Pit, or whether it is necessary to designate a Pole Position Milling Area. This will be indicated in the Race Instructions and/or Bulletins and/or Pilots' Briefing. In there is a Pole position Milling Area, the race boat must declare its' intended destination (pole position race course or pole position milling area).

After the start time of the Pole Position, priority use of the cranes is at the sole discretion of the Chief Technical Commissioner. No refueling no adding or removing of weight will be allowed during the Pole Position. Repositioning of solid ballast weights is only allowed under supervision by the Technical Commissioner

Under no circumstance during the Pole Position may any boat run outside of the Pole Position course area. Failure to comply will result in a fine of Euro 1,200 and/or disqualification.

Lap timing commences after the first passing of the timing gate.

The Pole Position format on the same day of the race will be held as part of the final practice session, unless otherwise stated in the race instruction or posted in a race bulletin due to unforeseen local conditions or weather.

No more than 50 KG can be added to a boat between a Pole Position and its Race.

After Pole Position all boats must be available for scrutineering at discretion of the UIM Chief

Technical Commissioner.

The 1st, 2nd & 3rd boats of the Pole Position round must proceed directly to the Race wet pit pontoon and any Podium area after the finish of Pole Position for the Prize Giving Awards, if requested in the race instructions or by the LOC.

The result of the Pole Position will be determined by the fastest timed lap.

Boats compete in the Pole Position for Race 1 for the Pole Position Trophy. Pole Position trophies will be awarded to the 1st, 2nd and 3rd placed boats for the overall Pole Position Trophy.

No part of the Pole Position is subject to protest.

Boats disqualified from the race for technical reasons will automatically be disqualified from the event, including the Pole Position Series.

In addition to the penalties determined by the Rules, the following penalties will be applied:

- a) In the case of non-participation in the Pole Position Round for certified technical reasons, the position in the Start will be in last place (e.g. 10th of 10 boats);
- b) In the case of missing or destroying a buoy, the time from the relevant lap will not be counted towards qualification; but does count as one of the allowed number of race laps.

In the event of bad weather or curtailment

In the event that the Pole Position has run for a minimum of 50% of the published duration the results will stand, any boats that have not chosen to run in that time will be classified after the qualifying boats in their World Championship order.

If 50% of the published time session is not completed, the session will be cancelled. In this case the Races start order will follow the Championship order. In case of new Teams Entry, their race start position will be placed after all the boats in championship order and in the Race number numerical order.

02. PIT STOP PROCEDURES

- Boats which wish to make a Pit Stop must follow the In & Out procedure outlined in the Race Instructions.

- Boats returning to the wet pit pontoon must turn on their strobe light.
- If several boats are returning at the same time, use of the cranes will be designated by the Chief Technical Commissioner based on the order of the boats entering the wet pit pontoon.
- Boats must be at very slow speed at all times while in the harbour (no wake). Penalty for infringement: Disqualification and/or fine.
 - Refueling is not permitted.
- Only the official crane areas may be used during pit stops unless agreed by the Chief Technical Commissioner.
- No boats will be recovered during the duration of Pole Position, except for damaged boats and under the responsibility of the UIM Technical Commissioner.
- Cranes may be used to recover boats during the Grand Prix race 1&2 period, but priority will be given to pit stops, except for damaged boats, and under the responsibility of the Chief Technical Commissioner.
- Craning time is fixed at ten minutes per Pit Stop. A boat can enter the craning area only when it is ready to be hoisted by the crane. Craning time starts from when the boat is attached to the crane by its straps and ends when the straps are released from the crane.

See also Class 1 event Rules Section H 31Craning and 32 Pit Stop Rules.

03. START PROCEDURE

All boats will be positioned in Pole Position order by the wet pit pontoon.

15 minutes before the start, a white flag will be raised in the wet pits and the drivers will take their positions in their cockpits with their attention focused on race boat VHF channel.

10 minutes before the start, the green flag will be raised and the UIM Commissioner/ Start Marshall will call by radio the race boats in starting order; all boats will then leave the pontoon accordingly, following the Start Boat to proceed on parade to the start. All boats will follow the Start Boat at very low speed (max 10 knots) in their qualifying order. Where possible a parade lap will take place straight in to a rolling start, with no Flare.

When the Start Boat turns in the direction of the Start Line, then all race boats have to take their position at the start.

All boats will line up in their qualifying order as described. The leading boat must be adjacent and a minimum of 30 meters astern of the Start Boat. All other boats must line up alongside the leading boat in starting order and maintain a distance of approx. 30 meters between them. When lineup is complete, a yellow flag will be raised. When all the boats are in position the green flag will be raised as soon as possible in the vicinity of the start chute.

A second start boat may be used where available to form an imaginary line between the 2 start boat transoms. In this instance the 30 metre rule will apply from both start boats.

If any of the race boats are out of starting order by the Start, a penalty will be applied as describe in the Race Instructions

The start of the race is denoted when the Start Boat has raised the green flag. The green flag will be raised at approx. 45 Mph and confirmed by radio. The start boat will not advise boats to speed up or slow down, this is the sole responsibility of the pilots.

All race boats must line up and remain at a constant speed in starting order – no 'runs' through the lineup will be permitted. Penalty will be applied as describe in the Race Instructions

If a race boat starts before the yellow flag is raised, the penalty is Disqualification.

It is the Team Manager's responsibility to guarantee the good functioning of the Race Boat's radio communications.

Penalties for infraction of the Starting Procedure are:

- Interference with starting procedure Penalty will be applied as describe in the Race Instructions and if the driver had caused danger, a Yellow Card.
- Should the bow of any boat be in front of the transom of the start boat when the green flag is raised Penalty will be applied as describe in the Race Instructions
- Starting on the incorrect side of the start boat or in front of the start boat - Disqualification.
- Damaging the position of another boat - Yellow card.
- Infringement of the 30 meters rule will be penalized as describe in the Race Instructions.
- See relevant diagram issued with Race Instructions.

NB - Refer to the race course location, bad weather condition, or others, different start procedure can be applied by UIM Commissioner.

The Start can be:

- 1) standard one line;
- 2) standard one line with multi gates on the first turn mark;
- 3) multi line;
- 4) single row;

See Appendix 5 – Start procedure drawing & clarification

04. RACE STOPPED/PACE BOAT PROCEDURE APPLIES ALL CLASSES

All Races – In the event of a race boat capsizing racing will be immediately Red Flagged until the incident is under control. Pace boat procedure will then commence as per rule and/or race instruction.

04.01. RACE STOPPED

In the event that a race has been stopped with a red flag, the race time will keep on running.

If the race is stopped during the first lap or, in any event, before the first timed lap has been completed by the leading boat, the Restart will be effected in accordance with the normal Start Procedure and starting order as given in the Race Instructions and/or Bulletin and/or Drivers' Briefing but with one lap less.

If the race is stopped, after the first timed lap has been completed by the leading boat, all boats must proceed to the Pace Boat Milling Area, and further instructions will be communicated via the radio race channel or by visual signals.

In any case, whichever procedure is to be followed (normal Restart and/or Pace Boat), all boats must observe the Pace Boat Milling Area procedure as described below.

- The stopping of the race will immediately be communicated via radio (VHF channel as announced in the Race Instructions) and via timing computer.
- It is the responsibility of the Team Manager to inform his pilots that the race has been stopped.

- All boats must immediately slow down. No overtaking will be permitted from the stopping of the race/red flag.
- Slowly, each boat must follow the race course, passing in the finish/timing gate, to the designated Pace Boat Milling Area;
- As soon as all race boats are assembled in the Pace Boat Milling Area they must follow the instructions from the Pace Boat:
 - If the Pace Boat shows the red flag, the race is definitively stopped and all boats must follow the Pace Boat into the wet pits.
 - If the Pace Boat raised the yellow flag and/or turns on the orange strobe lights, the Pace Boat procedure comes into force.

Refer also to Event Rules 35 Finishing Procedure 01, 02, 03.

04.02. PACE BOAT PROCEDURE

The pace boat and the location of the Pace Boat Milling Area will be announced in the Race

Instructions and/or Pilots' Briefing and/or Bulletin.

- Once all race boats have reached the Pace Boat Milling Area and are assembled, and the Pace Boat raised the yellow flag and/or turns on the orange strobe lights, the race boats must line up behind the pace boat in the order of the classification of the lap prior to the stopping of the race. From this moment on, each single overtaking will be penalized as describe in the Race Instructions per boat overtaken. (3 boats overtaken under Red Flag = 3 penalties). All Long/Short Lap made during a red flagged lap will not be counted.
- Race Control will announce the line-up via radio and timing computer.
- As soon as all race boats have taken their correct position behind the pace boat, they must closely follow the course of the pace boat.
- From the moment that the race course is cleared, the pace boat will follow the regular race course (race lap) as announced in the Race Instructions and/or Pilots' Briefing and/or Bulletin.
- The race laps runs under the Pace Boat procedure will be counted.
- It is the responsibility of the pace boat/Race Control to restart the race. The pace boat will accelerate to a high speed. The race boats must continue to follow the pace boat in the designated order.

- The restart of the race will be indicated by a green flag and communication via radio and timing computer.
- The race will be considered restarted either as soon as the pace boat, at any time, when the Green flag is raised and called on the Race boat channel.

The green flag will be displayed and the Team Managers will be informed via radio and timing computer.

Overtaking is only permitted, after the green flag is shown and after race boats have passed the finish/timing gate in the race order.

In the case of a boat having technical or other problems during the restart procedure, the strobe light must be switched on immediately and all following boats are entitled to pass this boat without penalization. If the strobe lights are not switched on immediately and the restart procedure is affected, the concerning boat will be as describe in the Race Instructions

If the boat is not able to perform this penalty during the actual race, the penalty will be valid for the next race in which the boat takes part.

05. FINISHING PROCEDURE

- 05.01. No Patrol Boat or Jury Boat shall be released from his station until the official in charge at Race Control has given clearance to that specific boat.
- 05.02. Once all the race boats have passed the Finish/Timing line, they must proceed to a safe area outside the race course as defined in the Race Instructions, where the Pace Boat will collect them.
- 05.03. Once all the boats have been collected by the Pace Boat, they must follow the procedure to return to the wet pits, following the In & Out Procedure given in the Race Instructions and/or Bulletins and/or Drivers' Briefing.
- 05.04. The Winner, 2nd- and 3rd-placed boats will be led to the Wet Pits or other designated area as announced in the Race Instructions by the Start/Pace Boat.
- 05.05. If the first three boats are not docking in the dry pit after the complete boat seal by the Technical Commissioner, they will be towed to the dry pit and under the crane by the organization.

APPENDIX 3

ON-LAND PROCEDURES AND GUIDELINES

01. PITS
 - 01.01. DRY PIT
 - 01.02. CRANING AREA
 - 01.03. REFUELING AREA
 - 01.04. WET PIT AREA
 - 01.05. HELIPAD
 - 01.06. RACE CONTROL/TIMING/TEAM MANAGERS' AREA
02. HOSPITALITY AND VIP AREA
 - 02.01. TEAMS' HOSPITALITY
 - 02.02. PODIUM AREA
03. OFFICES
 - 03.01. PROMOTER OFFICES
 - 03.02. OFFICIALS
 - 03.03. PRESS OFFICES
 - 03.03.01. PROMOTER Press - PR
 - 03.03.02. LOC Press Office
 - 03.04. CLASS 1 TV
 - 03.05. MEETING ROOM
 - 03.06. PILOTS' BRIEFING ROOM

01. PITS

The PIT area comprises the areas on land and the quayside where the activities connected to the event take place.

The Pit area is made up of: Dry Pit; Craning Area; Refuelling Area; Wet Pit; Helipad; Timing area;

01.01. DRY PIT

The Dry Pit is the area where the race boats and workshops are situated.

The usable area required is approx. 6,000 m² and it is requested that this be available from 00.00 on the day 5 days before the start of the race weekend until 24.00 on the day after the last Grand Prix.

It is compulsory that this area be closed off and the entrances protected 24 hours a day by security guards (the type of pass which allows access will be advised by the Promoter via panels displayed in the area).

The area must have sufficient space for the movement of trucks to and from the craning area (point 1.2) and the refueling area (point 1.3)

The Dry Pit must be equipped with the following:

- a. 8 multiple (minimum dual) 220/380V electricity connections with a maximum charge of 200 kW;
- b. 8 multiple (minimum dual) water supply points;
- c. minimum of 4 x 200 litre containers for collecting oil;
- d. 1 rubbish bin for each race boat (minimum of 10), 200 litres in size;
- e. 3 Fire points, marked with signposts, each containing 6 Extinguishers of 10 Kg, in separate locations in the pit area and a minimum of 2 fire prevention officers;
- f. 1 forklift truck, minimum 3 tons (no operator);
- g. 1 van for transporting materials (no driver);
- h. adequate electric lighting;
- i. adequate toilet facilities.

01.02. CRANING AREA

The craning area is where the cranes for launching and haulage operations are situated. The usable area required is c.1,000m² and it is requested that this be available from 00.00 on the day 2 days before the start of the race weekend until 24.00 on the day after the Grand Prix. It is compulsory that the entrances to this closed off area be protected 24 hours a day by security guards (the type of pass which allows access will be advised by Event Promoter/Organizer via panels displayed in the area).

The area must have sufficient space for the movement of trucks to and from the Dry Pit (point 1.1) and the refueling area (point 1.3).

The Craning Area must be equipped as follows:

- a. 2 x minimum 70 ton cranes with 20-metre range available for use and in position from 08.00 of the day before the race weekend until 24.00 of the day of the final GP, with operators present during the official hours. The operators must also be on call for other activities, if necessary, at other times.
- b. 2 multiple (minimum dual) 220/380V electricity connections
- c. 2 water pumps, one by each crane, to be used to pump out race boats in case of accident

- d. a minimum of one 200 liters rubbish bin;
- e. 2 fire extinguishers for each crane;

01.03. REFUELING AREA

The refueling area is where the race boats fill up with petrol.

The usable area required is a minimum of c.500 m² and it is requested that this be available from 00.00 on the first day of the race weekend until 24.00 on the second day of the race weekend. It is compulsory that this area be closed off and the entrances protected 24 hours a day by security guards (the type of pass which allows access will be advised by Promoter/Organizer via panels displayed in the area).

The area must have sufficient space for the movement of trucks to and from the Dry Pit (point 1.1) and the craning area (point 1.2).

The Refueling Area must be equipped as follows:

- a. N.1 Fire engine with a minimum of 2 cm extinguishing capacity for combustibles, fully equipped and manned by the proper crew. The fire prevention group must also be on call outside the official time;
- b. 1 ambulance with medical staff and paramedic in attendance during all official refueling times;
- c. 1 multiple (minimum dual) 220/380V electricity connection;
- d. a minimum of one 200 litre container for collecting oil/fuel;
- e. 4 x 30 Kg fire extinguishers;
- f. sufficient antipollution material.

01.04. WET PIT AREA

The Wet Pit is the area for the launching and haulage of the boats.

The usable area must be adjacent to the craning area and have sufficient space for the positioning of c.200 linear meters of pontoons.

It is compulsory that the entrances to this closed off area be protected 24 hours a day by security guards (the type of official pass which allows access will be advised by Promoter/Organizer via panels displayed in the area).

The area must be equipped with gangplanks, ladders, etc. which enable the authorised personnel to reach the pontoons.

01.05. HELIPAD

The helipad is the area from where the helicopters used for the race take off, land and park.

The entrance to this closed off area must be protected by security.

The usable area must be suitable for the use by 3 helicopters (e.g. Ecureil AS355 or similar).

As an alternative to a permanent helipad, it is possible for the Local Organizer to provide an area which can be used for this purpose on a temporary basis and request the “OPENING OF TEMPORARY HELIPAD” authorization from the relevant Civil Aviation Authority.

In this case, the Local Organiser must arrange for the temporary area to be set up in the following way or, in any event, in accordance with all legal requirements:

- a. 1 fire engine equipped for combustibles and with staff on call during the official times for helicopter use (the service must start 45 minutes before the official period and last until 45 minutes afterwards). The fire prevention group must also be on call for other activities, if necessary, at other times.
- b. 1 ambulance with medical staff and paramedic in attendance during all official refueling times (the service must start 45 minutes before the official period and last until 45 minutes afterwards).
- c. Water tank truck if surface is sandy.

01.06. RACE CONTROL/TIMING/TEAM MANAGERS' AREA

The Race Control/Timing/Team Managers' area is where the timing of the event is done and from where the participating Teams are controlled. It is situated close to the Finish Line.

The usable area required is a minimum of c. 50 m x 4 m and it is requested that this area be available from 00.00 on the day 3 days before the start of the race weekend until 24.00 on the day of the final Grand Prix.

It is compulsory that this area be closed off and the entrances protected 24 hours a day by security guards (the type of pass which allows access will be advised by Promoter/Organiser via panels displayed in the area).

The Race Control/Timing/Team Managers' Area must be equipped as follows:

- a. 220V electricity connection min 10 Kw;
- b. 3 rubbish bins;

- c. 2 fire extinguishers;
- d. fast ADSL Broadband internet connections, minimum 4 Mbit UPLOAD (either via cable or wireless);

02. HOSPITALITY AND VIP AREA

The Hospitality Area is the group of areas on land where the Teams' and LOC/Event Sponsors' hospitality activities take place.

The Hospitality Area made up of: Teams' Hospitalities; Hospitality for Local Sponsors (if requested locally); Local Organizer's Space (if requested locally); VIP Village (if requested locally); Commercial area (if requested locally); Podium Area.

02.01. TEAMS' HOSPITALITY

The Teams' Hospitality area is where the Teams' motorhomes and that of the Promoter are situated (including the Class 1 VIP area).

The usable area required is c.3,000m² and it is requested that this area be available from 00.00 on the day 3 days before the start of the race weekend until 24.00 on the day after the Grand Prix.

It is compulsory that the entrances to this closed area be protected 24 hours a day by security guards (the type of pass which allows access will be advised by Promoter/Organiser via panels displayed in the area).

The Teams' Hospitality area must be equipped as follows:

- a. 6 multiple (minimum dual) 220/380V electricity connections with a maximum charge of 200 kW;
- b. 6 multiple (minimum dual) water supply points;
- c. 200 litres rubbish bin for each motorhome (minimum of 10);
- d. 2 Fire points, marked with signposts, each containing 5 Extinguishers of 10 Kg, in separate locations.
- e. adequate toilet facilities;
- f. adequate cleaning of the area on daily basis and removal of rubbish;
- g. adequate drainage (white water) system

02.02. PODIUM AREA

The Podium Area is where the prize-giving takes place.

This is normally situated inside the VIP Hospitality area and where VIP activities are run.

The usable area required is c.100 m² and it is requested that this be available from 00.00 on the day before the start of the race weekend until the end of the prize-giving ceremony which follows the Grand Prix.

This area must be protected by security guards for the duration of the prize-giving ceremony (the type of pass which allows access will be advised by the Promoter via panels displayed in the area).

The Podium area must be equipped as follows:

- a. 1 multiple (minimum dual) 220V electricity connection
- b. 1 stage, minimum 10m wide, 6m deep, on which backdrop is to be set up (provided by Promoter/Organiser) steps up to stage
- c. 1 PA system with radio-microphones (2)
- d. Table to display trophies

03. OFFICES

The Offices are a collection of areas requested for the execution of the technical and administrative work relating to the event.

They comprise: Promoter Operations Office; Officials; Press/PR; Class 1 TV; Press Office; Meeting Room; Briefing Room;

Overall, they can be made up of offices and/or portacabins/stretched flexible structures set up for the purpose.

The Offices are normally located near the Pit Area.

They should be cleaned, with waste paper baskets to be emptied, on a daily basis.

03.01. PROMOTER OFFICES

Made up of 1/2 rooms totaling approx. 30m²:

- a minimum of 5 workstations, made up of desks and chairs with sufficient electrical sockets and internet connections (either via cable or wireless); also area to be used for private meetings.

03.02. UIM OFFICIALS

An office with a minimum of 8 workstations, made up of desks and chairs with sufficient electrical sockets and internet connections (either via cable or wireless); one colour photocopier/printer with sorter;

03.03. PRESS OFFICES

The Press Offices are divided between International (Promoter) and Local (LOC).

The Promoter Press/PR- and the local Press Office must be one large room, separated by a room divider.

03.03.01. Promoter Press – PR

An office c.30 m², to seat a minimum of 5 people, with workstations made up of desks and chairs with sufficient electrical sockets and fast ADSL Broadband internet connections, minimum 4 Mbit UPLOAD (either via cable or wireless);

03.03.02. LOC Press Office

An office c.100 m² (size, tables, chairs, etc. depend upon number of journalists expected) equipped with one fax, ADSL Broadband internet connections, colour photocopier,

03.04. CLASS 1 TV

A closed off office, minimum 40m², equipped with a minimum of 8 workstations, sufficient electrical sockets, internet connections, minimum 8 Mbit UPLOAD (either via cable or wireless);

03.05. MEETING ROOM

A room which can seat up to 30 people for private Class 1/Promoetr meetings, furnished with tables and chairs.

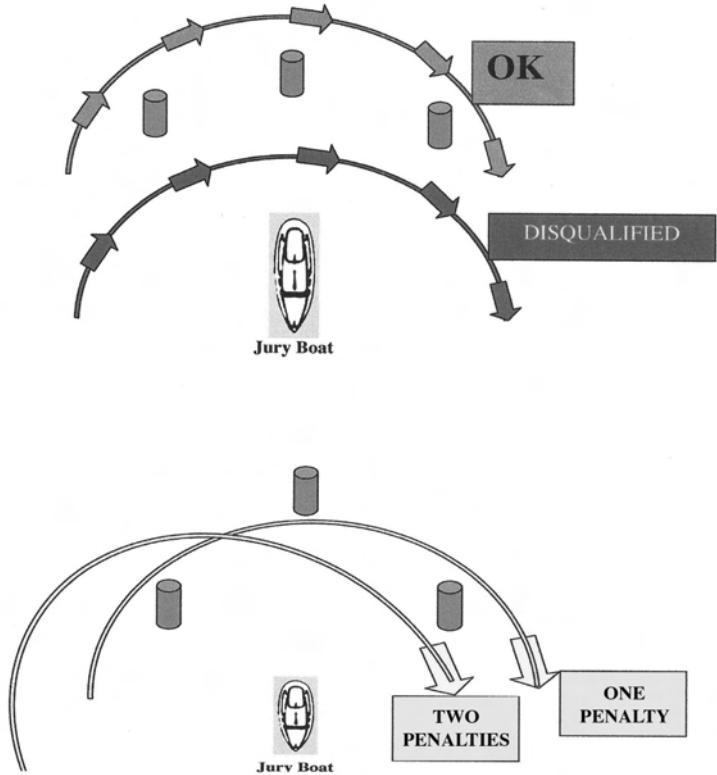
03.06. DRIVERS' BRIEFING ROOM

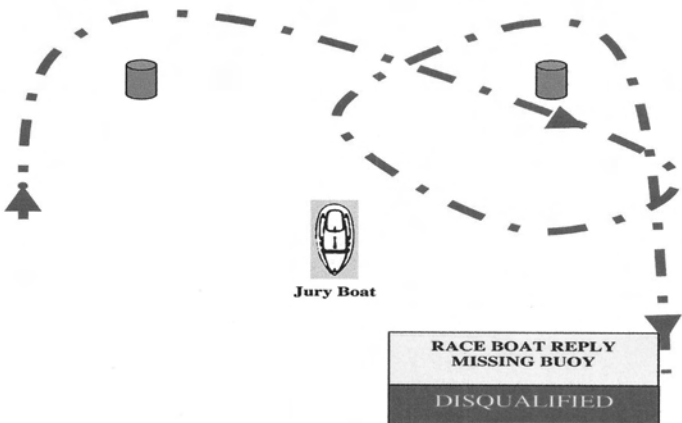
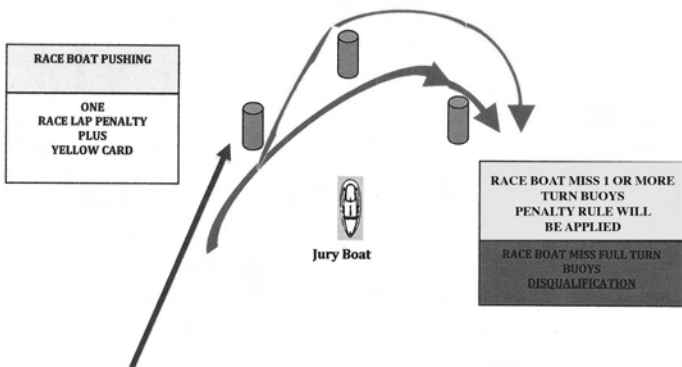
A well-presented room which can seat around 50 people for the Pilots' Briefing. It is set up with a top table (for a minimum of 8 people) and seating in theatre-style.

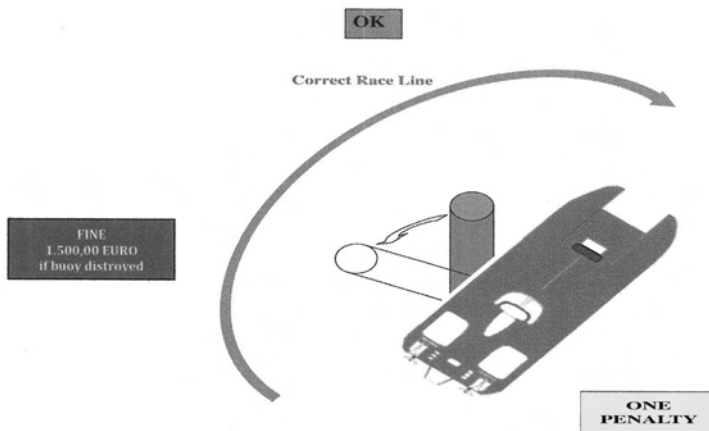
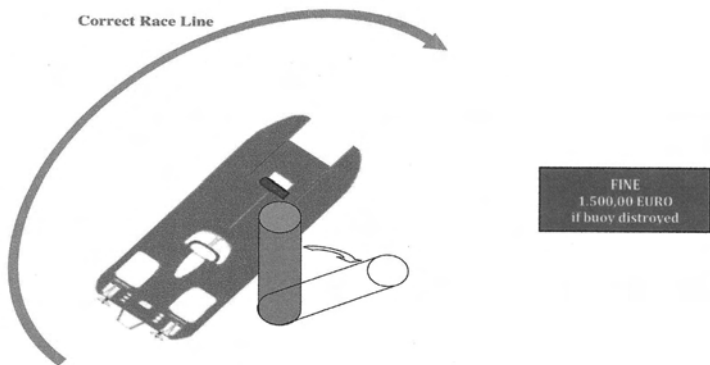
For use for two hours on the first day of the race weekend, normally 18.00 - 20.00.

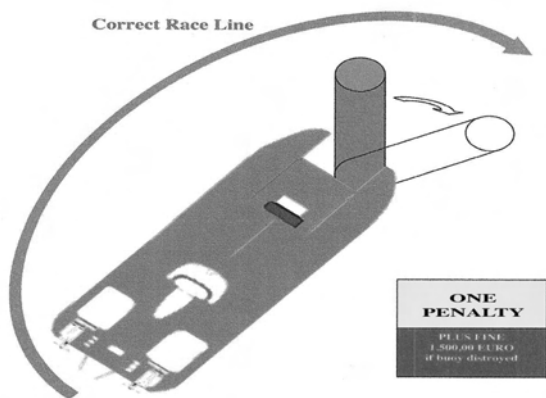
Equipped with PA system with radio-microphones (2), overhead projector and PC projector with projection screen.

APPENDIX 4 TURN BUOYS CLARIFICATION DRAWINGS



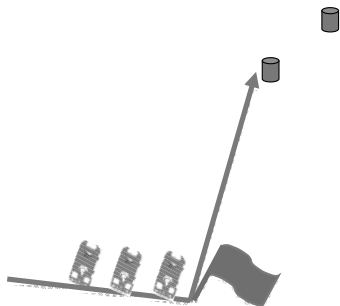






APPENDIX 5

START PROCEDURE DRAWING & CLARIFICATIONS



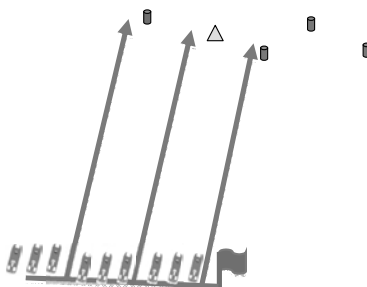
Standard Start Procedure

ALL
Race
Overtaking
Rounding marks
Rules will apply

Standard Start Procedure
On line with multi gates on the first
turn mark

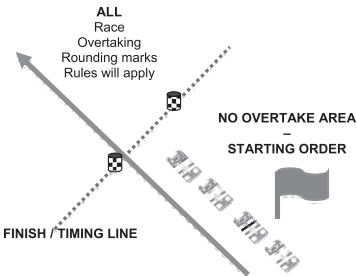
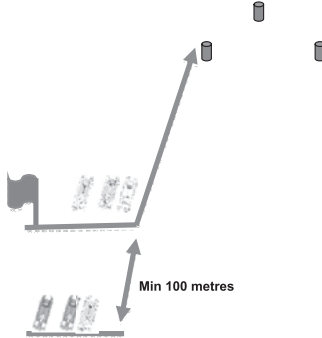
**ALL RACE BOAT MUST MAINTAIN
THEIR REFERENCE BUOY TURN
MARK**

ALL
Race
Overtaking
Rounding marks
Rules will apply inside each "gate
channel"



Start Multi Line Procedure

ALL
Race
Overtaking
Rounding marks
Rules will apply



Single Row Start Procedure (Valid also for Pace Boat re start procedure)

**ALL RACE BOAT MUST MANTAIN THEIR
REFERENCE BUOY TURN MARK**

Procedure

In the case of single row start, the race boats must be in line respect their starting order.

In the vicinity of the Finish/Timing line, the Start Boat will raise the Green Flag and will exit the race course.

At the green flag, all race boat must maintain their starting order till the Finish/Timing line (no overtake), as soon they have passed the Finish/Timing line, all race boat are under **ALL** Race - Overtaking - Rounding marks Rules.

APPENDIX 6

TECHNICAL CLARIFICATIONS

01. TORQUE SENSOR RACE WEEK END PROTOCOL

The Torque Sensor (**TS**) systems are owned by “Class 1 Promotion mfze”, and are leased to the teams participating in the UIM C1 World Championship, on a per Race weekend basis. They will remain under the jurisdiction of the UIM Technical Scrutineer. In principle one TS will be installed per boat. However, during the racing season and at their discretion, the Technical Inspector may require individual boats to run with a sensor on the second engine (see Point 6). The operating protocol for the sensors’ use during the race weekend is as follows:

1. The system components to be install on racing boat are as follows:
 - a. ABB Torque sensor, to be installed in the drive shaft line between the engine/s and the transom/s
 - b. ABB electronic interface unit/s for one or two ABB torque sensors
 - c. Race Technology Data Logger system and junction box for the two ABB torque sensors
 - d. Connecting Cables: Torque sensor-ABB interface box-Junction box-Data logger for one or two ABB torque sensors
2. During the TS distribution procedure time, defined on Advance Program the team representative will pick up the same system sets used in the previous races, as detailed above in Point’s 1.a.-1.b.-1.c., and will receive direction on which side it is to be installed. The cabling detailed in Point 1.d, will remain installed on each boat during the entire race season. However, the cabling will be subject to frequent inspection by the Technical Scrutineer for connectivity, condition and/or damage.
3. After the last race of each weekend the complete torque sensor and logger system as detailed in Point 1 a. through c., must be return to the Technical Scrutineer. All logistics related to the system will be announced in the Race Instruction documents.
4. Team Responsibility: All system components listed in Point 1, are the Team’s sole responsibility. The Team must pay for any damage occurring during the Race weekend (or for Point 1.d. during season), even if said damage is discover after removal from the boat.

5. Please note that it is at the sole discretion of the Technical Scrutineer to require a Team - at any time during a race weekend - to install a second ABB torque sensor supplied by C1P.
6. Teams are requested to install one torque sensor during race weekend. The team cannot install a second torque sensor device of any kind on the second engine during any part of the race weekend.
7. The use of any connecting cable or wiring loom as specified on 1.d. above is prohibited at any time other than the race weekend.

We would like to remind Teams that the application of the torque sensor, as requested by the UIM rules, is to measure and log **engine output power**. The power losses from any additional device/s such as a geared drop box, hydraulic oil or water pump/s, electrical generator/s, etc., that are not part of the engine's original support system, and which are installed between engine and torque sensor, will be added to the power measured by the torque sensor during the race to give a **total engine power output** that must, at all times during a race event, remain below the mandated power limit. The Race Technology Data Logger, independent of the engines' control system, will also record engine RPM, boost pressure and the depression (pressure drop) inside the air box as was done in previous years. Additional measurements may be required by the UIM/OPC, during a season, to be announced in due time.

UIM/OPC will determine, and issue a list, of the exact power loss for each additional device. The power losses listed at this time are conservative, and do not account for all the losses in the systems, therefore, a series of dyno tests will be performed to add the missing values and a new list will be issued upon completion. No appeals regarding the UIM/OPC's power determinations will be accepted.

Preliminary Power Loss List

- | | | |
|----|---|--------|
| 1) | Engine mounted drop box with three internal gears | +25 hp |
| 2) | Engine mounted drop box with two internal gears | +17 hp |

02. REQUEST FOR ENGINE APPRAISAL FOR CLASS 1 COMPETITION

The following information is requested for UIM/OPC appraisal and eventual homologation of candidate engines for use in UIM Class 1 Offshore racing

Manufacturer: _____

Addresses/Contacts: _____

Engine Model Proposed: _____

Is this model naturally aspirated or supercharged: _____

Manufacturer of Origin For Cylinder Heads (+part number): _____

Manufacturer of Origin For Cylinder Block (+ part number): _____

Number of Cylinders: _____

Number of Valves per Cylinder: _____

Maximum (total) Engine Displacement: _____

Stroke and Bore dimensions: _____ X _____

Compression Ratio: _____

Number of Fuel Injectors: _____

Any and All Components Made of Exotic Materials: _____

ECU's Manufacturer: _____

The ECU's must have single map built in only; please attach a signed confirmation declaration from the ECU's Manufacturer.

ALSO:

- 1) Please attach a dynamometer report, corrected to ISO 3046-1, for this engine showing at least the RPM, Torque and Horsepower at 250 RPM intervals through the operating range.
- 2) Please attach a signed declaration that at least 30 of these engines will be manufactured; the date at which that will be accomplished; that they will be available to competing race teams or the general boating public; and the estimated price in Euros.

Please submit this form for evaluation to the UIM OPC at the UIM offices by FAX or email. The addresses are:

Fax n° : + 377 92 05 04 60

Email : uim@uimpowerboating.com

Upon review and acceptance of this appraisal for Class 1 by the OPC, the manufacturer will be requested to file a completed Class 1 engine homologation form.

03. CLASS 1 OFFSHORE HOMOLOGATION FILE**CLASS 1 OFFSHORE
HOMOLOGATION FILE**

Class 1 Offshore Homologation File Number:

Homologation Valid from:

Manufacturer:

Engine Model:

Certified by the National Authority of:

At the date:

UIM Homologation Inspector:

UIM Certification Approval:

Running Production Changes

Change Detail Page No. Date Approved for Use Approved by

CLASS 1 OFFSHORE**HOMOLOGATION FILE**

Photo of the engine, 45° from the front at the starboard side.

Photo of the engine, 45° from the rear at the port side.

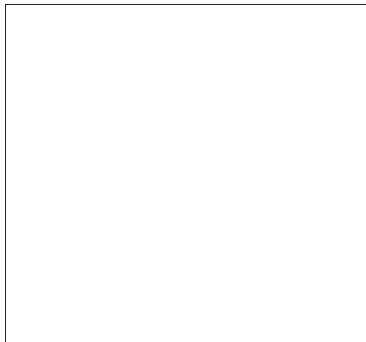
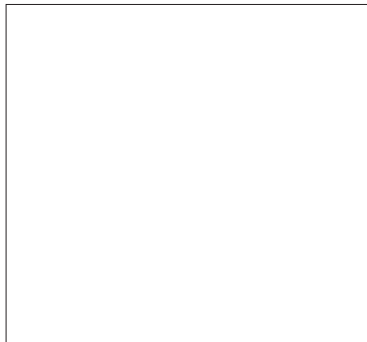
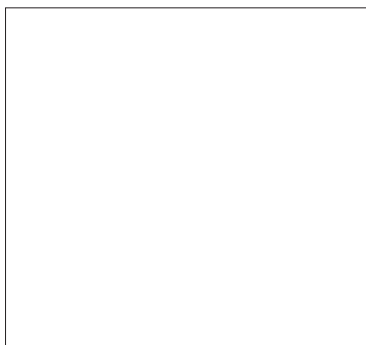
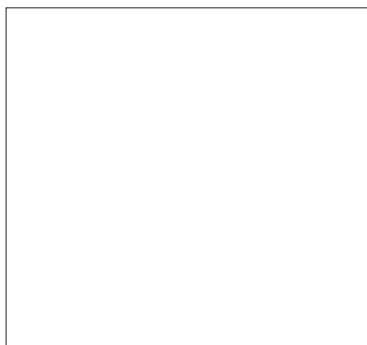


Photo of the cylinder block 45° from the front at the starboard side.

Photo of the cylinder block 45° from the rear at the port side.



CLASS 1 OFFSHORE
HOMOLOGATION FILE

Photo of the cylinder head from
the combustion chamber side

Photo of the cylinder head from
the valve assembly side.

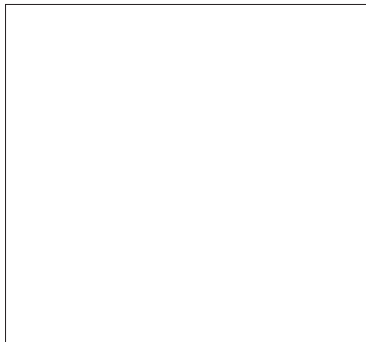
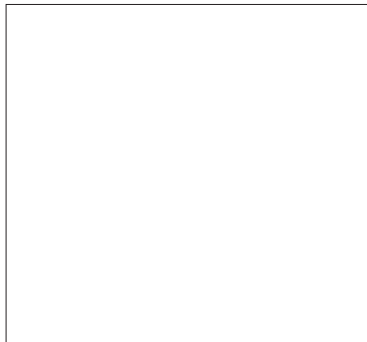
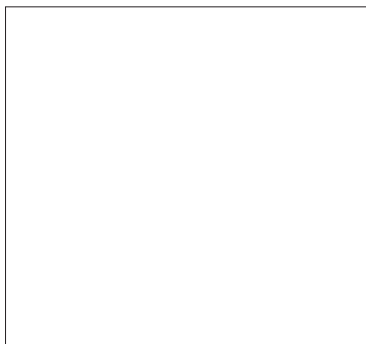
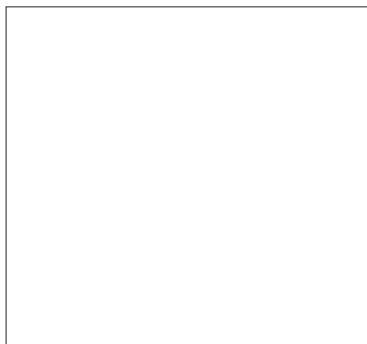


Photo of the flywheel.

Photo of the connecting rod.



**CLASS 1 OFFSHORE
HOMOLOGATION FILE**

Photo of the piston, viewed 45°
from the wrist pin.

Photo of the crankshaft.

Dynamometer results (corrected to
ISO 3046-1) of RPM; Torque; &
Horsepower every 250 RPM in the
operating range.

← Continued, if needed

**CLASS 1 OFFSHORE
HOMOLOGATION FILE**

ENGINE TYPE

Number of cylinders: _____

Cylinder arrangement and angle: _____

Naturally Aspirated or Super Charged: _____

ENGINE BLOCK

Origin of Cylinder Block (manufacturer and part number): _____

	<u>Tolerance</u>	<u>Measurement</u>
Bore	+/-	mm
Stroke	+/-	mm
Capacity per cylinder	...max...	cc
Total Capacity	...max...	cc

Cylinder block material _____

Cylinder liner material _____

CYLINDER HEAD

Origin of Cylinder head (manufacturer and part number): _____

Cylinder head material _____

Compression ratio ...max... _____

PARTS OF EXOTIC MATERIALS

List all parts made of materials other than steel or aluminum:

CLASS 1 OFFSHORE
HOMOLOGATION FILE

ENGINE MANAGEMENT

Origin of PCM (manufacturer) _____

Does the Engine Management Correct Power for Atmospheric Conditions _____

WEIGHTS

Piston and Rings	min	gm
Piston Pin	min	gm
Connecting Rod (with bearings)	min	gm
Crankshaft	min	gm
Flywheel Assembly	min	gm
Damper	min	gm
Other Rotating Attachments	min	gm

NOTES:

{Please attach a signed declaration that the engine will be available for any kind of use, to any entity, including competing race teams and the letter with the commercial price and delivery time as agreed with the UIM/OPC.}

04. NOT-TO-EXCEED TORQUE CURVE

The included chart shows the power and torque data for the Mercury 850 SCi Race engine that is the standard for competition in the Class 1 rules. These will be the not-to-exceed data during UIM Class 1 Championship Races (including Pole Position).

During each event, C1P will supply at least one torque sensor to each boat. The data recorded during the weekend will be compared to this chart.

For both engines in each boat, boost pressure for supercharged engines and air box pressure (depression) for naturally aspirated engines will be recorded. The UIM Class 1 Technical Commissioner may compare the pressure data between the two engines in a single boat. This may be used to confirm that the engine that does not have a torque sensor installed is running at approximately the same power as the measured engine.

NOT-TO-EXCEED TORQUE CURVE (CORRECTED)

MERCURY SUPERCHARGED 850

RPM	TORQUE (CT)	HORSEPOWER (CBHP)
2500	763.5	363.4
2750	789.0	413.1
3000	804.9	459.8
3250	850.0	526.0
3500	900.9	600.4
3750	917.0	654.8
4000	915.0	696.9
4250	874.1	707.3
4500	870.1	745.5
4750	870.5	787.3
5000	865.3	824.5
5250	850.9	850.9
5500	819.2	857.9
5750	786.0	860.5
6000	753.6	860.9
6083	742.7	861.0
6250	723.5 & SOFT CUT	861.0
6350	HARD CUT ENABLED	

05. PROTOCOL TO IMPLEMENT RULES, AND BOAT MAIN EQUIPMENT DATE BASE FOR TECHNICAL INSPECTION

PREAMBLE :

SECTION I - BOAT RULES 44-HULL DIMENSION, MINIMUM WEIGHTS AND EQUIPMENT pt.1

SECTION J - TECHNICAL RULES REQUIREMENTS 53. CREW CONTROL AND SYSTEMS –

Technical restrictions for all boats.

To define “Commercially Available”

The following are considered to be commercially available:

Any equipment for Marine applications such as:

- Engines package, drop box, drive line, trim equipment, rudder system (not rudder design), steering system, propeller, instrumentation, any accessory like bolt, nuts, backup plate and fitting, escape access, sea strainer, water pickup, potentiometer, relays, pumps (fuel, hydraulic, and water), high pressure line, fuel line.

These rules are to be followed before C1 boats/teams may use new or modified Machinery, Components or Systems (M/C/S). The following protocol is to help in order to avoid any misunderstandings that can result in a waste of time and investment by the teams and unnecessary clarifications/rulings at a race site by UIM officials.

01.01. PROTOCOL HOW TO PROCEED FOR RULES IMPLEMENTATION

Teams before starting to manufacture any M/C/S are to send To UIM/OPC a letter requiring “approval in principle” to install the specific M/C/S (one request per item) including the following documentation:

- a. A schematic drawing showing clearly the concept, operation and design of the device and the goal of its intended use.
- b. Specifications of all material which will be used in construction.
 - i. Standard material

- ii. Exotic material
 - c. Expected time frame for implementation.
 - d. Expected sale price “quantity related” to third parties.
 - e. Expected delivery time from order placement for third party sales.
 - f. Price for similar/equivalent M/C/S available on market.
- 01.02 After OPC investigation of the M/C/S, the OPC will take one of the following actions:
- a. If it is “in principle approved” the UIM/OPC will make its decision official to all teams.
 - b. If it is partially rejected, the OPC will require further data and evaluate that submission.
 - c. If it is rejected – no further action.
- 01.03 After OPC’s “in principle approval” the team may:
- a. Complete the activity to make the M/C/S prototype.
 - b. Do all non-race-site testing until satisfied that the item is ready for implementation in racing.
- 01.04 Following successful prototype testing, the team must homologate the new M/C/S with the UIM/OPC three (3) months prior to the first intended use at a race venue. The following documents will be required for homologation:
- a. Drawings and details of components.
 - b. Final price for other teams.
 - c. Delivery time to other teams.
 - d. Inspection venue for the UIM Technical Official, at the team’s expense, for final inspection.
- 01.05 The Homologation procedure must be completed positively before the M/C/S may be used at any race.

02.01. BOAT MAIN EQUIPMENT DATA BASE

The C1 Technical rules stress that the installed equipment is to be available commercially and there are also technical restrictions/controls to be inspected for checking the compliance to the rule synthesized on the above PREAMBLE.

Moreover, as per rule SECTION J 53. CREW CONTROL AND SYSTEM, UIM must guarantee to the series that the competitor understands and follows the rules. These are the main reasons to create a data base for each racing boat, compiled by the teams, to be ready and update at the start of the race season. The files will be strictly confidential and only UIM Technical Commissioners will have access. For the attachments, the following list will guide the team in compilation of the electronic file to be sent to the UIM Headquarters:

02.02. LISTS THE EQUIPMENT INVOLVED IN THE DATA BASE

C1 Racing Boat Main Equipment Data Base Requirements			DOCUMENTATION REQUIRED		
Reference Number	EQUIPMENT	RULE REFERENCE	Drawing or Comm. Reference	Picture or Drawing	Description: how system operates
			As per homologation procedure		
1	Engine	SECTION J 45.			
2	Crash Box		YES	YES	NO
3	Drop		YES	YES	YES
4	Cardan shaft		YES	YES	NO
5	Rudder/Skeg		YES	YES	NO
6	Steering System		YES	YES	YES
7	Trim pumps and piston		YES	YES	YES
8	Surface Drive/Stern Drive		YES	YES	YES
9	Ballast System	SECTION I 44 point 10	YES	YES	YES
10	Cockpit Hardware		YES*	YES**	YES
11	Telemetry Hardware		YES	YES	YES
13	Potentiometers		YES	NO	YES
14	Fuel System		YES	YES	YES

YES* All switches, displays, hardware commands must be identified, and their function described
 YES** A panoramic picture is required that includes all YES* marked items.

02.03. BOAT MAIN EQUIPMENT DATA BASE INSTRUCTION FOR COMPILATION

Each of the equipment listed on 02.02 are shown on 02.04 with at least one empty box on the left where a picture is to be inserted, and a box on the right where all the required information is to be compiled.

Some of the empty boxes on the left do not require a picture but instead, the functional scheme, sketch, and/or drawing as per the written instruction in the box. The space available may not be sufficient, if so, please create a separate file with the equipment name to be attached to the 02.04 file.

If the right space for information, especially for custom equipment, is not sufficient, again create a separate file with the equipment name.

At the end of the file note some pages available for future UIM requests and/or for your modifications to update the data base of your boat. Please, for modifications do not forget to follow the procedure in Appendix 6.05.01.01 if required.

The Cockpit sector is quite large. It is extremely important that all controls you have on board your racing boat BE PICTURED AND PROPERLY DESCRIBED, and updated for any change, before you show the boat at the next race. Please, if you need more space you can copy the template and add to the file. For the cockpit we require information related to the safety of the escape hatch access and of the canopy structure.

Once you have completed the file on 02.04.with all attachments send all via e-mail to UIM Headquarters only requiring reply of delivery.

02.04. RACE BOAT FILE

File to be completed by the teams, inserting pictures saved in Word where required, and inserting in a separate file the drawings and functional schemes, plus more space for the team to update the file during the life of the boat, as per instruction on 02.03.

The Technical Inspector at each race venue will define how many boats he intends to check according to these criteria:

- Two thirds boats selected of the first three boats in overall C1 Championship points.
- One-third of boats from the remaining fleet.

Any discrepancy from the original data base file found during the boat inspection will open an investigation file to determine if a penalty is to be assessed.



CLASS 1 OFFSHORE

Boat Main Equipment Data Base

Before the start of the 2012 C1 race season

Team:

Racing Number:

Boat “Measurement Certificate” copy to be attached here

Picture

Crash Box

Manufacturer :

Part Number:

Picture

Cardan shaft

Manufacturer :

Part Number:

Picture

Commercial Drop Box

Manufacturer :

Part Number:

Custom Drop Box

Designed by:.....

Picture

Rudder/Skeg

Manufacturer :

Part Number:

General Information

- Alloy specification
- Working Hours through 2011 (to be updated annually)

Picture

Steering System

Manufacturer :

Part Number:

Additional Information

- Hydraulic only.....
- Hydraulic plus electric.....

Picture

Trim pumps and pistons

Manufacturer :

Part Number:

Picture

Surface Drive/Stern Drive

Manufacturer :

Part Number:

Custom Package Information

Dimension drawing of tanks or other system, functionality scheme of transfer weight

Ballast System

Manufacturer :

Part Number:

Documentation as per C1 rules: 44 point 10:

44 point10. It is mandatory that fully dimensioned drawings of all liquid containers and/or tanks are given by each team before the first race to the OPC Technical Inspector. This drawing shall include measurements from the nearest reference point e.g. a bulkhead to show its location. Methods of filling and emptying must also be shown and the tanks must be able to be checked if empty or not. All onboard liquid ballasting must be coloured with a permanent dye.

Picture of at least two installations, if any

Potentiometer

List all potentiometers installed with their use described

1. Manufacturer :
- a. Part Number.....
- b. Description/use.....
.....

2. Manufacturer :
- a. Part Number.....
- b. Description/use.....
.....

3. Manufacturer :
- a. Part Number.....
- b. Description/use.....
.....

4. Manufacturer :
- a. Part Number.....
- b. Description/use.....
.....

Please continue on next page

Picture of at least two installations, if any

Potentiometer

List all potentiometers installed with their use described

5. Manufacturer :
- a. Part Number.....
- b. Description/use.....
.....

6. Manufacturer :
- a. Part Number.....
- b. Description/use
.....

7. Manufacturer :
- a. Part Number.....
- b. Description/use
.....

8. Manufacturer :
- a. Part Number.....
- b. Description/use
.....

Please continue on separate page if needed

Cockpit View No. 1

Picture



Cockpit View No. 2

Picture



Cockpit View No. 3

Picture



Cockpit View No. 4

Picture



Picture

Cockpit

Manufacturer :

Part Number:

Picture

Cockpit

Manufacturer :

Part Number:

Picture

Cockpit

Manufacturer :

Part Number:

Picture

Cockpit

Manufacturer :

Part Number:

Picture

Cockpit

Manufacturer :

Part Number:

Picture

Cockpit

Manufacturer :

Part Number:

Picture

Cockpit

Manufacturer :

Part Number:

Picture

Cockpit

Manufacturer :

Part Number:

Picture

Cockpit

Manufacturer :

Part Number:

Picture

Cockpit

Manufacturer :

Part Number:

Picture

Cockpit

Manufacturer :

Part Number:

Picture

Canopy water deflector

Picture

Cockpit Safety Device

External Hatch Locking Handle Mechanism

describe its design and use:

Picture

Cockpit Safety Device

Internal Hatch Locking Handle

Picture

Divers Grab Handle

Picture

Canopy Hinge Attachment

Describe the attachment method

Number of bolts per hinge

Canopy reinforcement structure
supporting the bolt fixture

laminated schedule

Canopy (Lid) Structure

Supply the laminated schedule

Picture

Bottom Escape Access

External Tunnel Side

External Hatch Locking Handle Mechanism

describe its design and use:

Picture of data logger

Telemetry

Manufacturer:

Part Number:

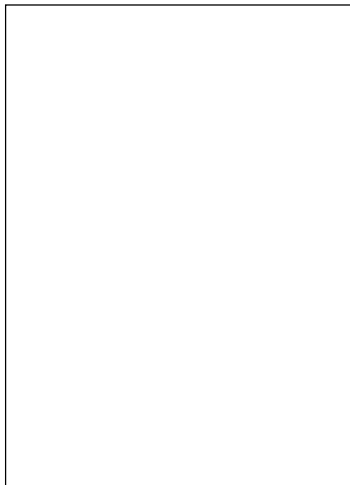
Please certify below that there is no transmission hardware/software capability onboard the boat, to transmit real-time data anywhere.

Functional scheme

Fuel System

The following information is required:

1. **Fuel tank**
 - a. Number
 - b. Capacity per tank
 - c. Location (simple sketch)
2. **Fuel pump**
 - a. Number
 - b. Supplier
 - c. Part Number
3. **Fuel hoses and pressure regulators**
 - a. Functional scheme



Device name:.....

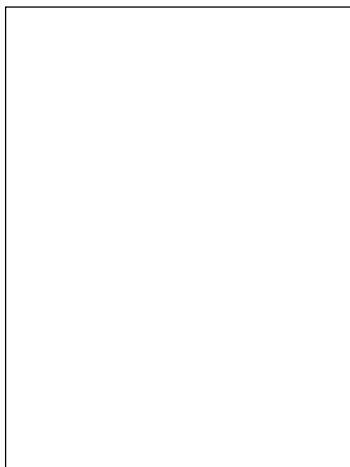
Update Information:

Date.....

Manufacturer :

Part Number:

Change description.....



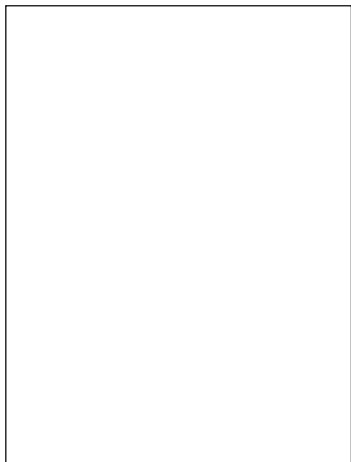
Device name:.....

Update Information:

Date.....

Manufacturer :

Part Number:



Device name:.....

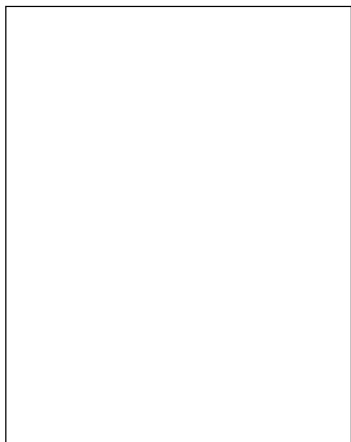
Update Information:

Date.....

Manufacturer :

Part Number:

Change description.....



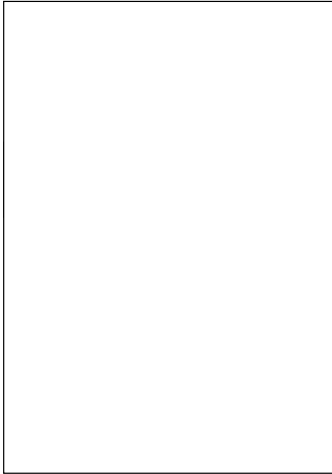
Device name:.....

Update Information:

Date.....

Manufacturer :

Part Number:



Device name:.....

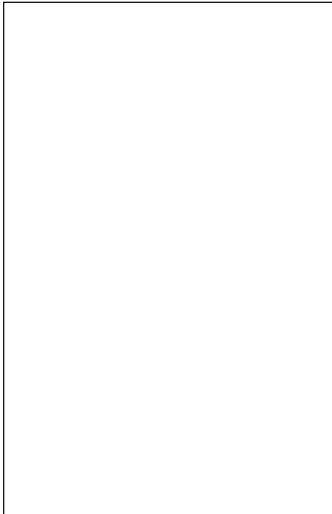
Update Information:

Date.....

Manufacturer :

Part Number:

Change description.....



Device name:.....

Update Information:

Date.....

Manufacturer :

Part Number:

Change description.....

06. MANAGEMENT OF THE ENGINES' RPM LIMITER FUNCTION IE. ALLOWED/NOT ALLOWED

1. Any device utilizing the ECU to control the rpm limiter must only utilize the engine rpm signal.
2. Boat speed, throttle position or other such inputs may not be used in conjunction with the rpm limiter.
3. The rpm limiter may only operate by cutting the injection or ignition separately or together.
4. The soft limiter is only allowed to activate above 95% of the maximum rpm listed in the UIM rules.

Engine Manufacturers and Teams are requested to send to the UIM/OPC a self-declaration that the above points are maintained and specify in detail the Map setting used to allow the UIM Official to check during the championship and/or on Dyno for post-race scrutineering. This must be done 30 days before the start of the Championship

07. BANNED METAL LIST

Nickel

Chromium

Cobalt

Hastelloy

Haynes

Inconel

Molybdenum

Monel

Platinum

Tantalum

Titanium

Tungsten or Wolframite

Waspaloy

08. BOAT LIFT EQUIPMENT

Teams shall ensure that their Boat has fixed lifting points. Teams must have all lifting equipment (straps and shackles) annually tested and supply a valid test certificate. Each Team must ensure that all of the lifting straps are of appropriate length and connected to a certified central ring or shackle to facilitate connection

to the weighing device. It will be the responsibility of Team members to connect lifting straps to their Boat and the central shackle or ring to the crane to facilitate lifting in or out of the water. Any damage caused by failure of straps or central ring, or due to incorrect attachment to the crane, shall be the responsibility of the Team.

09. PROTOCOL FOR UPDATING AND CHECKING ECU SOFTWARE

Per: Rule SECTION J.45.18. Single Map ECU's required

Preamble: Definition of map: An engine map is the implementation software, for all variables and settings that regulate the engines' operation.

1. Any supplier of an ECU that, from its original production, has continuously only had one map capability, must produce an homologated version of their ECU software that cannot be modified to have or use more than one map.
2. ECUs originally built with software capability for more than one map must supply the following:
 - a. The ECU supplier must provide UIM with a program that runs on a Windows PC that allows race officials to plug into an ECU and verify that it contains the homologated software. To validate the ECU, the program must work by reading the memory out of the ECU, and confirm that the software embedded in the ECU is the homologated one, and be able to read the actual map.
 - b. At least 30 days before the first C1 race, the engine manufacturer registered on the UIM homologation file must send an original written declaration, on company letterhead, certifying that the ECUs conform to the rules and specifying the names of their customers in the UIM Class 1 World Championship by courier to the UIM headquarters.
 - c. Each ECU supplier must also provide the UIM, via the engine manufacturer, the ECU's technical information specifying the function related to each connector pin.
 - d. One month before participating in the first C-1 Championship race each C-1 team must supply to the UIM the following:
 - i. Scrutineering software to validate their ECUs, including:
 1. A CD copy of the engine map used at the first race of the championship (there will be no obligation to keep the same map during the entire championship series).

2. The capability to read and copy the installed map.
 - ii. Information on the “communication methods” (Ethernet or CAN or RS232 serial).
 - iii. A communication cable to check an ECU in their boat.
 - iv. A declaration that their two engines’ ECUs do not communicate with each other for any kind of strategy.
 - v. A simple loom to allow C-1 to check an ECU out of the boat.
- e. Teams must issue a written declaration, to be delivered at race registration, that the ECUs installed are the ones the supplier certifies, the declaration will stay valid until the team changes its ECU supplier.
3. Teams must contact their engine manufacturer to insure UIM will receive the information on above points 1, 2 and 2.b. at least 30 days before the first C-1 race.
4. ECU retrofitting with a different supplier is allowed but must follow the above procedure.
5. If any ECU is found during a race weekend breaking the rules and the above protocol, the competitor will be penalized as follows:
 - a. Disqualification of the team/drivers from the involved race results.
 - b. A reduction of 50% of the team/driver’s accumulated overall championship points.
6. The above protocol does not replace, but integrates into the existing rules about ECU’s.

APPENDIX 7

DIGITAL BOAT AND EVENT LOGBOOK

A digital Event Logbook approved during 86th UIM G.A. must be implemented as from 2014; the web program is based on a modern Internet technology, logging all information available including the mandatory by- rules. The digital Event Logbook will be cloud-based with direct access from the UIM website, providing secure access through use of dedicated user credentials and all traffic will be SSL encrypted. (See details on APPENDIX 7)

The architecture of the project will consider two sections:

Boat Logbook

Event Logbook

- **Boat Logbook** will contain the following digitized document:
 - All Boat measurements
 - Actual measurements certificate (as per rules 501 integrated with more info)
 - Old scanned measurement certificate
 - Change old ownership
 - Boat leasing history
 - Actual cockpit registration document
 - Old cockpit registration document
 - Restrain annual renewal
 - Hull modification information
 - Boat equipment modification procedure as per rules
 - Attendance on race event
 - Accident report
 - UIM Officer decision for repairing after accident
 - Boat's main equipment data base
 - Engine manufacturer file
 - Engine UIM approval file
 - Engine type replacement file during measurement certificate validity
- **Event logbook** will contain the follow digitized document:
 - Event basic information:
 - Entry list

- Pole Position/Races result
- Championships overall points
- Timed lap on free test and races
- Technical Information:
 - Scrutineering files
 - Power and boat weight report
 - UIM Technical commissioner report
- Propeller file life information
- UIM Commissioner report
 - Report
 - Penalties
 - Race Bulletin
 - Race Instruction
 - Timetable
 - Administration file
 - Yellow flag file containing number and reason
 - Red flag file containing number and reason
 - Official mailing after race event
- Protest appeal copy file
- Miscellaneous information
 - Weather, sea condition
 - Media information
- Email and correspondence (non-confidential) between the involved parties: UIM-Commission- Committee-Promoter-Teams- Manufacturer

**THE RACING RULES OF
OFFSHORE POWERBOATING
EXTREME CAT
XCAT SERIES
2013/2014**

“We give credit to WPPA on the work done since 4 years to elaborate and make the racing rules for XCAT simple and objective”

CONTENTS

Introduction		538
Part 1	Fundamental Rules	539
Part 2	General Application	540
Part 3	Event Rules	545
Part 4	Technical Rules	591
Appendix 1	Recommended Safety Procedures and Guidelines	614
Appendix 2	On-Water Procedures and Guidelines	623
Appendix 3	On-Land Procedures and Guidelines	630
Appendix 4	Turn Buoys Clarification Drawings	637
Appendix 5	Start Procedure Drawings	640
Appendix 6	Advertising & Branding Code	642

INTRODUCTION

The Racing Rules of XCAT includes three main sections. The first “Parts 1–3”, contains rules that affect all competitors. The second “Part 4” provides details of rules, rules that apply to Class Promoter racing which affect only competitors or officials. The third section “Appendices 1–6”, provides details of rules and Guidelines required for the events.

No expressed or implied warranty of safety shall result from publications of, or compliance with these rules and/or regulations. They are intended as a guide for the conduct of the sport and are in no way a guarantee against injury or death to participants, spectators, or others.

The racing rules are revised and published every year by the (UIM), **the world governing body for the sport.**

This edition becomes effective January 1, 2014 **and supersedes all previous editions.** Any changes determined to be urgent during the season will be announced through national authorities and XCAT Class Promoter and posted on the UIM website. www.uimpowerboating.com

PART 1

FUNDAMENTAL RULES

01. FAIR RACING
02. ACCEPTANCE OF THE RULES
03. DECISION TO RACE
04. BANNED SUBSTANCES AND METHODS
05. MISCONDUCT

01. FAIR RACING

A driver and team owner shall compete in compliance with recognised principles of sportsmanship and fair play. A driver may be penalised under this rule only if it is clearly established that these principles have been violated. A disqualification under this rule shall not be excluded from the boat's series score.

02. ACCEPTANCE OF THE RULES

By participating in a race conducted under these racing rules, each driver and team owner agrees

- (a) to be governed by the rules;
- (b) to accept the penalties imposed and other action taken under the rules, subject to the appeal and review procedures provided there in, as the final determination of any matter arising under the rules; and
- (c) with respect to such determination, not to resort to any court or other tribunal not provided in the rules.

03. DECISION TO RACE

The responsibility for a driver's decision to participate in a race or to continue racing is his alone.

04. BANNED SUBSTANCES AND METHODS

A driver shall neither take a substance nor use a method banned by the Olympic Movement Anti-Doping Code or the World Anti-Doping Agency, unless a T.U.E. has been granted.

05. MISCONDUCT

Rules and Regulations exist for the safety and security of competitors, officials, and the public and for the orderly conduct of racing events and must be followed. All team members are required to behave in a responsible and acceptable manner throughout the duration of any event. Any behavior which acts to disrupt the smooth running of the events the series or could be deemed to have brought the sport in to disrepute may be subject to penalties (which may include disqualification, suspension from an event, reprimands, yellow cards, expulsion or financial penalties may be applied.

Abuse of Officials – Instances of verbal or physical abuse against any race or event Official or competitor will result in a penalty. Penalties may include suspension or disqualification from an event, exclusion from the Series/ Championship or imposition of a points and/or fines.

PART 2 **GENERAL APPLICATION**

SECTION A

01. GENERAL APPLICATION TO ALL EVENTS
02. GENERAL PRE-RACE REQUIREMENTS FOR CONDUCT OF EVENTS
03. COMPLIANCE WITH RULES AND REGULATIONS
04. INTERPRETATION OF THE RULES
05. AWARDS AND PRIZE MONEY
06. PAYMENT OF FINES
07. CONSUMPTION OF ALCOHOL
08. ANTI-DOPING REGULATIONS

SECTION B

09. INDIVIDUAL REQUIREMENTS FOR PARTICIPATION IN AN EVENT

SECTION C

10. UIM RULE CHANGE SUBMISSIONS PROCEDURE

SECTION A

01. GENERAL APPLICATION TO ALL EVENTS

Unless otherwise specified the following General Racing Rules apply to all UIM Races/events, and Special Events and time/speed trials.

A Race is defined as from start of Registration time on Day 1 to end of Final Day closure of Race Administration, unless stated otherwise within the UIM Rules.

All stated numbers of days for any post race correspondence is considered to be in accordance with the International dateline and not the race/event local time.

02. GENERAL PRE-RACE REQUIREMENTS FOR CONDUCT OF EVENTS

Event Participation - Teams are encouraged to participate in all media opportunities, autograph sessions, sponsor parties, parades, etc.

03. COMPLIANCE WITH UIM RULES AND REGULATIONS

Race Teams must comply with all UIM rules and regulations established for each particular Event, including speed zones, testing times, and testing only in designated areas, if applicable. The teams shall be solely responsible for checking with the appropriate UIM Officials at the Event during registration as to the applicable rules and regulations for testing at the event. The team must obtain the Race Instructions.

Any telemetry/locator GPS/Data or TV/Video recorder(s) required by the UIM to be fitted to the race boats must remain in place and removed or its content reviewed only with the permission of the Technical Director.

Violation of this rule may result in penalties including, but not limited to, fines, time penalties, disqualification and suspensions of not less than 30 days.

Ignorance of the rules is no excuse for not complying with the rules.

04. INTERPRETATION OF THE RULES

In all cases not foreseen by the international rules or seemingly inaccurately defined, any decision has to be taken bearing in mind the spirit of the rules. Decisions on interpretation will be taken by the Race Director and /or the Race Jury or the UIM.

Any telemetry/datalogging device or GPS transponding and/or TV/Video equipment required by the UIM to be fitted to all competing race boats is deemed to be the official technical recording of the race. Team owned devices are not accepted as evidence.

Television/Video evidence may be used for either the correction of a race decision, or implementing a change to provisional results up to 5 days after the posting of results.

05. AWARDS AND PRIZE MONEY

No prize money shall be distributed to any driver until protests have been completed and the official order of finish is posted by the race committee in that class.

Any boat which receives prize/start money and has an outstanding debt with UIM/Event Promoter will have that debt taken out of the prize/start money that is due them. If fees are paid by cheque at the race site, prize/start money may not be paid until the cheque clears.

The sponsoring organisation may designate special awards for outstanding performance.

A minimum prize fund may be established and announced in the Race Instructions.

Any XCAT awards provided by sponsors have to be agreed with Series Promoter.

Trophies will be awarded to the first three finishers. It is the responsibility of the competitor to accept their trophy at the awards ceremony or have a representative accept on their behalf. UIM will not be responsible for the trophy after the event.

All rules in the Rule Book, and all requirements of sponsors must be met, in order to participate in the prize fund or start monies.

06. PAYMENT OF FINES

All fines must be paid to the UIM by the end of the race. If the fine has been issued in conjunction with penalties qualifying for appeal, then the appeal must be written and properly filed within the specified time limit for appeals. Fines that remain unpaid after 14 days shall be subject to additional penalties. Post race/event issued fines must be paid within 14 days of notice being issued or will become subject to additional penalties.

07. CONSUMPTION OF ALCOHOL

No contestant shall participate in a race / practice or any Media-Hospitality Promotional Official Run, nor shall any official serve in an official capacity, while under the influence of intoxicants, or any dangerous drug, which shall include narcotics, depressants, stimulants, or hallucinogenic drugs. The race officials (Officer of the Day or the UIM Commissioner) shall suspend for minimum of six (6) months any competitor or official whom he believes to be under the influence of intoxicants or dangerous drugs as defined above or during the time the competitor arrives at the race site, on race day, until the competitor has been cleared through post-race inspection and for one hour after the race is over. No alcoholic beverages are to be consumed until one (1) hour after the unofficial results are displayed. No contestant or official participating at a UIM event may evade this rule by withdrawing his entry or by resigning his duties while at the event. Ceremonial champagne or equivalent usage in the winner's circle shall be permitted provided that only minor consumption occurs.

A competitor shall, if requested by an authorised officer, submit to an alcohol test. Written notification of selection for testing will be handed to a competitor by an authorised official. Refusal or failure to do so may be taken as if a positive test had been ordered and dealt with accordingly.

Testing when carried out shall be using an Alcometer.

For International Races, any competitor found to have more than BAK 0,10 0/00 (0,10 g/L blood // AAK 0,020 mg/L air) 1 hour before and during testing or race shall be immediately suspended and disqualified from the whole event. A format report shall be forward to the N.A. of the competitor.

If in an Organising Country, the legal requirements of the above numbers are lower, then these numbers have to be announced in the advance regulations and will prevail.

08. ANTI-DOPING REGULATIONS

The Anti-doping regulations are in line with the World anti-doping code as published in the UIM Offshore Rule book.

SECTION B

09. INDIVIDUAL REQUIREMENTS FOR PARTICIPATION IN AN EVENT

Releases - All participants in each event must execute a liability release to UIM, prior to racing. No owner, driver, contestant or his representatives or any race official shall hold any other owner, driver, competitor or his representatives or any race official liable for any personal injuries or damage resulting from an accident occurring during a sanctioned race, except as the result of a deliberate collision or premeditated act of unsportsmanlike conduct. The interpretation of this shall rest with the Race Director subject to review by the UIM.

Suspension, Expulsion - No individual who has been expelled from the UIM or its events or who is currently under suspension by UIM shall be permitted to officiate, serve as a race committee member, act in any capacity of assistance, or participate as a boat owner, driver or crew member in any sanctioned racing event. No boat belonging to that individual shall be allowed to compete, nor shall it be chartered or otherwise transferred except by final sale to render it eligible for a sanctioned race. The UIM also reserves the right to deny future membership to anyone who has previously been expelled or suspended from any category of UIM.

Medical / Rescue, Tow and Patrol Boat Coverage - LIFE JACKETS/ BUOYANCY SUITS - During all official times, all assistance / safety / rescue boat teams and all teams in support boats must wear life jackets and to conform to the LOC/NA safety regulation or special prescriptions.

The race organiser may or may not provide medical and rescue coverage for testing prior to the race event. If medical and rescue coverage is to be provided, a schedule will be posted at Race Registration detailing times and specific location of covered testing.

PART 3

EVENT RULES

SECTION A

- 01. RACE ORGANISATION AND SANCTION
 - 01.01. ELIGIBILITY
 - 01.02. RACE DATES
 - 01.03. SANCTION/EVENT HOSTING FEES
 - 01.04. REQUIREMENTS

- 02. TITLE

CHAMPIONSHIP /SERIES

- 03. SERIES POINTS & SCORING SYSTEM
- 04. CHAMPIONSHIP
 - 04.01. CHAMPIONSHIP/SERIES TIES
- 05. CREW/BOAT COMBINATION
 - 05.01. NATIONALITY

SECTION B

LOC ADMINISTRATION/ORGANISATION

- 06. RACE COMMITTEE MEMBERS AND ROLES
 - 06.01. LOCAL RACE OFFICER
 - 06.02. LOCAL SAFETY OFFICER
 - 06.03. JURY BOAT COORDINATOR AND COURSE OFFICIALS
 - 06.04. LOCAL PIT OFFICER
 - 06.05. LOCAL EVENT SECRETARY
- 07. EVENT INSURANCE

SECTION C

RACE ADMINISTRATION

- 08. RACE REQUIREMENTS
 - 08.01. PIT REQUIREMENTS
- 09. ADVANCE NOTICE
- 10. RACE INSTRUCTIONS

SECTION D

- 11. RACE MANAGEMENT AND UIM OFFICIALS
 - 11.01. RACE DIRECTOR
 - 11.02. SAFETY OFFICER
 - 11.03. ASSISTANT RACE DIRECTOR/ UIM COMMISSIONER
 - 11.04. TECHNICAL DIRECTOR
 - 11.05. RACE ADMINISTRATOR
 - 11.06. OFFICIAL RACE TIMING
- 12. RACE REGISTRATION (ON-SITE)
- 13. PRE-RACE INSPECTIONS
 - 13.01. REQUIREMENTS
- 14. PRE-RACE TECHNICAL RULES VIOLATIONS
 - 14.01. NOTICE TO OWNER
 - 14.02. CORRECTION OF VIOLATIONS
 - 14.03. PROTEST AND APPEALS
 - 14.04. GENERAL INSPECTION
- 15. TEAMS REPRESENTATIVE
- 16. DRIVERS REPRESENTATIVE
- 17. TEAM MANAGERS'

SECTION E

- 18. PILOTS' BRIEFINGS
 - 18.01. ATTENDANCE
 - 18.02. PILOTS' BRIEFING TIMES
 - 18.03. ALCOHOLIC BEVERAGES
 - 18.04. UNIFORMS

SECTION F

- 19. UIM LICENCE
 - 19.01. ISSUE OF SUPERLICENCE
 - 19.02. COCKPIT EVACUATION / IMMERSION TESTING
- 20. PARTICIPATION RULES

SECTION G**SAFETY RULES****GENERAL APPLICATION**

- 21. REQUIRED SAFETY FACILITIES
 - 21.01. MEDICAL/RESCUE, TOW AND PATROL BOAT COVERAGE
 - 21.02. SEA SAFETY
- 22. FUELLING
- 23. SMOKING
- 24. RACE OVERALLS AND PERSONAL SAFETY WEAR
- 25. RADIO COMMUNICATION EVENT - PRACTICE / POLE POSITION / RACE

SECTION H**GENERAL REQUIREMENTS, PRACTICES AND PROCEDURES FOR CONDUCTING RACES**

- 26. BOAT PARADE
- 27. TESTING/PRACTICE
- 28. POLE POSITION
- 29. RACE COURSE
 - 29.01. RACE COURSE LENGTH AND RACE COURSE DESIGN
 - 29.02. RACE LENGTH
 - 29.03. LONG LAP
 - 29.04. BAD WEATHER
 - 29.05. START CHUTE
 - 29.06. TURN MARKS
 - 29.07. TURN MARKS AND/OR ABSENT OR NON-CONFORMING TURN BOAT OR BUOY
 - 29.08. RIGHT OF WAY
 - 29.09. SPECTATOR AREAS
- 30. LAUNCHING
- 31. CRANING
- 32. PIT STOPS
 - 32.01. DEFINITIONS
 - 32.02. PIT STOP RULES
- 33. OUTSIDE ASSISTANCE

- 34. START PROCEDURE
- 35. FINISHING PROCEDURE
 - 35.01. STOPPING THE RACE
 - 35.02. CURTAILING THE RACE
 - 35.03. STOPPED / CURTAILED RACE
 - 35.04. FINISHING PROCEDURE
 - 35.05. 70% RULE
 - 35.06. TIME LIMIT
 - 35.07. NO FINISHERS
 - 35.08. RETURNING TO THE PITS
 - 35.09. RETIREMENTS
 - 35.10. POST RACE
- 36. POST RACE TECHNICAL INSPECTIONS
 - 36.01. ALL BOATS SUBJECT TO
 - 36.02. TECHNICAL DIRECTORS DISCRETION
 - 36.03. FINAL RESULTS
 - 36.04. ENGINE INSPECTIONS
 - 36.05. REFUSAL OF INSPECTION
- 37. POSTPONEMENTS
- 38. PRIZE GIVING
- 39. LAY DAYS
- 40. RESCHEDULING OF RACES

EVENT RULES

The following General Racing Rules apply to all UIM-sanctioned XCAT Series events.

The Rules and/or Regulations set forth herein are designed to provide for the orderly conduct of racing events and to establish minimum acceptable requirements for such events. These rules shall govern the condition of all XCAT Series events and by participating in the events, all UIM members are agreed to abide by these rules. No expressed or implied warranty of safety shall result from publications of, or compliance with these rules and/or regulations. They are intended as a guide for the conduct of the sport and are in no way a guarantee against injury or death to participants, spectators, or others.

All Race event rules and criteria/requirements may be subject to venue specific requirements and National Authority or geographic conditions that result in

changes to suit as contained in race instructions, bulletins or changes on safety concerns.

SECTION A

01. RACE ORGANISATION AND SANCTION

01.01. ELIGIBILITY

In order to be eligible to organise a UIM XCAT Series event, a Local Organising Committee must be sanctioned by the XCAT Promoter (WPPA) and recognised by the UIM. The National Authority or the Local Organising Committee must be recognised by the UIM, which must authorise the event, ensuring that the minimum requirements established in mutual agreement with the XCAT Promoter are in place.

01.02. RACE DATES

Candidatures for any XCAT event (World – Middle East- Continental Championship or any event using the name “Extreme Cat / XCAT”) must be addressed to the XCAT Promoter (WPPA). Event dates will be agreed between the UIM and the XCAT Promoter, which must present the annual calendar in accordance with a timescale to be agreed with the UIM.

Final approval for Race events must be obtained from the UIM after been approved and submitted by the Promoter.

01.03. SANCTION/EVENT HOSTING FEES

The XCAT Promoter is responsible for the definition of all aspects relating to the contract with the XCAT Local Organising Committee.

The financial conditions for staging the XCAT event will be established between the XCAT Promoter and the Local Organising Committee.

The XCAT Promoter and the UIM will establish, on an event-by-event basis, the fee due to the UIM, which will not include the event insurance required or the cost of UIM officials assigned to the event.

01.04. REQUIREMENTS

The Local Organising Committee contracted by the XCAT Promoter must follow the requirements and criteria established in these rules with regard the organisational and safety aspects of the event.

02. TITLE

The title of the Championship will be known as the “UIM XCAT World Powerboat Series”.

CHAMPIONSHIP/ SERIES

03. SERIES POINTS & SCORING SYSTEM

In any of the XCAT races, the points will be awarded to the first and second named pilots only. The first and second named pilots must be the helmsman, throttleman or navigator. The Navigator cannot be the first named pilot.

Unless otherwise specified, the following scoring system is to be adopted:

1	-	400 pts	11	-	22 pts
2	-	300 pts	12	-	17 pts
3	-	225 pts	13	-	13 pts
4	-	169 pts	14	-	9 pts
5	-	127 pts	15	-	7 pts
6	-	95 pts	16	-	5 pts
7	-	71 pts	17	-	4 pts
8	-	53 pts	18	-	3 pts
9	-	40 pts	19	-	2 pts
10	-	30 pts	20	-	1 pts

For events with more than 20 entries, the following bonus points should be given, so that all drivers can get points.

03.01. 21-25 BOATS: 5 BONUS POINTS

Position 1 to 20: should all have 5 extra bonus points

Position 21: 5 points

Position 22: 4 points

Position 23: 3 points

Position 24: 2 points

Position 25: 1 point

03.02. 26-30 BOATS: 10 BONUS POINTS

Position 1 to 20: should all have 10 extra bonus points

Position 21: 10 points

Position 22: 9 points

Position 23: 8 points

Position 24: 7 points

Position 25: 6 points

Position 26: 5 points

Position 27: 4 points

Position 28: 3 points

Position 29: 2 points

Position 30: 1 point

04. CHAMPIONSHIP

The XCAT World Series will be determined from the points accrued from all the XCAT World Series events. All Races carry points with no discards.

04.01. CHAMPIONSHIP TIES

In the case of a tie in the overall Championship/Series, the number of first places shall be considered, then the number of second places, etc.

In the case of the Championship/Series still being a tie, the boat with the fastest average speed in any of the Series races will be deemed the Champion.

05. CREW/BOAT COMBINATION

The points and penalties for the UIM XCAT World Series will be awarded in principal to the Registered Boat number and Team entry.

It is mandatory to register the Team Owner Name before the start of the Series (Team Name - Boat number & Crew) eligible to race.

05.01. NATIONALITY

The nationality of the entry is that determined by the Team/Owner as declared at the first race. The nationality of the Pilots is as declared at the first race and whose nationality is determined by either the Passport or Racing Licence of that pilot.

SECTION B

LOC ADMINISTRATION/ORGANISATION

06. RACE COMMITTEE MEMBERS AND ROLES

The Local Organising Committee appoints a Race Committee which is composed of:

- Local Race Officer
- Safety Officer
- Jury Boat Coordinator
- Course Officials
- Pit Officer
- Event Secretary

06.01. LOCAL RACE OFFICER

The Local Race Officer forms the principle link with the local organisational race management safety, Jury boats, air safety etc.

The Local Race Officer must direct the organisation of the races on behalf of the Local Organising Committee.

Good knowledge of the UIM Rules, especially those relating to XCAT / Offshore Power Boat Racing.

Fluent written and spoken English are obligatory.

He is responsible for the implementation of local services and must maintain communication with Jury boat coordinator and local safety officer.

06.02. LOCAL SAFETY OFFICER

The Safety Officer reports directly to the Local Race Officer. He/She coordinates with the XCAT Safety Officer, in accordance with the UIM Safety Procedures and Guidelines (Appendix 1), for all safety and rescue activities on land, on the water and in the air, including the preparation necessary for these activities.

During all official race activities the Safety Officer must be present at Race Control.

06.03. JURY BOAT COORDINATOR AND COURSE OFFICIALS

This group is composed of one Coordinator located at Race Control, plus a minimum of two observers for each turn - buoy. The group must attend training by a qualified UIM Official a minimum of two hours prior to Pole Position.

At the end of the race, they draw up and sign a statement certifying that all participants' conduct during the race and at the turn marks was in accordance with the UIM On-Water Procedures and Guidelines (Appendix 2), UIM Turn buoys clarification drawings (Appendix4) and with the Race Instructions. This statement is handed to the Race Director, who will apply the UIM rules in the case of infringements. The Course Officials monitoring turning marks must have a clear, unobstructed view.

06.04. LOCAL PIT OFFICER

The Pit Officer must guarantee security of the areas and all the services as detailed in the PIT REQUIREMENTS under item 08.01 and the UIM XCAT On-Land Procedures and Guidelines - APPENDIX 3.

06.05. LOCAL EVENT SECRETARY

The Event Secretary is responsible for the general local organisation of the event. He/She must ensure that all the various local officials know their respective duties and that they have the necessary documents and equipment to fulfill their duties. He/She must collate all documents following the race.

07. EVENT INSURANCE

The Organising Committee must have an Insurance Policy which covers all Race and Non-Race activities for both Competitor and Organiser liability. It must include event third party insurance which is to cover all racing and non-racing activities and, in particular, any damage resulting from towing or launching of the boats and full third party cover for all race officials and associated race helpers.

The Organiser's insurance will be available from the Organising Committee during Race Administration.

The competitors are strongly recommended to have a racing third party insurance. The amounts must not be inferior to those required by the law of the countries where the races will be held. In addition to any other insurance as required by the National Authority or local organizer, Fee to be paid by the team.

All insurance must cover the whole period of the Practice, Pole Position and Races from beginning to end of the event.

SECTION C

RACE ADMINISTRATION

08. RACE REQUIREMENTS

The latest version of all UIM Procedures and Guidelines, as listed below, must be strictly adhered to:

- APPENDIX 1 - Safety Procedures and Guidelines
- APPENDIX 2 - On-water Procedures and Guidelines
- APPENDIX 3 - On Land Procedures and Guidelines
- APPENDIX 4 – Turn Buoys Clarification Drawing
- APPENDIX 5 – Start Procedure Drawing & Clarification
- APPENDIX 6– Advertising and Branding Code

Failure to comply with the indications given therein will result in a penalty to be applied at the discretion of the Race Director.

08.01. PIT REQUIREMENTS

At all Events the Local Organiser must establish clearly defined, readily identifiable areas for Dry Pits, Wet Pits, and Crane/Launch Areas in accordance with the outline given below.

In all cases these areas must be set apart from the general public by being fenced off or some other means, except that the public may be admitted to the Dry and Wet Pit Areas during designated times. Only contestants, crews, officials, approved event staff or current members of the UIM or the XCAT Promoter shall be admitted to the Launch Area. The sale to the public of pit admission tickets or passes to any pit area is strictly prohibited. These areas require the execution of Insurance Waiver/Indemnity by all participants (racers, crews, family members, officials and event staff - not spectators) for admission. Alcohol use within any of the designated pit areas by any UIM/XCAT Promoter members or a signee of the insurance Waiver is strictly prohibited and may result in the imposition of a penalty at the discretion of the UIM events have three mandatory pit area classifications with the following restrictions:

Crane and/or Launch area and/or Fuelling area: shall be defined as an adequate area around launching cranes, fuel storage, fuelling area. Entry into these pit areas requires UIM or XCAT Promoter Racing, Crew or Official Membership. Except for cases of emergency, the maximum number of boat crew in the lift and drop areas is restricted to maximum 4 support crew and 2 competitors. Team guests are not to enter this area.

Fuelling of boats shall be allowed only in the approved fuelling area(s). Fuelling or de-fuelling of race boats in the water is specifically prohibited. Fuelling at commercial marina pumps on the water or commercial land-based filling stations is allowed, providing they have met the fuel requirements (as specified in the Official Advance Notice & Race Instructions); it is also mandatory to use any fuel supplier provided.

Enclosed Footwear is recommended in the Craning Areas.

Waste oil and fuel must be disposed of in a responsible manner by all race teams.

Signs shall be posted around all working pit areas designating "No Smoking or Drinking of Alcoholic Beverages."

All UIM race events are required to provide crane service for boats that provide their own single point lifting harness. All boats are required to provide their own single point lifting harness for weighing purposes.

The crane area shall be highly restricted and fenced off from all other areas. No persons shall be permitted to the crane area except specifically authorised personnel.

Wet Pits - Wet Pits shall be defined as the docking areas, as crew working areas and, except in cases of emergency, the maximum number of boat crew in wet pit areas is restricted to 4 support crew and 2 competitors. Team guests are not to enter this area. Access to this area may be available by controlled admission pass dependent on pontoon capability.

Dry Pits - Dry Pits shall be defined as those areas where race boats are on display prior to launch. Race Officials have the right to remove a race team or race boat from this area for disruptive conduct. Teams may be subject to other disciplinary actions.

09. ADVANCE NOTICE

The Advance Notice must be received by the UIM and, after the approval of both the XCAT Promoter and the UIM, it must be sent to the XCAT Promoter's Office for distribution at least 30 days before the Race.

The Advance Notice shall contain the following information:

- Local Organising Committee contact details
- Provisional timetable
- Race course and distances
- Map of Pit Areas
- Accommodation, travel and transport details for teams and equipment
- Details of insurance, fuel and social events, if this information is available

10. RACE INSTRUCTIONS

The information contained within the Race Instructions is to be considered as a clarification of the UIM Rule Book or additions to take account of special local conditions.

These Race Instructions shall contain all of the following information:

- Official timetable and locations.
- Official race course and details.
- Local conditions, such as any special instructions and/or information pertinent to the race, e.g. special radio channels required, nautical chart numbers, tides and any venue-specific Authorities' instructions.
- Terms and conditions of insurance for personal and material damage to third parties, competitors, to be included if available.
- The minimum limit of insurance cover required.
- The type of risks to be covered by the insurance.
- The amount of insurance fees to be paid at the race site.

No course, timetable or race site logistics can be published without direct approval of UIM and the XCAT Promoter.

SECTION D

11. RACE MANAGEMENT AND OFFICIALS

The following UIM officials will be present at all Events to assist in the management and conduct of the Event:

- Race Director (RD)
- Safety Officer
- Assistant Race Director / Race Commissioner
- Technical Director
- Race Administrator

Each of the above may have an assistant, to be appointed from the pool of UIM-approved personnel.

11.01. RACE DIRECTOR

The overall on water management of the race is the responsibility of the RD who liaises with the (his assistant - RD - and the Safety Officer).

The RD keeps an overview of race and is the ultimate decision maker on the overall conduct of the event. This includes such matters as to whether the weather conditions are suitable for racing, while the RD is responsible for the actual conduct of the race. It is therefore important that the RD is a very experienced Official and that he is recognized as such. The RD also supervises the onshore aspects of race management.

Cominoff shall appoint a Race Director who shall serve at the discretion of the UIM. The Director shall be responsible for the supervision of the race and the correct application of the UIM rules. He shall perform such other duties as are requested from time to time by the UIM. The Race Director must be included in the race/event approval process and receive all course and race instruction detail for approval.

Presence at Events - Each UIM-sanctioned racing event must have a Race Director present. Should an officially designated Race Director be unable to attend any sanctioned racing event, the UIM may appoint an alternate to serve at that racing event.

Duties and Responsibilities

Pilots' Briefings - The Race Director shall attend all pilots' briefings. He shall see that all questions relative to racing rules are properly answered and that the conduct of the race itself is in accordance with all racing rules.

Enforcement of Rules

Purpose - The Race Director shall see that all UIM racing and technical rules are enforced throughout the event. All possible rules infractions shall be reported only

to the Race Director by the appropriate UIM official. It shall be the sole responsibility of the Race Director to communicate with any team member or other party who has been charged with the violation of any rule, as well as the proposed action that UIM shall take as a result.

Multiple Roles - Unless there is prior agreement and approval by the UIM, the Race Director shall not serve in any other official capacity such as Timer, Technical Director, Jury Chairman/Member etc., nor shall he act as a member of any committee charged with the actual management of the racing event or any other racing organisation that directly competes with the UIM while also serving as the Race Director.

Interpretation of Rules - He/She will along with the Assistant Race Directors be responsible for overseeing all sporting matters or penalties of a Championship / Race nature outside the remit of the race jury. The Race Director will additionally chair and coordinate event meetings between race officials, promoter and local authorities representatives on event. Should any questions regarding the interpretation of racing rules arise, the Race Director shall rule on these matters after consultation with all contestants, witnesses and other UIM and local officials involved. In the event that the Race Committee or another UIM Official makes an erroneous decision or interpretation, the Race Director is authorised to overrule that decision.

Training - Members desiring to become a UIM official must complete a training programme by the UIM.

Other - The Race Director of a sanctioned event also must ensure that:

- All proper officials be present and function during the conduct of an event to the relevant UIM standard.
- The approved course is provided (courses must be pre-approved by the UIM before the sanctioned testing or racing begins).
- At race sites, the UIM safety and rescue requirements are met.
- All bulletins, results when he also fulfils the role of the Local Race officer (OOD) - and official race documentation are signed by him, and that he is on hand at all times for this purpose.

Post Race Reports – The Race Director must file a report in writing to the UIM detailing all of the events, the conduct of the races, any accidents or other incidents such as disciplinary actions, technical Disqualifications, weather conditions at race and test times, course infractions, and penalties assessed interviews with competitors or witnesses to infractions, etc.

11.02. SAFETY OFFICER

The Safety Officer is appointed by the XCAT Promoter and recognized by **Cominoff**, is responsible for safety and rescue operations. He must be familiar with the race venue, with the characteristics of the class(es) competing powerboats and any applicable governmental or similar rules. The cooperation with local or private non-profit rescue organizations is highly recommended.

The Safety Officer must be familiar with the safety regulations under which the race is being organised, that is to say the safety requirements of the National Authority, of the Class rules, of the Race Instructions and of any authority over the race water such as the local harbour master/board.

It is highly desirable that any possible conflict between these be resolved before the race and that the Race Instructions give the final word, including resolution of any conflict.

11.03. RACE COMMISSIONER /ASSISTANT RACE DIRECTOR(S)

The Race Commissioner /Assistant Director(s) will be appointed as chairman of the Protest Jury. The Commissioner/Assistant Race Director will assist the role of race director where necessary in his/her absence. The assistant acts with the director in the delivery and coordination of race rules and the local services.

The Assistants are also course race officials with regard to race rules, and may be acting as Director within race control, or as Start Marshal or Course Judges.

Post Race Reports – The Race Commissioner shall file a report in writing to the UIM detailing all of the events, the conduct of the races, any accidents or other incidents such as disciplinary actions, technical Disqualifications, weather conditions at race and test times, course infractions, and penalties assessed interviews with competitors or witnesses to infractions, etc. The Assistant Race Director shall report all accidents, even when no injuries are involved, to the UIM. This does not stop local authorities from reporting those accidents, as required by any local authority law or otherwise to be reported. Race Officials will assist local authorities in completing their reports where necessary.

11.04. TECHNICAL DIRECTOR

Cominoff shall appoint a Technical Director who shall serve at each event. The Technical Director shall be responsible for developing and implementing the Technical Inspection plans, procedures and protocols (including branding marketing requirements) for each Event, and managing the Assistant Technical

Director (if any) and their implementation of those practices, procedures and protocols.

The Technical Director is a member of **Cominoff**. He's responsible for the research into future technical rules and requirements, and is empowered to liaise with manufacturers to facilitate the future technical advancements.

Assistant Technical Director - The Technical Director shall appoint from time to time as necessary additional qualified assistants to serve in conjunction with him or in his place. They shall be charged with specific functions as determined by the Technical Director. A list of such Technical Assistants may include suitably qualified individuals from a UIM-recognised National Authority.

Presence at Events - Each UIM-approved racing event may have an Assistant Technical Director present who must be appointed by **Cominoff** upon the request of the Technical Director.

DUTIES AND RESPONSIBILITIES

Compliance with Technical and Safety Rules - The Technical Directors and assistants main responsibility is to inspect the competitors' racing equipment for compliance with the applicable technical, equipment and safety rules and document their findings on each piece of equipment inspected at an Event.

Unsafe Equipment - The Technical Director shall also have the right to refuse any entry at any time prior to the start that in his/her opinion is not designed, built and equipped to the standards of seaworthiness and safety required for any adverse weather and water conditions during the race.

Further Technical Responsibilities - It is also the Technical Directors responsibility to check all the activities relating to technical areas (craning, fuel, etc.) and to report his findings to the Race Director.

Reporting Infringements - The Technical Director inspect racing equipment for compliance with the technical and safety rules and if they find what they believe to be a violation of any of those rules, then they shall report the infraction to the Race Director for the event. The Technical Director do not discuss their findings with anyone including any member of the relevant team, until the matter has been reported to the Race Director and the Race Director has authorised same. **Extra**

Duties in the Case of Accident - In the case of accident, the Technical Director must check all race boats involved for seaworthiness and report his findings to the Race Director.

11.05. RACE ADMINISTRATOR

The UIM shall appoint a Race Administrator. He/She is responsible for the general sports-related administration and organisation connected to the event, and shall also perform such other duties as requested from time to time by the UIM. The Race Administrator shall be responsible for managing the assistant administrator (if any).

DUTIES AND RESPONSIBILITIES

Membership, Registration at the Event - The Race Administrator shall be responsible for collecting and accounting for all registration details along with the respective forms.

Insurance Disclaimer - It shall also be the duty of the Race Administrator to have all members sign the insurance Disclaimer at each race site.

General documents and Race Bulletins - The Race Administrator will supervise the Local Event Secretary in organising and drafting all general racing documents and bulletins as required. The Race Administrator will also organise any briefings/daily meetings as required.

Signature and Posting of Results - The Race Administrator shall be responsible for posting the arrival order in a pre-determined and announced location, and communicating those results to the designated media representative following the final race of the day.

The Race Administrator shall also be responsible for arranging for the final results to be signed and for posting them on the Official Notice Board.

11.06. OFFICIAL RACE TIMING

An Official timekeeping system and operator will be appointed by the UIM and is the official race timekeeping system. In XCAT the system and operator will be provided by the XCAT Promoter. Official timekeeping system may be linked to the mandatory transponder equipment required by the UIM. The mandatory transponder/virtual system may be used as either the principle timing system or the reserve system and operated by the UIM and/or its contractors.

The UIM Race Officials will provide to the operator the necessary entry list details and the official course distance calculations and information.

The Race Director and his assistants will provide information for the operator regarding race information, penalties, communications etc. as necessary during the course of each session and race.

At the end of each on water timed session the operator will make available to the Race Administration the system results.

Race Administration will publish the official timed and signed result.

These results are all provisional and subject to post race technical scrutineering, post race/pole position reports, protests etc.

12. RACE REGISTRATION (ON-SITE)

Registration will take place at the Race Administration office.

Race Registration times will be as per the Advance Notice. All Race Registration requirements (including payment of insurance) must be completed by the time Race Registration closes for signing in to the event.

Failure to do so will result in non-participation in the racing event.

On the registration form for each XCAT Race, the names of the Team Owner, Team Manager, first and second pilots, together with reserve and test pilots, must be specified. Also the name and boat number and a copy of the current valid measurement certificate with registration number must be supplied.

On signing in at Race Registration, a Scrutineering/Technical inspection slip will be issued, which will enable competitors to present their boats for Scrutineering/Technical inspection.

All Official Race Information, Bulletins and Results will be posted on the Official Notice Board at the Race Registration Office.

Competitors will be provided with identification tags/wrist bands, if required, which must be worn at all times during practice, Pole Position and Racing.

Any person other than a racing crew member wishing to embark on the boat for practice will only be authorised under the condition that they are a UIM-licensed reserve/test pilot and have registered as such during Race Administration or as otherwise agreed by the UIM Race Director.

Changes to crew must be notified to race control a minimum of one hour prior to testing. PENALTY Euro 600.

He must sign up with the Organiser's Insurance Agent with either the Local Organiser or the XCAT Promoters' representative.

He must be in possession of all current documents required by the UIM and organising committee.

All race numbers will be between 1 and 99 and will be allocated by the UIM after the entry has been accepted. Number 1 will be allocated to the previous year's World Champion, unless otherwise requested. The replaced number of the World Champion's boat will be reserved for this boat to resume when it is no longer the World Champion. Race Bulletins will be issued and signed for at Race Briefings and/or the Race Administration Office.

Restraint Indemnity - UIM rules mandate the signing of the National Indemnity form regarding restraints. In view of the International nature of the XCAT series and the fact that all boats use restraint systems, in applying for the licence, the competitor confirms that he has read and understood the UIM, and his National Authority's rules regarding restraint systems and confirms that the boat entered for the event in which he is taking part has been constructed to and conforms with these rules. He further undertakes not to hold the UIM, his National Authority, the Organising Club, or any of the servants or agents of the foregoing, nor any other person connected with the organisation of an event, responsible for any personal injury incurred by his wearing this restraint system. He will also produce on demand a copy of his National Authority's rules for restrained cockpits.

13. PRE-RACE INSPECTIONS

13.01. REQUIREMENTS

The Technical Director may also at any time during the event pre testing, pole, and/or race, require random inspection and testing of equipment for compliance to the rules.

All race boats entered in a sanctioned race are subject to a pre-race inspection by a Technical Director.

The time and place of such inspection shall be published in the Advance Notice for the Event.

No race boat may be considered a bona fide entrant in a UIM race until such time as the Technical Director has passed and signed the official pre-race technical inspection form.

It is the responsibility of the Team Owner or Manager to submit his team's equipment to the Technical Director for his inspection. If, in the judgment of the Technical Director, a boat and/or safety equipment is unseaworthy, unsafe, or unmanageable, the non-compliance must be brought to the attention of the Race Director. If the Race Director determines that the condition cannot be rectified prior to the start of the race, then he shall have the right to prohibit the boat from competing.

The Technical Director shall examine each entry for compliance with all safety requirements and shall also visually inspect hull, propulsion and engine for compliance with XCAT Technical Rules. The spirit as well as the letter of these rules shall be enforced equitably to all entrants.

14. PRE-RACE TECHNICAL RULES VIOLATIONS

14.01. NOTICE TO OWNER

In the event that a pre-race inspection finds a technical rule violation, the Race Director or Technical Director must notify the Team Owner of the boat or the Team Manager of the specific nature of the alleged violation as soon as practical, but in no instance later than the Final Pilots' Meeting. Depending upon the nature of the violation, and whether this is a first or multiple offence, the UIM may issue notice with a range of penalties from fines to immediate Disqualification from the event.

14.02. CORRECTION OF VIOLATIONS

After a race boat owner has been notified of the violation, he shall have until 1 hour prior to Pole Position to correct the violation, unless he has been disqualified. Should he elect to correct the violation then it shall be his responsibility to have the boat re-inspected and cleared for competition.

14.03. PROTEST & APPEALS (Refer to Article 400 of the UIM Rule Book white pages and any amendments as described in the Race Instructions)

14.04. GENERAL INSPECTION

During this Inspection the UIM shall also check the team for compliance with the marketing requirements in force for the race series or event venue. Teams shall comply with the marketing requirements as set out in their contract with the Series Promoter.

15. TEAM'S REPRESENTATIVE

A Teams' representative must be elected by the XCAT Teams Owners in the presence of the UIM Official and candidates must be teams actively participating in XCAT Series for a minimum of two complete seasons.

The team's Representative is elected at the first race of the season for a one-year term.

The Teams Representative ***may attend all Cominoff meetings and shall receive all UIM communications relevant to XCAT. She/he has no right of vote.***

16. DRIVERS' REPRESENTATIVE

A Drivers' representative must be elected by the XCAT Drivers in the presence of a UIM Official. Candidates must be drivers actively participating in XCAT Series for a minimum of two complete seasons.

The Drivers' Representative is elected at the first race of the season for a one-year term. He/She should be available to attend meetings called by the Promoter at annually, to report and advise on the consensus recommendations and safety requests from the drivers. He may be invited to attend all ***Cominoff*** meetings and may receive all relevant UIM communications relevant to XCAT.

17. TEAM MANAGERS

Each boat must designate its Team Manager at each race venue for contact in case of an accident or Force Majeure this representative must be present and registered at Race Administration.

Similarly, each boat must have its representative present at the Team Managers' Area (as described in the Race Instructions) throughout Official Practice sessions, Pole Position and Grand Prix races for all official communications. PENALTY Euro 500.

It is mandatory to have at all times during on water activities an efficiently working radio system, and to stay in permanent radio contact with Race Control. PENALTY Euro 500.

Team Managers' briefings (if planned as per Race Instructions) must be attended by the Team Manager. It is his responsibility to ensure that all team and crew members are informed about all local authority requirements both on land and on water. He must also ensure that the crew are fully conversant with all testing, pole position and race course areas and all relevant safety requirements.

SECTION E

18. PILOTS' BRIEFINGS

18.01. ATTENDANCE

Pilots' briefings shall be confined to crew members, Team Owners / Managers and concerned officials only. Unless otherwise provided in these rules attendance at these meetings is mandatory for every boat crew. All pilots and crew members attending any pilots meeting must wear a team uniform.

Racing Team members who fail to wear a uniform as specified above will be subject to a fine of Euro 200.

18.02. PILOTS' BRIEFING TIMES

It is mandatory for one pilot from each team to attend the Teams course inspection at all venues. Time to be announced in race instructions.

It is mandatory for both first and second pilots to be present at the main briefing and 1 of the 2 nominated pilots for the weather briefing (if held).

Signing in for any briefing will begin 20 minutes before scheduled pilots' briefing time. It is mandatory for all competing crew members to attend and sign in at the briefing. All signing in must be completed prior to the pilots' briefing time. Penalty for non-attendance - Euro 700 fine per person and / or Disqualification. Pilots who are late for a briefing: Penalty Euro 350.

It is essential that all competitors read and are totally aware of the contents of the Race Instructions and/or any Race Bulletins before attending the Pilots' Briefing.

It is the responsibility of all pilots to bring the Race Instructions and any bulletins to all Briefings.

Event Race Instructions may contain one mandatory Official Reception that competitors must attend. Penalty Euro 700.

The use of mobile phones during briefing is strictly forbidden Penalty Euro 150.

18.03. ALCOHOLIC BEVERAGES

Sale or consumption of alcoholic beverages is strictly prohibited at all pilots' briefings.

18.04. UNIFORMS

All racing crew and team members are required to be in team uniform at Pilots' Briefings, race/event functions and on race day, unless other requirements are published in the Race Instructions / Programme. Penalty Euro 200.

SECTION F

19. UIM SUPER LICENCE

In order to participate in UIM XCAT Series, every crew member of the boat must hold a UIM XCAT Superlicence.

All UIM licences are valid from the date of issue to 31st December of the same year.

In the event of any accident either within the sport or beyond it's jurisdiction that prevents an individual from racing on medical grounds, a new post injury medical certificate must be submitted prior to re issue of the individuals licence.

Additionally in the case of a race incident resulting in medical care the competitor must be signed fit to return to racing by the race/event doctor. If this is not obtained, a signed declaration from a Doctor of fitness to return must be submitted to the UIM.

The event insurance must be complied with.

19.01. ISSUE OF LICENCE

A UIM XCAT Superlicence can be obtained following a request by the pilot's National Authority to the UIM.

The application for a UIM XCAT Superlicence must be lodged with the UIM four (04) weeks prior to the first event in which the pilot wants to participate is held.

The superlicence fee will be doubled for applications received within the 4 weeks deadline.

NO Application will be taken in consideration within one week from the following event.

The UIM XCAT Superlicence fee has been fixed to 300 euro by the UIM Council and must be paid to the UIM Secretariat.

For late application, the superlicence fee has been fixed to 600 euro and must be paid to the UIM Secretariat.

A Medical expertise fee for drivers over 50 years of age in the season must be paid to the UIM Secretariat : 150 euro.

All required documents must be completed and the following additional documentation, or proof of such, must be provided to the UIM Secretariat:

- The sports career of the pilot, including the results obtained in the previous two seasons of offshore racing.
- Evidence of having raced during the previous 2 years in Extreme Cat (6 litre), or equivalent as pilot or throttleman in at least 7 races. (Other classes may be taken into consideration as agreed by the UIM).
- Current immersion certificate.
- A medical certificate providing all the data required by the UIM Official medical form. (This form is available from the UIM Secretariat and published at the end of the rulebook
- Two photographs (passport size).
- The fees fixed by and paid to the UIM.

The UIM may issue the UIM XCAT Superlicence on the basis of the information supplied by the pilot's National Authority. The UIM may withdraw a UIM XCAT Superlicence on the recommendation of the relevant pilot's National Authority.

Any pilot who has passed their 50th birthday before the date of application for a UIM XCAT Superlicence will be obliged to submit a medical report from their own doctor, and also a report of an examination by a doctor nominated by the UIM.

The minimum age for a pilot to qualify to hold a UIM XCAT Superlicence and to compete in a XCAT race is 18 years unless otherwise agreed by the UIM.

The application and nomination by the pilot's National Authority will be deemed to be the National Authority's consent to represent their country for the duration of the UIM XCAT Superlicence. Should the NA not wish a UIM XCAT Superlicence holder to represent his country at any time, the NA must inform the UIM in writing.

The UIM may authorize a provisional Licence for the second Driver who may not have the above - mentioned sports career to participate in the competitions provided that:

- The First Driver is well - experienced, has attended the last two C-CAT Series and takes the responsibility of the Second Driver's behavior in the race, the first driver must also sign the UIM letter of agreement regarding responsibility and;
- He should have an Offshore Licence issued by his relevant National Authority.
- The application should be should be authorized by his N.A.

An anthropometric check of data reported in the Superlicence application may be carried on at any event during the relevant season by a UIM appointed Medical and/or Anti Doping delegate. In case the pilot's data would not be within the mandatory parameters, the Superlicence will be immediately withdrawn.

19.05. COCKPIT EVACUATION / IMMERSION TESTING

Before racing in a craft with restraint systems, all crews must have passed in the last fourteen months, an immersion test in a restraint system to ensure that they can exit a reinforced cockpit crew compartment successfully.

Prior to taking the immersion test, all crews must have a valid scuba certificate or have received suitable training. This alternative training should be approved by the National Authority.

An Immersion Certificate to certify the passed test, showing the expiry date, must be delivered by experts recognised by a National Authority.

All riding crew members using restraints must sign the National indemnity form prior to competing in any race or practice.

UIM XCAT Cockpit Evacuation training and testing is available at the XCAT Series races.

Regardless of any current immersion training certificates held by competitors the UIM XCAT Cockpit Evacuation training and testing is mandatory.

Immersion testing is to include training for all crews in both self extraction and full partner rescue training utilizing on board air systems. All crew members must be capable of full use of on board air systems to the satisfaction of the appointed UIM testing facility.

The UIM will also request mandatory random testing throughout each racing season for all crews, where possible this will be integrated with the local event rescue crew training.

20. PARTICIPATION RULES

The UIM and its National Authorities and Organising Clubs reserve the right of entry of any team or competitor and has the right to accept or reject entry/eligibility at the discretion of the UIM on the grounds of the unsuitability of any entrant for reasons such as and not limited to unsportsmanlike conduct, bringing the sport into disrepute.

Eligibility - The Team entries for the Series/Championship must be received by the UIM via the XCAT Promoter and notified to the relevant recognised NA and must include at least the Owner's name, the name of the Team Manager, the names of the First and Second Pilots and the race number of the boat. The name of the two pilots must be stated including any reserve/test pilots if any.

It is mandatory for each entrant to participate in all XCAT races to accumulate points unless otherwise agreed in writing by the UIM.

The UIM will decide the penalty which may be a fine of min. Euro 5,000 max. Euro 35,000 and/or alternative penalty for contravention of the above.

In the case of accident damage to a boat, the pilot may apply to the UIM for dispensation not to compete in the next races until the damage is repaired. The Technical Director must verify and certificate the damage and report to the UIM. If the dispensation is requested for, the applicant must pay all the costs.

New series entries arriving after the first round will not be eligible for Series/Championship points but will be podium placed. Late entries are at the discretion of the promoters and the UIM only.

Boats may be replaced with no loss of points in the following circumstances :

- 1. Should the registered hull become totally lost or totally destroyed (as certified by UIM Officials and/or approved marine surveyors, in writing, at the expense of the owner), it may be replaced by like kind, for completion of the racing season, or if seriously damaged, until restoration of the original hull. (This replacement hull then becomes the registered hull during its period of use). If the replacement hull is a boat already in the championship, it may re-enter the championship under its original race number.***
- 2. Teams may also change hulls, with the same boat number, once during the season, provided this new hull is used for all the remaining races. In this case it is not possible for this designated driver/hull/number combination to use the previous hull any more in that racing season***

unless the replacement hull becomes seriously damaged. This replacement hull then becomes the registered hull.

Inscription Fee & Prize Money - as per contracted promoter's agreement.

Entry to the UIM XCAT Series must be made on the official UIM/WPPA entry form with the accompanying Race Number registration fees prior to racing and include the names of the first and second pilots. Also the teams request for allocation of race number.

Race numbers are allocated as follow:

Race Number 1	Reserved for Current Champion (Unavailable if not registered by the last years Champion)
---------------	---

Fee:	Free
------	------

Race Numbers 2-9	Fee: Euro 1050
------------------	----------------

Race Numbers 10-99	Fee: Euro 550
--------------------	---------------

Previous holders of numbers have the first option to keep their race number subject to above.

Renewal of race numbers	Fee Euro 100.
-------------------------	---------------

3 Digit numbers are not permitted.

SECTION G

SAFETY RULES

GENERAL APPLICATION

The following Safety Rules apply to all XCAT UIM Series and Championship Events and are in addition to all safety related rules set forth above in the UIM General Racing Rules.

21. REQUIRED SAFETY FACILITIES

In General - All International-Series Event Medical and Rescue facilities must be pre-approved by the UIM at least 30 days in advance of the Event. It shall be the Local Organising Committee's responsibility to communicate with the UIM and perform all safety functions and secure all necessary safety facilities in accordance with these rules and the UIM Safety Procedures and Guidelines Requirements.

Each Local Race Promoter shall be required to provide all personnel, materials and services required by the UIM Safety Procedures and Guidelines.

It will be the responsibility of the Local Race Promoter to provide, to complete and return the UIM XCAT Pre-Race Safety Questionnaire Sheet to the UIM office no later than 30 days prior to the event.

21.01. MEDICAL/RESCUE, TOW AND PATROL BOAT COVERAGE

For detail of Safety/Medical cover requirements, see APPENDIX 1.

21.02. SEA SAFETY

Safety Equipment & Sea Safety - Whenever a registered race boat bearing UIM numbers or branding is being operated at a UIM event, including while testing, or at any other times while the craft is on plane, or while off-plane under its own power and within 250 feet of the outside of an active race course, the competitors must wear complete approved and tested personal safety equipment including helmet and life jacket. This rule shall be in effect at a race site from the time of arrival and until one hour after the chequered flag of the last event of the day. Whenever such registered equipment is being operated at times other than UIM events, such operation shall be conducted in a careful and prudent manner according to local rules and regulations with regard to safety, so as not to endanger life or property and so as not to bring powerboat racing into disrepute.

Violation of this rule shall result in a fine or Disqualification from that event or a subsequent event at the discretion of the UIM Race Commissioner.

Pilots must have the full Safety Equipment for all Practice, Pole Position and Races.

It shall be the pilot's sole and ultimate responsibility to ensure the fitness of his boat and to decide whether or not to start or to continue in the race once the boat has passed scrutineering. The Organisers wish to inform competitors that all safety and patrol boats will be instructed that their first priority at any incident is saving human life. If possible, although no guarantee can be given, assistance may be given to boats.

In case of an incident or accident on the racecourse, a yellow flag will be flown or waved by any "Official" boat. Race boats should keep a distance of at least 50 metres away from any boat displaying this flag and the accident. A minimum penalty of a Yellow Card for infringement of this rule will be enforced.

Harbour restrictions. All boats must respect the local rules with reference to the maximum speed in the harbour. A Euro 500 penalty for infringement of this rule may be enforced. Crews may also be subject to Local Authority conditions and possible legal actions.

It is the Team's sole and ultimate responsibility to recover damaged and/or sunken participating race boats.

22. FUELING

Any fuelling infringements will result in disqualification from the current race weekend

- ***Boats are to arrive at the pits empty of fuel, all boats will be inspected during scrutineering and must activate the fuel pumps into a container, until no further fuel comes out to ensure only residue is present in the tanks, no outside fuel is allowed in the pits including fuel in containers.***
- ***All fuel containers must be empty when arriving at the pits.***
- Fueling times will be notified in the Race Instructions and/or Bulletin, posted on the Official Notice Board.
- The only fuel approved for use in UIM Events, will be that supplied by the local organiser or series supplier, as the case may be. The Unleaded fuel supplied will have a minimum of 95 Octane. Unless otherwise notified in the Race Instructions.
- The addition of 2 stroke oil is permitted.
- No modifications or additives are permitted to the fuel.
- Fuel samples showing any characteristics other than the fuel issued at the event, will be deemed illegal. The penalty is Disqualification from the event for a first infringement, and more serious penalty for a second offence.
- The UIM ***technical*** inspector, ***or his designated person***, can at any time take samples from boats to ensure compliance.

23. SMOKING

"All smoking of any type is prohibited in pit areas". The deemed area for non-smoking is in the dry pits (excluding inside workshops), craning area and in the wet pits. Competitors are reminded that it is their responsibility to ensure that the rule

is strictly adhered to by all their team members. Any contravention of this rule shall incur a penalty of Euro 100.

24. RACE OVERALLS AND PERSONAL SAFETY WEAR

Any person aboard any boat partaking in Races, Pole Position and Practice must always wear an adequate race vest as per UIM rules. Life jackets/Buoyancy suits must be worn during all races, Pole Position and practice. Life jackets/Buoyancy suits may only be removed when effecting repairs and with the boat at very slow speed (less than 10 knots). The choice and efficiency of Life jackets/Buoyancy suits is the sole responsibility of the wearer.

Any person aboard any boat partaking in Races, Pole Positions and Practice must always wear an adequate helmet of predominantly orange colour. Helmets must be worn during all races, Pole Position and Practice. Helmets may only be removed when effecting repairs and the boat is at very slow speed (less than 10 knots). The wearer is entirely responsible for the choice and the efficiency of his helmet. Helmets may be removed when returning to pits at less than planning speed.

Each crew member's helmet must be identified with their boat race number. This number shall be placed on the top surface of the helmet and shall be read from the rear. The numbers shall have a minimum height of 7.5 cm and shall be of the same style as the boat race number. These numbers shall be painted in waterproof black paint or be made of black coloured self-adhesive material of adequate strength. The wearer is entirely responsible for the choice of his helmet. The organisers are requested to repeat this important rule in their particular rules, as well as in their Advance Programmes and Race Instructions. Each National Authority may require that their nationals wear a helmet of a type laid down in their own National rules.

At all times during Practice, Pole Position and the Race, competitors or test pilots must wear suitable protective clothing that covers the torso and all limbs to the wrists and ankles. Protective clothing used must be durable enough to provide bodily protection and it is recommended to be fire retardant.

This protective clothing must have the name of the competitor clearly visible on the front chest section or waist band and the competitor must apply to Race Control for a wristband. The inclusion of Blood Group detail added to the race overalls is recommended but not mandatory.

Contravention of any of the above clothing rules will result in a minimum of a Euro 500 fine. It is strongly recommended that neck protection is worn.

25. RADIO COMMUNICATION EVENT - PRACTICE / POLE POSITION / RACE

Radio communications with the Race Boats and Team Managers will be maintained on the designated VHF channel as detailed in the Race Instructions and/or Bulletins and/or Briefings.

The Team Manager is responsible for ensuring that all communications to be made to the Race Boats have been received by the pilots on board.

Communications via VHF radio are allowed excluding Official Race Control Channels and Channel 16. Competitors must be fully conversant with, and at all times, must comply with, the International Regulations for the Prevention of Collisions at Sea and such other safety measures promulgated in connection with this event.

Competitors must at all times use the Race Number of their boats their call sign.

Competitors are required to communicate with Race Control before leaving and returning to the Wet Pits at all times. A team representative must be present in the timing area during all on water activity - no boats may leave the Pit area without the presence of the team representative.

No boat will be allowed to leave the wet pit before its radio communications system, as well as that of its Team Manager, has been checked and approved by the Technical Director.

It is mandatory to have at all times during on water activities an efficiently working radio system, and to stay in permanent radio contact with Race Control. PENALTY Euro 500.

It is prohibited to use hand held Radio by the Riding Crew Members to communicate with the race control. Riding Crew members must have a radio capable of monitoring race control integrated in their race helmets. PENALTY – DSQ- Disqualification

Communications - Radio, cell phone or other electronic communication from any aircraft, boat, or shore crew, to any race boat shall be permitted provided that those communications are not used to violate or assist in the violation of any of the rules contained herein, and provided further that any team using such communication must provide UIM live access to said communications through assigned radio frequencies or otherwise so that UIM Officials can monitor those communications when requested by UIM. Such communication is prohibited on official Race Control channels.

SECTION H

GENERAL REQUIREMENTS, PRACTICES AND PROCEDURES FOR CONDUCTING RACES

26. BOAT PARADE

All boats must take part in the Boat Parade unless previously agreed by the Race Director.

27. TESTING/PRACTICE

Inspections and Registration - Race boats must have a pre-race technical and safety inspection, the boat and all riding crewmembers must complete all registration requirements prior to any on water testing. The owner and all riding crew members must sign the Mandatory Insurance Waiver/Indemnity prior to any on water testing.

Testing must always take place on the race course where possible.

Any boats wishing to practice during the official practice period, must communicate with Race Control on the designated event channel for approval before proceeding out on the course and on safe arrival back in the Wet Pit pontoon, in accordance with the procedures contained in the Race Instructions. Penalty Euro 500.

Practice times will be detailed in the Race Instructions and/or Bulletins. Any practicing outside of these times must be agreed by the Race Director. Boats must not cut across the practice course except in the case of Force Majeure and with extreme caution. Penalty for non-compliance - Euro 500 per boat for first offence, Euro 1,000 for subsequent offences in the event and/or the Series/Championship.

Official Practice will only take place after Administration and Scrutineering have been completed satisfactorily.

Penalty for non-compliance will result in a penalty of Euro 1,000.

28. POLE POSITION

A Pole Position will be held on the race course either by designated Pole Position session the day before the race, the final practice session unless otherwise stated in the Race Instructions or posted by Race Bulletin as change due to unforeseen local conditions or weather.

The UIM will approve all rules, procedures and guidelines for the official Pole Position.

All communication will be on the event official VHF channel, as announced in the Race Instructions, via the Team Manager and/or Race Boat.

Boats may run in accordance with the official "On-Water Procedures and Guidelines". As per Race Instructions.

PROCEDURE - APPENDIX 2-01.

29. RACE COURSE

29.01. RACE COURSE LENGTH AND RACE COURSE DESIGN

The UIM must approve all XCAT Series Race Courses. Courses must be submitted by the Local Organisers at least 30 days prior to event.

The promoter and/or the UIM will correspond with the drivers representative with the draft race course for comments and observations in writing for consideration by the UIM and within 48 hours of the notification to the representative.

29.02. RACE LENGTH

The XCAT Race lap must be a minimum of 2.8 Nautical Miles and a maximum of 4 Nautical Miles. Race Laps must be repeated. The minimum distance for race duration is 60 Nautical Miles or 1 Hour plus a lap, or as stated in Race Instructions.

Under no circumstance during the event may any boats practice, Pole Position or race outside of the designated area. Failure to comply will result in a fine of up to Euro 1,000.

Failure to complete the course correctly as described in the Race Instructions and/or at Race Briefing and/or Race Bulletins, will result in time added or lap penalties.

29.03. LONG LAP

It is mandatory for all racing boats to complete the Long Lap(s), as specified in the Race Instructions, at any time from the start of the third lap; the first long lap is to be completed before the end of the 50% lap and the second by the end of the 70% lap. Failure to complete the Long Laps will result in time added or lap penalties, as follows:

- Failure to do the first long lap by the end of the 50% lap - Penalty 1 Long Lap.
- Failure to do either of the 2 long laps - Penalty Disqualification.
- It is recommended that the Long lap should be approximately 25 and 35 seconds longer than the course race lap.

29.04. BAD WEATHER

If it is necessary to run a Bad Weather course and/or bring the Race start time forward, full points will be given provided:

- The length must be a minimum of 50% of the original programmed number of laps.
- A minimum of one-hour notification must be given prior to the start time.
- The Teams' Representative, Race Director, Promoter's Representative, Safety Coordinator and Local Race Officer must agree the bad weather course and the number of laps.

29.05. START CHUTE

Minimum Width - The recommended minimum starting chute width shall be 30 metres per race boat.

Minimum Length - The recommended minimum starting chute length shall be 1.5 miles before the first turn mark.

If the starting chute length is less than 1,5 nautical miles, the first turn must not be more than 90 degrees to the second buoy of the turn.

Other Requirements - The chute must be maintained in a straight line so that the boats can maintain their position. The start cannot be set up with a turn before the end of the chute.

The start run must allow for the pole position boat to have the shortest distance to the first turn mark.

The start chute may be clearly marked, buoys or fixed points on either side of the course. All competing boats must pass through the start chute between the markers.

The finish boat shall fly a XCAT Flag or a specifically-designated flag, and chequered flagging will be done from the official finish boat.

29.06. TURN MARKS

All turn marks on the course, approved by the UIM, will be published in the Race Instructions.

When a turn is less than 90 degrees, it should be designated by a minimum of 3 mark buoys a minimum of 100 metres apart.

Above 90 degree turns should be designated by a minimum of 2 mark buoys a minimum of 100 metres apart.

Any buoy used to designate race marks should be a minimum of 1.5 metres tall, and made of vinyl or plastic material capable of withstanding conditions encountered in powerboat racing.

It is recommended that where turn marks are located in the vicinity of permanent navigation marks, the turn mark must be laid to the outside of the permanent mark at a minimum distance of 10 metres.

Each turning point must be marked by a buoy in accordance with the relevant guidelines and controlled by the Course Officials. Different colours must be used to indicate whether the boat must pass inside or outside the buoy.

29.07. TURN MARKS AND/OR ABSENT OR NON-CONFORMING TURN BOAT OR BUOY

The procedures for passing buoys are included in the UIM On-water Procedures and Guidelines (Appendix 2), with related illustrations of the various scenarios in the turn Buoys Clarification Drawings (Appendix 4)

All marks are laid in the approximate positions referred to in the Race Instructions and/or Drivers' Briefing and/or Race Bulletins.

It is the competitor's responsibility to drive close enough to the course marks to ensure that he/she has been seen to pass correctly and to satisfy him/herself to this effect.

Unless otherwise specified in the Race Instructions and/or Drivers' Briefing and/or Race Bulletins, the colour of the Course Buoys must be as follows:

All Outside Course Marks YELLOW

All Inside Course Marks ORANGE

Should a buoy go 'missing', the pilot must pass through the approximate geographical position referred to in the Race Instructions and/or Drivers' Briefing and/or Race Bulletins. Penalty for not passing through the geographical position

or around the Boat waving the yellow flag - one Long Lap or Time Penalty 30 Sec minimum, 40 Sec Maximum.

If a boat hits a buoy on the correct side, no action will be taken. If a boat goes over a buoy, demolishes and/or dislodges a buoy, a one Long Lap or Time Penalty will be applied.

If the buoy is also demolished, the team will be fined Euro 1,500 to refund the cost of the buoy. See (Appendix 4)

The repetition of missed buoy is not allowed – Penalty: DSQ and Yellow Card

Any buoy passed on the wrong side during practice may result in a Euro 500 fine, and if safety or other boats are compromised a Yellow Card.

Course IN and OUT Procedures and mark buoys must be follows at all time during Practice, Pole Position and Race.

Failure will result in Euro 500 fine.

Infraction during race will result in additional time penalties and/or disqualification.

Any buoy passed on the wrong side will result in a Penalty specified in the table below:

(See also Appendix 4).

Turn Mark Comprising of 1 buoys		
1 Buoy missed	=	DISQUALIFICATION
Turn Mark Comprising of 2 buoys		
1 Buoy missed	=	One Long Lap Penalty or Time Penalty
2 Buoys missed	=	DISQUALIFICATION
Turn Mark Comprising of 3 buoys		
1 Buoy missed	=	One Long Lap Penalty
2 Buoys missed	=	Two Long Laps Penalty or Time Penalty
3 Buoys missed	=	DISQUALIFICATION
Repetition		
Repetition of a missed buoy	=	DISQUALIFICATION
Course Correction		
Re-taking of a missed buoy or turn is not permitted	=	DISQUALIFICATION

29.08. RIGHT OF WAY

Overtaking - When two race boats are on the same course or approximately the same course on a straight, and one boat is passing or attempting to pass another boat, the passing boat shall keep clear of the boat being passed, and in passing, shall allow at least 2 boat-lengths of clear water between its transom and the bow of the boat being passed, before altering its helm so as to assume the same or essentially the same line or lane as that of the overtaken boat.

Passing in a Turn (Formerly “Overlap”) - If two or more boats are approaching a turn mark side by side, the boat on the inside of the turn has priority and the outer boat must keep clear.

Any overtaking boat must still give way to the overtaken boat as per overtaking above.

Priority must be given to the race boats on the race lap.

Long Lap - Boats returning from their long lap to the race lap must give priority and give way to the boats on the race lap unless they are clearly ahead as overtaking above. The return to race lap line is at any point when returning from Long Lap mark buoys and rejoining the race line at any point.

Yellow Flag - On seeing the Yellow flag signal from a safety boat, race boats must slow down, acknowledge the signal, proceed with caution and keep clear of the danger area. No overtaking is permitted under this caution. Any competitor who ignores the Yellow flag signal will be penalized and/or issued with a Yellow card and minimum Euro 1000 Fine.

Red Flag - All boats must safely slow from racing and make their way under safe speed as described in Race instructions or drivers briefing to the muster/milling area.

29.09. SPECTATOR AREAS

Organisers should designate spectator areas. If a race boat finds itself in a spectator area, it must reduce speed to less than 12 knots immediately and proceed at less than 12 knots with caution until back on the race course.

Penalty Yellow Card.

30. LAUNCHING

Launching and recovery facilities are available to competitors during Official Races, Pole Positions and Practice. The operational hours are indicated in the

Race Timetable. No facilities will be available to competitors until they have signed in and completed Administration and Technical Inspection.

All boats' movements, launching and recovery instructions will be under the responsibility of the Technical Director. Once signed in, no boat may leave the compound for any reason unless agreed by the Race Director.

At the end of the Pole Position and the Grand Prix race, unless otherwise advised by the Technical Director, boats are obliged to return to the crane from which they were launched.

In all cases when a crane is used, it is forbidden for any person to be on or in the race boat while the boat is being lifted. The penalty for non-compliance will be a Euro 1,000 penalty and/or Disqualification from the event. Exception is rule 32.02 Pit Stops.

The order of launching for the race and the choice of the crane and launch time will be decided by the classification order of the Pole Position, or based on the previous race result if no Pole Position takes place.

If the race boat is not ready to be launched at its designated time:

- The team may be fined Euro 500
- The boat will be launched at a time to be decided solely by the Technical Director.

No boat will be recovered from the water once launching has commenced unless with the permission of the Technical Director.

If a race boat, having been launched needs to use the crane, it may only do so after all race boats have been launched and with the permission of the Technical Director.

No propeller or ratio changes will be permitted until the start has taken place and the change will be deemed a pit stop.

31. CRANING

Craning time is fixed at a maximum of ten minutes. A boat can enter the craning area only when it is ready to be hoisted. The time starts from when the boat is attached to the crane by its straps and ends when the straps are released from the crane.

Boats exceeding the craning time during pole position or race will not be permitted to continue in the Pole Position or race.

Practice session time infringement Penalty Euro 500.

32. PIT STOPS

32.01. DEFINITIONS

Pit Stops - visit to the designated Pit Stop area for assistance during the Race and/or Pole Position i.e. from the time of the race Start until the time the last boat crosses the finish line. The Pit Stop area is the Wet Pit unless otherwise defined in the Race Instructions.

32.02. PIT STOP RULES

The Pit Stop procedure is under the responsibility of the Technical Director.

No-one is permitted in the water during pit stops.

Whilst on the crane pilots may remain harnessed for prop changes (safety issue exiting and entering the boat). In this case, 1 x support crew to check with race crew that BATTERY SWITCHES ARE OFF for duration of change. No Crew are allowed on deck Penalty Euro 1000. If advantage gained and/or time penalty and/or disqualification.

PROCEDURE - APPENDIX 2-02

33. OUTSIDE ASSISTANCE

Refuelling is not authorised for any race boat whilst competing in the Pole Position or Grand Prix race.

Outside assistance may only be provided in the officially designated Pit Stop Area.

Changes - No change is allowed during Pole Position and Grand Prix Race, except if conforming to Pit Stop Rules. Otherwise the Penalty is Disqualification.

The Race Director has the right to decide if there was illegal outside assistance, which will result in Disqualification.

34. START PROCEDURE

Start/Pace Boat - The Start/Pace Boat Procedure is as described in the latest version of the Appendix 2 UIM On-water Procedures and Guidelines and/or the Race Instructions or race bulletins and must be followed at all times.

Description - The description of the Start/Pace Boat will be given in the Race Instructions.

Safety Equipment:

- Life Jackets to be worn by all onboard personnel.
- Radio VHF marine system.

Start Procedure - The Start Procedure is described in full in **APPENDIX 2-03**.

35. FINISHING PROCEDURE

35.01. STOPPING THE RACE

Emergency Race Stoppage Procedures/Restarts (Force Majeure).

The procedure is included in **APPENDIX 2-04**.

Only the Race Director shall have the authority to stop an event.

Red Flag race stopped procedure (see Pace Boat Procedure Appendix 2-04).

The severity of the incident and race stop decision is taken only by the Race Director with information from his race officials.

If the race is stopped after completion of 70% of the race laps, then the boats will be classified based upon their position when crossing the previous lap timing line. In this case, a restart will not be implemented.

35.02. CURTAILING THE RACE

In the case of Force Majeure or accident, the Race Director may curtail (shorten) the race by the waving the Finishing (Chequered) Flag. Boats will be classified on completion of their current lap, based on their position at the Finishing line.

35.03. STOPPED / CURTAILED RACE

In the event of a Stopped or Curtailed Race, if the 'winning boat' has completed:

- A minimum of 50 % of the actual programmed number of laps: full points will be awarded.
- Between 25% but less than 50 % of the actual programmed number of laps: half points will be awarded.

- Less than 1 lap: the race may be restarted using the normal procedures.

Any Engine Bonus points are awarded following a race start if stopped by this procedure.

In the event of a Stopped or Curtailed race, boats must have completed a minimum of 70% of the laps completed by the 'winning boat' to be classified a finisher.

LONG LAP CALCULATION "Stopped/Pace boat Procedure"

- If a race is stopped before the completion of 70% and the Pace Boat is used, the mandatory long laps can be done after the restart at any time before the end of the race.
- If the race finishes while the Pace Boat is running 1 hour (time Limit), the order of the boats will be used for the final classification. Time compensation will apply provided that all the boats have done at least one Long Lap. In this case, the boats that have done a Long Lap will be compensated replacing their long lap time with the time of their best race lap (excluding the first/start lap).
- If the race finishes by the 50% lap, one Long Lap will be counted.
- If the race finishes by the 70% Lap, then two Long Laps will be counted.

LONG LAP CALCULATION "Stopped/Curtailed"

- If the race is curtailed between the beginning of lap 2 and the 50% lap and not all the boats have done at least one mandatory Long Lap, none of the long laps will be counted. The boats that have done a Long Lap will be compensated with the time of their best race lap (excluding the first lap/start lap).
- If the race is curtailed between the 50% and the 70% laps, two long laps will be counted. Time compensation will be applied to the boats that have done only one mandatory Long Lap, replacing their fastest Race Lap with the time of their previous mandatory Long Lap.

35.04. FINISHING PROCEDURE

White Flag - When the leading boat crosses the finish/timing line to commence its last lap, a white flag with XCAT logo will be raised on the Finish Boat.

The white flag will also be announced via radio and timing computer.

Chequered Flag - When the leading boat completes the course and crosses the finishing line, the chequered flag will be waved. It designates the 'winning boat' for the Race and the start of the finish procedure.

The chequered flag will also be announced via timing computer.

The full finishing procedure is included in **APPENDIX 2-05**.

- The Winner, 2nd and 3rd place will be awarded for the Grand Prix.
- In order to be classified as an official finisher, a race boat must make a legal start. The Driver and Throttleman must remain in the boat during the entire race, except during Pit Stop procedure.
- After crossing the finish line, a race boat shall not interfere with any other boat still in the race so as to affect the time of such boat at the finish or endanger its crew.
- Official race results shall be posted on the Official Notice Board and on the UIM website and XCAT website.

A boat is timed when the bow crosses the finishing line.

A boat that finishes a race must follow the procedure described in the UIM On-Water Procedures and Guidelines and Race Instructions and/or Bulletins and/or Pilots' Briefing. Penalty Euro 1000 and Yellow Card.

The Team support crew cannot under any circumstance open any engine, cockpit or any other hatch on the race boat until they receive the permission of the Technical Director. Penalty Disqualification.

Posting of Results - The arrival order will be posted at a designated location, and may be announced on radio after the race. A racer or team cannot badger any officials for their decision which they are ruling on.

For any race that is subject to Post-Race Dyno Testing, the finish order is provisional pending report from Dyno Test Facility.

As soon as the results have been officially confirmed by the Race Director the final official results are posted. Once the official posting of the results has taken place, there is a period of one hour in duration during which protests can be made. These results are deemed to be provisional for the purposes of post race dyno and/or engine inspections testing and subject to any video evidence of race infractions.

35.05. 70% RULE

CALCULATIONS (PERCENTAGE/FRACTION)

Throughout the % rules, where a percentage/fraction needs to produce a whole number, the number will be rounded up or down to the nearest whole number. Where the percentage/fraction results in .5 (1/2) or less, the number will be rounded down unless specifically stated in the rule.

For example:

- 70 % of 7 laps is 4.9, 5 laps would count.
- 70 % of 5 laps is 3.5, 3 laps would count.

The minimum number of laps to qualify as a finisher may also be announced at the relevant Pilots' Briefing and/or bulletin. This should equate to 70% of the laps of the race.

Any boat crossing the finishing line after the 'winning boat', will be classified based on the number of laps completed and its time, provided it has covered the minimum number of laps.

Any boat that fails to cross the finishing line after the winning boat will be classified based on the number of laps completed and its time, provided it has covered the minimum number of laps.

The Course will close 15 minutes after the 'winning boat' has finished unless otherwise stated in the Race Instructions, Race Briefing and/or Bulletin. Any Lap completed after this time will not be included in the final results.

35.06. TIME LIMIT

All Grand Prix races will have a maximum time limit of 1 hour. The chequered flag will be waved when the lead boat crosses the line after the 1 hour maximum time limit, and all boats will finish as they complete that lap. The number of laps completed by the 'winning boat' will determine the number of laps for the 70% rule. Alternate distance may be set by Race Instruction. Extreme Cat have maximum time limit of 60 mins + 1 lap unless stated otherwise in race instructions.

35.07. NO FINISHERS

Should no boats finish, each boat will be classified based on the number of laps completed and its time, provided it has covered the minimum number of laps of the 70% rule, as announced at the relevant Pilots' Briefing and/or Bulletin.

35.08. RETURNING TO THE PITS

When a race boat retires, the strobe light must be on until in the wet pit. A Euro 500 penalty for infringement of this rule may be enforced.

35.09. RETIREMENTS

Boats, when retired, must report to Race Control on the VHF channel detailed in the Race Instructions. If a boat has announced its retirement to Race Control, the timing will be stopped at the moment of the announcement. Retired boats are forbidden to proceed on the course and must return to the wet pits, with assistance if necessary.

35.10. POST RACE

After the end of the race, to be classified as a finisher competitors must not receive any outside assistance, go alongside, collect other persons aboard, allow access to any deck/engine hatches or remove or place in electronic equipment etc., before reporting to the official Post-Race Scrutineering area and being cleared by the Technical Director. Disregard for this rule will result in Disqualification. With the exception of medical or mechanical emergency by permission of Race Control.

36. POST RACE TECHNICAL INSPECTIONS

36.01. ALL BOATS SUBJECT TO

All boats are subject to a post race technical inspection conducted by a Technical Inspector. A list of approved inspectors shall be issued annually by **Cominoff**.

36.02. UIM TECHNICAL DIRECTOR DISCRETION

The Technical Director has the discretion to develop and implement a Technical Inspection Plan for each Event, and may vary the items and boats to be inspected at each Event. The Race Director may also direct the Technical Director to conduct an inspection of any competing race boat.

36.03. FINAL RESULTS

No points shall be awarded until the legality is confirmed by the Technical Director to the Race Director, but still remain provisional subject to post race dyno and/or

engine inspection and any video or virtual/Telemetry evidence regarding race infringements.

36.04. ENGINE INSPECTIONS

At the discretion of the Race Director or Technical Director, any engine may be stripped down to check legality. It is also the Race Director's and Technical Director's right to request re-inspection at any point up to the presentation of awards. When an owner or pilot has been notified that his equipment is to be stripped down, he has the option of having either an "Open" inspection (in full view of competitors and any other observers) or a "Closed" inspection (open only to the official Technical Director's and the Race Director assigned to this inspection). The results of a technical inspection may be made public by a designated UIM Official but only after the information has been transmitted to the Team Owner.

Engines may be sealed for transport to official inspection or dyno testing facility.

36.05. REFUSAL OF INSPECTION

Any entrant to UIM events refusing an inspection after having been notified by the Race Director or Technical Director that equipment within his control must be checked for conformity with safety and/or technical specifications shall be disqualified from that event and suspended for sixty (60) days.

Violations - In addition to the penalties provided in above clauses of Rule 36, the following procedures and penalties shall apply to post race technical inspections and violations.

First Offence - A first offence of a technical, weight, safety nature at any time during the annual registration of the boat in question may result in the forfeit of monies for the event, a deduction in position for that event, Fine Euro 1,000 - 10,000.

This is applicable unless a penalty is established in any other specific rule.

Second Offence - A second offence of the same nature at any time during the annual racing registration of the boat in question shall result in a **mandatory Disqualification from the event**.

Subsequent Offence(s) - A subsequent offence(s) of the same nature at any time during the annual racing registration of the boat in question will result in **mandatory Disqualification from the event and the current racing year**.

Team Responsibility - The owner shall be responsible for the condition of the engine and hull as raced. Errors on the part of the manufacturer, boat builder,

engine builder, mechanic or previous owner shall not excuse non-compliance with the rules. The Owner is also responsible for their team's conduct at an event.

Additional Responsible Parties - The party responsible for making illegal modifications to engines shall be subject to up to one year suspension from UIM events and /or a Euro 50,000 fine and if a commercial homologated vendor, possible barring of all equipment prepared by them for competition in UIM Events.

37. POSTPONEMENTS

If a race cannot be run due to inclement weather or any other unforeseen circumstances, it can be rescheduled to run on the day following the scheduled, date or a storm course may be substituted. This decision shall be made by the Race Committee in conjunction with the Pilots' Representative and Local Authorities.

Hourly postponements will be permitted when weather or other conditions are such as to make it unsafe to start a race at its designated time, but there is a reasonable chance that conditions will improve. In no case shall a race be started if it will place race boats on the course within one (1) hour before sunset.

If a race cannot be run due to inclement weather or any other unforeseen circumstances, and cannot be rescheduled, the entry fee is not refunded.

Changes to the Event - Any race postponement or substitution of an alternate storm course shall be announced by Race Bulletin. The Race Committee shall adhere to the schedule set forth in the Race Instructions unless some emergency shall dictate otherwise.

In the case of any such changes, the UIM Race Officials must notify the teams at least one hour prior to the race.

38. PRIZE GIVING

1st, 2nd and 3rd place will be awarded for the Grand Prix.

Additional prizes may be awarded at the discretion of the Organisers.

The No.1 and No.2 pilots of the first three classified boats must appear at the prize-giving (and at the following press conference if any) at each XCAT Heat, unless agreed in writing by the XCAT Promoter. The prize-giving (and the press conference if any) will be held within one hour after the end of the race. The pilots must wear their own race overalls.

Penalty for either of the above: Euro 1000 fine.

39. LAY DAYS

Lay days are allowed if previously agreed with the UIM and announced in the Advance Notice.

40. RESCHEDULING OF RACES

Should it be impossible to hold a scheduled race due to Force Majeure, the UIM may reschedule to race at a different time and place, the commitment of the teams to participate remaining unaffected, provided a minimum of 28 days' advance notice is given prior to the date of the original event that is being cancelled. Unless agreed by the promoter and the UIM.

If insufficient notice is given, the race may not be rescheduled and may be lost.

Reserve dates/venues will be fixed by the promoter in agreement with UIM.

PART 4

TECHNICAL RULES

XCAT - TECHNICAL SPECIFIC RULE

01. HULL MEASUREMENT

02. HULLS MINIMUM/MAXIMUM LENGTHS AND WEIGHTS

03. MINIMUM FLOTATION

04. HULL/DECK REQUIREMENTS

05. MANDATORY EQUIPMENT

06. ENGINES

07. GENERAL ENGINE MODIFICATION RULES

07.01. NON EPA/CARB PERMITTED MODIFICATIONS

07.02. MODIFICATIONS NOT PERMITTED (EPA/CARB)
OUTBOARD ENGINES

08. GEARBOX & WATER PICK UP

09. PROPELLORS

10. SCORING SYSTEM

10.01. 21-25 BOATS: 5 BONUS POINTS

10.02. 26-30 BOATS: 10 BONUS POINTS

11. CANOPY RULES & CREW SAFETY
12. DRAWINGS AND MEASUREMENT
13. REINFORCED COCKPIT AREA AND CANOPY
14. OTHER REQUIREMENTS

XCAT - TECHNICAL SPECIFIC RULE

Hull Dimensions Requirements etc.

The UIM Extreme Cat “XCAT” class strictly prohibits the use of mono hulls and inboard engines.

WPPA XCAT Class racing shall only permit the use of boats powered by Outboard engines.

Implementation will be by notice from UIM to all teams.

All equipment fitted to the race boat or used for the purpose of XCAT racing must be available commercially, any equipment or component not covered by the rules must be approved by the UIM before its use in XCAT

It is forbidden to use any form of live telemetry, Data loggers may be used but can only be accessed post race or Pole

01. HULL MEASUREMENT

All hull measurements are to be taken while the boat is ashore.

The length must be measured between perpendiculars of the external molded length of the boat, taken at the extreme bow and the rear most planning surface (the rear most part of the keel) including all hull planning steps (regardless of height), but excluding trim tabs, either fixed or movable.

Hull extensions beyond the transom shall be deemed to be “fixed rim tabs” and shall not be included in the measured length.

Hull extensions added to the bow and/or any extending parts, rubbing strakes, fenders, outboard motor spacing brackets, stabilising trim tabs and rudder assemblies are not to be included in the measured length.

When defining “extreme bow”, any form that constitutes the bow and its construction and contributes to the performance of the boat shall be included in the

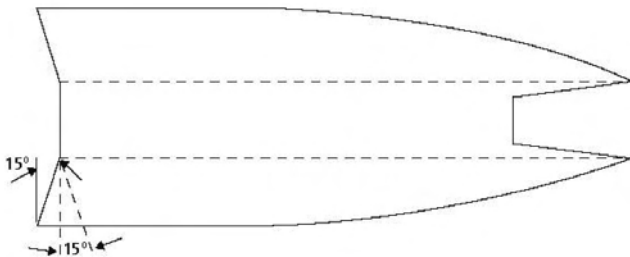
measured length. Any other attachment that is added to meet a minimum length requirement shall not be included in the measured length.

Should the trailing edge of the planning surface of a hull or sponson terminate with an angle greater than 15° measured between the keel and outer chine in plan view (birds eye view), such excess shall not be included in the measured length.

The beam measurement shall be taken at the widest part of the moulded hull, excluding any extending parts, rubbing strakes and/or fenders.

Trim tabs, either fixed or moveable, shall not exceed a length of 10% of the measured length of the hull.

Tunnel flaps shall not exceed a length of 10% of the measured length of the hull.



NOT GREATER HAN 15°

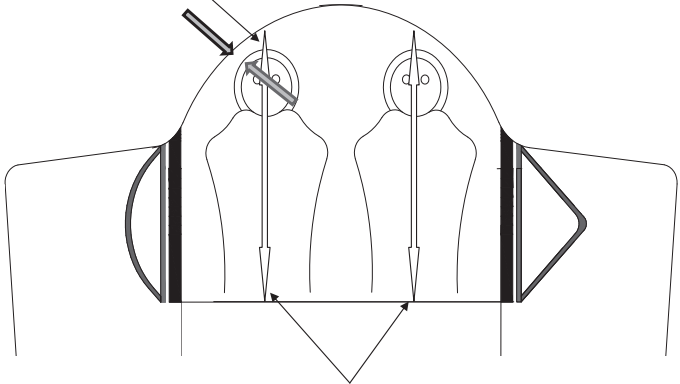
CATAMARAN IN PLAN VIEW

02. HULLS MINIMUM/MAXIMUM LENGTHS AND WEIGHTS

1. *Boats must fit in the WPPA supplied shipping container on the angled shipping cradle (Dimensions are available from the WPPA office upon request) to be eligible to race.*
2. *The minimum internal measured height of the cockpit from the floor to the inside of the cockpit roof, is to measure 1.10m. Measured in the centre line of the cockpit seats or the centre line of the pilot directly above his head from the floor to underside of the cockpit roof. 120mm*

head clearance directly above and at 45 degrees to the pilot's head, whilst the pilot is seated wearing all safety equipment is to be maintained at all times. Boats currently registered in Xcat are to be exempt from the new measurement of 1.10m providing they can maintain the 120mm head clearance.

Clearance between the top of the helmet and the inside of the canopy is to be a minimum of 120mm directly above and at 45 degrees to the pilot's head, with the pilot strapped in position



The minimum height inside the cockpit. From the floor to the cockpit ceiling. In the centre line of the pilots and measured directly above the back of the headrest, from the floor to the cockpit ceiling must be a minimum of 1.1 metres

3. It is mandatory that fully dimensioned drawings of all liquid containers and or tanks are given by each team before the first race to the UIM Technical Director. This drawing shall include measurements from the nearest reference point e.g bulkhead to show its location. Methods of filling and emptying must also be shown and the tank must be able to be checked if empty or not.
4. **No liquid ballast is permitted, whether in sealed containers or not. All ballast in the boat must not be movable/transferable whilst the boat is underway.** Liquid Ballasting is not permitted to be pumped in from the sea or pumped overboard during the Race or Pole position.
5. The UIM X- Cat class weight criteria for both Non EPA and EPA engines, is as follows:

Non EPA

4101 - up to 6000cc

Max HP — 600

EPA

up to Max 2 x 300hp

All EPA engines must be approved by the UIM Technical Director for eligibility if new to the XCAT series.

All weights and dimensions below refer to the use of both EPA and non EPA engines.

6. The weighting procedure will be defined as the scale of the day (further instructions will be published in the Race Instructions).

Min Length **8.50** m

Max length **9.50** m

Min weight is **2000 kg fixed for any boat length. Minimum weight must be reviewed together with the introduction of the new 4 stroke engine.**

Minimum weight is calculated as post **pole or** race, including all safety equipment (except the crew, their helmets and life vests), residual fuel in tanks will be included in the minimum weight. Slings carried in the boat during the race will count as safety equipment. The boat must be empty of water.

1. **No additional weight can be added after the green flag, for Pole position or the race.**
2. **No loose weights can be kept on the jetty during pole or race.**
3. **All weight that is to be added or removed after craning and before the green flag, must be shown to the Technical Director, before been**

installed. And if removed from the boat it must be brought to the crane area for inspection.

- 4. All water tanks must be completely empty upon craning in, this includes bilges and separate compartments.***
- 5. Boats weighing under their minimum weight post pole, will have their pole position times cancelled and will take last position for the following race.***
- 6. Boats weighing 30kg under their minimum weight Post race will have 30 seconds added to their time. Boats weighing more than 30kg under their minimum weight, will be disqualified.***

03. MINIMUM FLOTATION

All boats shall contain a minimum volume of buoyancy permanently installed in the hull.

The minimum volumes measured in litres are mentioned in the chart.

If a boat is constructed of wood, the wooden weight content of the boat may be accepted as buoyancy as per the following formula : Wood weight in kg x 2 = litres of buoyancy.

04. HULL/DECK REQUIREMENTS

All hulls racing must have side by side seating canopies with secondary escape hatches. Tandem and/or separate sponson type canopies are not permitted.

The deck must be able to bear the weight of a standing man (100 kg) at any point.

All cable and wiring deck exits should be watertight.

All boats shall be fitted with a well secured mooring cleat or tow bracket adequate for anchoring in a seaway and for towing at sea over a prolonged period.

The hook is to be fitted on a reinforced area designed for the purpose of towing a water laden hull.

Boats must be fitted with a minimum of three lifting eyes, either through deck or topside hull cleat type, suitably engineered to withstand lifting the boat fully equipped and fuelled with an adequate safety margin.

Each boat shall be equipped with a detachable strop assembly providing a single lifting point.

Boats must have fuel filling and fuelventing capabilities outside of the crew containment area.

Canopied craft are to have fuel tanks contained in sealed compartments from the hull and crew area to avoid leakage of liquid or vapour being released into the area being used by the crew. Fuel filling / fillers are to be outside of the crew compartments.

The fuel tank or tanks must be grounded to enable the discharge of static electricity.

Fuel tanks may be constructed as an integral component of the hull construction, or may be fabricated material suitably secured to the hull.

Purpose designed and constructed rubberised collapsible fuel cells may be used.

It is recommended that new boats are fitted with explosafe type fuel tanks.

05. MANDATORY EQUIPMENT

Any television equipment and/or telemetry equipment required by the UIM or its contractors or associated equipment and is deemed to be mandatory and must remain in place at all times and maintained for the intended purpose.

When additional electric pumps are fitted to the fuel supply a fuel cut-off switch for the pump shall be fitted within reach of either crew member.

It is recommended that all crews carry emergency harness cutters as used by emergency rescue services.

On board air systems are currently under review by the UIM. It is recommended that all crew carry secondary/spare systems for use in emergency evacuation.

Other items may be required as listed on the class scrutineering checklist.

06. ENGINES

Non conformance of engine rules, or deliberate modification will result in a minimum of disqualification.

Engine blocks and cylinder heads will be sealed with UIM seals, restricting the head, crankcase or powerhead from being opened or removed, for a minimum of two complete race weekends. Removal of the seals for any reason will result in teams taking last position for the next race to be raced by that team/boat. If more than 1 Team changes or removes the power head,

then the Team with the most championship points will go last for the race start.

To be eligible for racing, teams must submit **three** brand new Ecus. The Ecus will be held by The Technical Director. Teams will be given two random ECUs **for pole and race**; NO other ECU may be used for the race weekend. Non conformance, interruption or modification of the Ecu or signal will result in a minimum of disqualification. The race organisers/committee cannot be held responsible for any failure or defective Ecu or connections, causing interruption or loss of position to the race or racers

The Technical Director may at anytime request the engine ECU,s for recalibration or inspection. The recalibration may include a reduction in RPM or total power output. Any recalibration to reduce either RPM or power output on the grounds of safety may take place for all boats at any designated time in the race year or end.

Manufacturers new models EPA/CARB type engines may be used subject to the UIM **Cominoff** approval. Only those that closely match exiting horse power capabilities will be considered. No modifications will be permitted.

All engines must conform to those entered and accepted on the UIM measurement certificate.

Internal or external spraying of any substance into the airstream except fuel through the engines injection system is forbidden.

Engine jacking plates must not be able to be adjusted whilst the boat is under way, only mechanically adjusted jacking plates will be accepted.

07. GENERAL ENGINE MODIFICATION RULES

Revolution counters, tachometers, water pressure and temperature gauges may be fitted to the engines.

Thermostats of the cooling circuit may be removed.

The use of an ECU outside of the official homologated standard offshore equipment or factory offshore standard, or any other device is not permitted. The modification and/or reprogramming is strictly forbidden.

Re-boring and fitting of oversize pistons is allowed as made available by the manufacturer for that model.

The steering bar and brackets may be reinforced as a safety measure.

It is also permitted to modify the standard engine cowling in order to facilitate the fitting of steering assemblies, provided that any openings are sealed so the engine can not pull in any additional air.

The rubber mounts of the engine may be changed or substituted.

A spring may be added to the intake butterfly valve.

All external (not contained within the powerhead) studs, screws, nuts, bolts and their washers are free as well as the method of locking them, providing there is no performance advantage.

Manufacturers available counter rotating gearboxes may be used.

The use of thrust blocks mounted on the lower unit and or on the boat is permitted.

Power Trim: a properly engineered system for altering engine trim whilst under way is permitted.

Transom brackets may be reinforced.

Lighter flywheels as made available by the manufacturer may be fitted as per homologation tolerances.

07.01. NON EPA/CARB PERMITTED MODIFICATIONS

1. All measurements are to be taken from the UIM Homologation file 00491 for the Mercury 2.5 EFI offshore race engine.
2. All other components of the 2.5 EFI outboard engine not given a dimension or mentioned in the Homologation file or XCAT Rules, must remain standard (outboard engine is defined as all components from gearbox to top engine cover).
3. The motor type, and visual appearance must conform to the homologation file.
4. Measurements not quoted in the homologation file may not be altered. If a detail cannot be verified with the aid of the pictures in the homologation file, the part in question can be compared to a corresponding stock part. In such a comparison it is only possible to approximate the comparable measurements as the manufacturing tolerances are not available.
5. A component may be machined in order to reach a specific dimension, which is quoted in the engine homologation file. A component for which the weight is quoted in the homologation file may be adjusted down to the weight, by machining or grinding provided that the area where metal is removed is the same as is used by the engine manufacturer. This is known to be at the top or

at the bottom, or both for piston connecting rods. Balancing of one or all piston connecting rods is permitted, as long as this is in accordance with manufacturer's procedures.

6. Polishing of connecting rods is not permitted.
7. When the volume of the combustion chamber is specified in the homologation file, the cylinder head may be faced to achieve the volume allowed.
8. When ports in cylinders are adjusted to the dimensions specified in the homologation file, material may only be removed in the specified opening to a depth of 10 mm, to match the shape of the original adjacent connecting passage (channel) outside the adjusted port opening. The dimension is for blending of the port opening to the port passage only.
9. The measurement for the cylinder wall is from the inside of the liner where the piston runs to a total 10mm depth or length after grinding/blueprinting.
10. The Blueprinting/grinding dimension gives no allowance to reshape or deepen a port that is not 10mm deep to a depth of 10mm (original depth must be maintained).
11. The 10 mm blending rule applies to all openings given a dimension and tolerance on the homologation file. ***With the exception of the reed valve block, this must remain standard and cannot be machined or ground.***
12. It is permitted to replace lubricating systems using oil tanks and oil pumps with lubrication oil mixed into the fuel. Caps or covers in the motor cover cannot be removed to provide extra air inlet.
13. The fuel connector in the lower cover may be removed and the fuel hose from the fuel tank connected directly
to the fuel pump. If this is done, the opening left after the connector and around the fuel hose must be sealed to prevent extra air entering inside of cover.
14. Fuel flow regulator ***must not be*** substituted. The original manufacturers fuel flow pressure must be in place and unchanged.
15. Mercury 2.5 EFI the exhaust plate may be removed.
16. Fuel must not be chilled or cooled in any way.
17. ***Carbon reed valves may be used, they must be at least the minimum thickness quoted on the homologation sheet.***

07.02. MODIFICATIONS NOT PERMITTED (EPA/CARB)

OUTBOARD ENGINES

The following modifications are expressly NOT permitted.

- Blueprinting' of the powerhead.
- Alteration or modification of any powerhead components.
- Removal of material from the powerhead for any reason other than recognised (by the manufacturer) repair.

08. GEARBOX & WATER PICK UP

The water intake may be repositioned; it must however remain on the gear case torpedo. The width or height of the gear case torpedo must not be changed. The length fore and aft may be increased. The rear edge of the water intake opening must not be further back than 30% of the total length of the modified gear case torpedo.

The original intake to the circulation pump may be filled in and faired.

For cooling purposes it is allowed to add material in front of the union leg, as long as the union leg width is not increased, except where the union leg meets the lower side of the anti-cavitation plate.

The modification shall be for cooling purposes only and shall form a radius. The radius shall be lengthwise parallel to the anti-cavitation plate.

It is also permitted to add material to the centre section exhaust housing for the purpose of reducing noise.

Remote water pick-ups are not allowed.

Bearing carriers and propshafts may be changed from original.

All craft must be able to be maneuvered ahead and astern and have neutral capability, operated by controls at the helmsman's position. This facility must be a permanent installation and able to be demonstrated as practical at any time.

09. PROPELLERS

The use of forged propellers is strictly prohibited. Maximum number of permitted blades is 5.

10. SCORING SYSTEM

Unless otherwise specified, the following scoring system is to be adopted:

1	-	400 pts	11	-	22 pts
2	-	300 pts	12	-	17 pts
3	-	225 pts	13	-	13 pts
4	-	169 pts	14	-	9 pts
5	-	127 pts	15	-	7 pts
6	-	95 pts	16	-	5 pts
7	-	71 pts	17	-	4 pts
8	-	53 pts	18	-	3 pts
9	-	40 pts	19	-	2 pts
10	-	30 pts	20	-	1 pt

For events with more than 20 entries, the following bonus points should be given, so that all drivers can get points.

10.01. 21-25 BOATS: 5 BONUS POINTS

Position 1) to 20) : should all have 5 extra bonus points

Position 21 : 5 points

Position 22 : 4 points

Position 23 : 3 points

Position 24 : 2 points

Position 25 : 1 point

10.02. 26-30 BOATS: 10 BONUS POINTS

Position 1) to 20) : should all have 10 extra bonus points

Position 21 : 10 points

Position 22 : 9 points

Position 23 : 8 points

Position 24 : 7 points

Position 25 : 6 points

Position 26 : 5 points

Position 27 : 4 points

Position 28 : 3 points

Position 29 : 2 points

Position 30 : 1 point

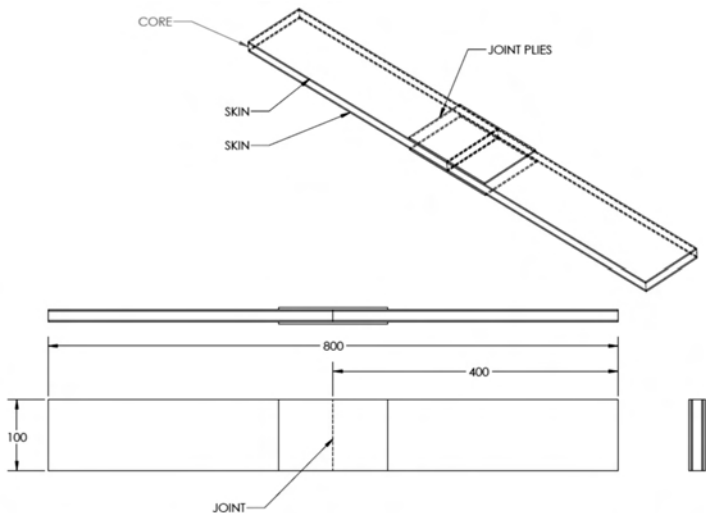
11. CANOPY RULES & CREW SAFETY

1. All XCAT boats must be equipped with a Reinforced Canopy/Cockpit, it is defined as a containment area for the crew and should be constructed as an integral part of the boat or suitably attached to boat if a drop in cell is fitted this should be complete with reinforced shield/crunch zones. This Reinforced Cockpit Area must be designed and constructed to a specification capable of withstanding the forces of a water impact when running at the highest design speed of the boat.
2. ***All canopies in XCAT boats manufactured after January 1, 2014 and all replacement canopies manufactured or installed after January 1, 2014 must be built by registered UIM cockpit builders. Constructors wishing to be registered must submit panels for test according to the following standards. Constructors meeting these standards will be registered as UIM registered cockpit builders.***

UIM XCAT COCKPIT/CANOPY TEST STANDARD

Sample Construction Requirements

- q. ***Sample quantity must be 3, one of which must include a sample of the joint/bonding of the cockpit parts, transverse to the length of the sample.***



- r. **Trimmed sample size must be 100 mm \pm 1.0 mm wide x 800 mm \pm 5.0 mm long, with the width being parallel.**
- s. **For fibre orientation the 800 mm length is to be parallel with the centerline of the boat.**
- t. **Sample must be laminated on a flat surface using the same manufacturing process, materials, and fibre orientations as the intended homologated cockpit construction.**
- u. **The sample must have a uniform thickness with no core crushing along any edges.**
- v. **The sample must have one moulded face and the other face being unmoulded, the moulded face will be taken as being the external surface of the cockpit during testing.**
- w. **The sample must be representative of the thinnest lay-up of the cockpit/canopy (excluding the various flanges for windshields, hatches etc.).**

- x. *The sample and cockpit must be manufactured using balanced or unbiased materials.*

Sample Test Method

- g. *The sample will be supported across the full width perpendicular to the 800 mm edges by two parallel 25 mm Steel bars at a distance of 500 mm apart. The load will be applied equally through two 25 mm Steel bars, each a distance of 167 mm parallel from each support.*
- h. *The moulded face of the sample will have the load applied and the unmoulded face will support the sample.*
- i. *The load will be applied at 0.4 mm/sec and the deflection will be measured at the two 25 mm Steel bars applying the load within 2 minutes.*

Sample Test Requirements

- g. *The sample when loaded with a force of 10000 Newtons must have no more than a maximum deflection of 25 mm without the sample failing.*
- h. *The sample weight in gm/sq m will be calculated, skin thickness and sample thickness will be measured to enable inspection and comparison of damaged homologated cockpits/canopies.*
- i. *Further non-destructive test analysis methods may be used to compare test samples with registered cockpits during the life of each cockpit/canopy.*

Sample Manufacturing Information Requirements

- r. *Ply laminating sequence (stating which ply is the moulded face).*
- s. *Ply materials.*
- t. *Ply weave styles.*
- u. *Ply material weight in gm/sq m (dry weight ie. Without resin).*
- v. *Ply orientation (where 0° is parallel with the 800 mm edges).*
- w. *Core material and density in lbs/cu ft or kg/cu m.*
- x. *Manufacturing method (stating vacuum, pressure, and temperature).*

- y. **A 100 mm x 100 mm sample of all materials used (resin samples not required).**
- z. **The completed questionnaire for offshore cockpits (available on the UIM web site) along with the supporting analysis for the question on “Primary Structure Strength”.**

Samples as per sample construction requirements must be sent to the UIM appointed person.

- 3. All new boats built after January 2010 must conform to the current UIM rules, All current boats to be assessed if water deflectors and crunch zones can be fitted. Any boat failing to carry out the Technical recommendations will not be eligible to race. All other canopy rules apply to the current boats.
- 4. New build manufacturers should contact the UIM for any planned rule updates due to the development program of canopy structure and safety designs.
- 5. For design and construction, an impact pressure of **167** tonnes per square meter **shall** be met or exceeded, with the highest importance given to protecting the crew in the event of severe accident. Any specification or safety device over and above the requirements will be assessed and supported as required.
- 6. The extremity of a Reinforced Cockpit comprises of a canopy, forward and rear bulkheads, sides, floor, and water deflectors. Additionally the cockpit is to be protected by a reinforced shield fitted into the boat surrounding the cockpit with a cavity between to form a 'crunch zone'.
- 7. Mandatory Water impact deflectors surrounding the cockpit, designed with a shape and structure so that the water is diverted around the cockpit and away from the crew in the event of the boat being breached to be fitted. There shall be no openings within the exterior confines of the impact shield/crunch zone allowing the forced ingress of water.
- 8. A continuous fitted structural framework inside the cockpit must be installed reducing the unsupported panel area sizes of the cockpit cell lining. The framework will incorporate roll bars fore and aft of the hatch with extensions to support the screen aperture divisions, the rear bulkhead directly behind each seat and the top escape hatch flange. The flange is to be a minimum of 25mm width (New build boats must have a minimum of 50mm) measured across the flange recess, with the hatch opening measuring 0.55m by 0.825m at the widest points. The canopy apertures should be cut with all corners having a radius of minimum 25mm. The radius should be constant and have a smooth finish to relieve stress. The canopy aperture must have a 20 mm

wide (minimum) fluorescent orange band around the opening. All boats are recommended to use the double flange method of installation as per the graphic below. All new build boats 2010 onwards must use the double flange installation.

9. There must be a minimum of 1 compression strut installed to support the canopy.
10. The main hatch being directly above the crew's helmets and its supporting flange must have further reinforcement to maintain its shape under stress and be able to withstand the impact of the water and retain its ability to function. The hatch thickness must be maintained throughout and not reduced at the flange where possible. The hatch should be protected by water deflectors incorporating internal and external method of prizing open the hatch caused by water pressure or binding to assist in emergency underwater rescue and escape.
11. The hatch shall be fitted with a catch which has a positive open and positive close mechanism and should hold the hatch against lateral forces. These catches shall be able to be opened from both inside and outside the cockpit and must have a second emergency mechanism to allow the rescue team to easily remove the hatch from outside if necessary. The hatch should be fitted with hinges with large bases and backing plates to spread the load and short release pins. This is important, because long pins invariably bind the hinge. It should be considered that the hinge centers be as far apart as possible. The release mechanism or hinges must not encroach within the canopy aperture area, and must not in any way hinder the exiting of crew members when fully race fitted. It is recommended that the catch have at least 2 locking points as far apart as possible, whilst still maintaining head clearance. It should be operated by a single handle with the addition of individual mechanisms internally in case of jamming. The hatch must be removable when the quick release hinge pins are removed. The locking mechanism must protect against both forward and aft stuffing
12. Mandatory secondary tunnel escape hatch, it is strongly recommended that polycarbonate windows are applied to the tunnel hatch to help illuminate the cockpit for ease of escape. Alternatively a waterproof light which is activated by water and/or being inverted with its own power source is fitted.
13. Canopy hatches and release handles, must be painted fluorescent orange or have a fluorescent orange background panel to identify them, with directional arrows to indicate the method of opening must be provided both inside and out.

14. There should be one or more divers' grab handles fitted to the outside of the top hatch.
15. The clearance between and around the crews helmet and the canopy must be a minimum of 120mm.
16. It is mandatory to close the canopy hatch, and for the hatch to remain closed during all racing and practice.
17. Raised sections above deck level, including hatches and openings, connected to the canopy, at least to the extent of the mandatory water deflection zones must be structural and comply
18. With the 'Reinforced cockpit area' rule. Any hatch within this area must have the same strength as the main cockpit hatch.
19. Cockpit seating must be side by side seating only.
20. The Cockpit should have flood tubes or other means of flooding the cockpit to equalize the pressure quickly in an accident. The floor of the cockpit should be as airtight as possible when in an upturned position. It is mandatory that sufficient buoyancy is provided in the boat, or in the material used for its construction, to ensure that the boat floats if capsized or holed, the hull should float as parallel with the surface of the water as is practical, to help in rescue accessibility. If extra buoyancy is needed, the buoyancy system described by the designer should be verified by the Measurer. This added buoyancy must be in at least four separate flotation units.
21. The various components that constitute the Reinforced Cockpit shall be properly maintained to ensure reliable operation of all components, with emphasis being placed on the canopy release mechanisms, emergency air supply and restraint systems, in accordance to the UIM cockpit guidelines.

Windscreens

1. Polycarbonate (Acrylic will not be accepted) areas are strongly recommended to be as small as possible, while still maintaining that the pilot and co-pilot have clear, safe and undisturbed visibility ahead at sea level whilst racing. it is strongly recommended that these polycarbonate areas are built using 12 mm thickness, or more.
2. The combined visibility the pilot and co-pilot must be through a horizontal arc of 225 degrees (112.5 degrees either side of the centre line of the boat).
3. These polycarbonate panels are to be recessed into the composite structure and should be bonded using a suitable bonding agent, and/or "bobbins" (Bonding the Windscreen is the recommended method of fitment. Boats built

after January 2010 must use bonding only, to fit the windscreen no bobbins will be allowed)

4. It is highly recommended that there is also a through bolted outer flange for the fitting of the polycarbonate panels.
5. Screen flanges should be a minimum of 50 mm, if bobbins are to be used they should be fastened every 100 mm it is recommended to use metal bobbins” with heads, as opposed to the recessed plastic type.
6. The outer polycarbonate area of the flange fitting must not be painted, so that the measurer / scrutineer may monitor any discrepancies.

Seats and Seatbelts Side of head protection

1. Cockpits must be fitted with **rear and side** of head protection for each crew member. This must be an integral part of the seat, which must be attached directly to the structure of the Restraint Compartment. The head protection must be a minimum of 0.2m wide and extend at least 80% of the height of the safety helmet as worn by the crew whilst in the normal seating position. There must be a minimum of 0.12m vertical and lateral clearance between the canopy and each of the crewmembers when in the normal seating position.
2. The Restraint System must consist of a 5 or 6 strap harness and should utilise a 75 mm lap belt, a 50 mm strap over the shoulder harness rated at 4,100kg (9,000 lb.) and grommeted to prevent chafing or cutting of the belt. Harness straps must be attached directly to the cockpit structure. Those straps close behind the driver’s head and neck must be 100 mm to 150 mm apart at point of attachment. The shoulder harness should be installed at 90 degrees to the spine at shoulder line to minimize compression injuries under high “G” loading. All straps must be free to run through intermediate loops or clamps/buckles. All anchor point bolts must be fitted with backing plates of 10cm minimum width. The driver harness attachment bolts in reinforced cockpits must consist of minimum grade EN8 bolts, with an 8 x 1.25 mm thread and locked nuts. There must be a spacer and plain washers on each bolt. The spacers must be glued to the cockpit structure. Intention of these spacers is to prevent buckling of surface material near bolts. This always leads to local delamination which easily spreads out over cockpit structure, when it is under stress.
3. On the sides of the structure, which has to take up the force on the attachment bolts, there must be a stainless steel plate (washer of minimum 3 mm thickness and 100 cm² area).
4. When using seats with suspension, and therefore not using a bulkhead restraint anchorage, drawings must be lodged with the National Authority of the measurer and approved prior to boat measurement.

5. All restraint systems must have a common method of release. The single lever method (sometimes called the NASCAR type) or rotary type, are both acceptable restraint release systems. Both types of restraint release must be examined for satisfactory operation by the scrutineer before every race.
6. The shoulder harness should be installed 90 degrees to the spine at shoulder line to minimise compression injuries and the high "G" loading. 75 mm minimum/maximum to Centre line of Lap Belt at Seat Back, Seat Bottom junction. Lap Belt should continue in straight line to anchorage.

12. DRAWINGS AND MEASUREMENT

Drawings of the Reinforced Crew Canopy/Cockpit to be supplied will include the following

1. The type of Canopy, Plan, side and elevation.
2. The Bulkheads and buoyancy system
3. Restraint System anchorage Points
4. Canopy aperture dimensions
5. The type and method and construction of release devices.
6. Material specification of the transparent areas.
7. Water impact deflectors and crunch zones
8. Material mechanical property data of the canopy, hatch, cockpit and crunch zones, running surfaces should be provided in support of the drawings to establish a database for future assessment and analysis.

Prior to Boat Measurement the drawing and material specifications shall be sent to the Measurer requested to measure the boat. On completion of measurement, the drawings and material specifications called for by the designer shall be lodged with the measurers National Authority before they issue a certificate of compliance and measurement. Drawings must also be submitted to the UIM for all entries to UIM races prior to racing.

The construction, strength safety and conformity to meet and exceed the recommended requirements is the sole responsibility of the boat owner and or boat builder. All points of impact incidence to the canopy, hatch and screens, (as being a primary impact area) cockpit and crunch zone extremities should be afforded the diligence deserved in protecting the crew. Any scrutineering or measurement by the UIM officials is not condition/construction survey.

Reference: Also 44.01 MEASUREMENT CERTIFICATE.

13. REINFORCED COCKPIT AREA AND CANOPY MANDATORY REQUIRMENTS

1. One single air supply (not oxygen) and a bottle will be provided for each riding crew member. The air supply must be securely fixed adjacent to, or on each one of them. It is recommended that sufficient air be provided in each individual bottle for ten minutes.
2. Air bottles must have a pressure gauge fitted for visual checking at pre-race scrutineering. This gauge should be filled with liquid and be at least 5 cm in diameter for easy reading.
3. Air supply bottles shall be "Turned On" before starting a race or taking part in practice and/or testing. All air hoses to be secured and mouthpieces to be placed within reach of the pilots and mounted on retainers
4. Diving Masks for each crew member, stowed securely and accessible.
5. A minimum of 1 belt cutter or knife is to be secured in the cockpit
6. Stop buttons/switches located in the cockpit area, immediately accessible to the crew and rescue officers. The stop buttons/switches must be identified by a fluorescent colour. These switches must shut off all fuel pumps as well as the ignition circuit.
7. A White High Intensity Strobe Light fitted to indicate "coming off the plane". The strobe light must be able to be operated by both crewman, and should be operated if a problem occurs, to enable any following race boats to take avoiding action. The strobe light shall be mounted on the top rear of the canopy. This strobe light may also be used as a substitute for the orange retirement flag when returning to port under reduced power.
8. A quick release steering wheel must be fitted on all boats but all pilots must be able to exit the cockpit without removing the steering wheel.
9. Rear view mirrors are mandatory, as well as a method of cleaning the canopy whilst underway.
10. Two fire extinguishers, each a minimum of 2kg must be carried and be readily accessible to the crew. All crew containment areas of inboard engine canopied boats must be fitted with a carbon-monoxide alarm.
11. Racing Vests - the efficiency of the racing vest is a matter of the exclusive responsibility of the wearer. Every crew member whilst on board must wear a

rating vest during the practice runs and throughout the race. Racing vests must be colored high visibility orange or yellow. The racing vest must have epaulets/handles to help extract crew from the boat. The racing vest must have crutch straps or a method of ensuring that the vest does not “ride up”.

Notes

“Cool Suits” - may contain any proven safe fluid or gas cooling agent other than Freon.

Eye protections - must be constructed of shatterproof material. Eyeglasses shall not be accepted as eye protection.

The Race Director - or Technical Director may prohibit use of any equipment he deems unfit for service.

Violation - of safety rules will result in a penalty assessed by the Race Director in conjunction with the Medical and Safety.

14. OTHER REQUIREMENTS

Non-compliance - The Technical Director has the authority to deny entry to any race boat subject to these rules that has non-compliant cockpit safety systems. The Technical Director also has the authority to allow a non-compliant boat to compete, provided that after consultation with the Technical Director, the Technical Committee determines that the intent of the guidelines has been met and that the safety of the riding crew and fellow competitors is not jeopardised.

Intercoms - Locking boat intercom plugs connecting helmets to racing craft are expressly prohibited. Plugs must disconnect easily, placing no additional stress on competitors’ necks. Inflexible metal or plastic intercom microphone booms are strongly discouraged.

Radios - A licensed ship-to-shore radiotelephone must be permanently installed in an unexposed location with a minimum power input of twenty-five watts with international marine VHF channels available and any channel designated by the Race Committee as being essential for race communications.

Bilge Pumps - Two (2) bilge pumps, at least one of which it is recommended shall be hand-operated. Electric bilge pumps are to be automatic and wired so as to maintain operation independent of the battery cut-off switches.

It is recommended that two Coast Guard Approved fully charged fire extinguishers with metal pull rings and an indication gauge showing amount of charge secured in a position are readily accessible to the crew.

Cleats - All cleats and other deck hardware must be recessed or protected.

Non-Skid - It is recommended that non-skid material be installed on the deck surface surrounding the cockpit to the satisfaction of the Chief Safety Inspector (Clear non-skid is available).

APPENDIX 1 **RECOMMENDED SAFETY PROCEDURES AND** **GUIDELINES**

RECOMMENDED SAFETY PROCEDURES

01. GENERAL REQUIREMENTS
02. TECHNICAL RECOMMENDATIONS FOR SAFETY BOATS
03. INTERVENTION PROCEDURE

SAFETY GUIDELINES

SAFETY

LAND SERVICES

- A. DRY PIT AND WET PIT AREAS
- B. FUELLING AREA
- C. HELICOPTER AREA (IF REQUIRED)
- D. HOSPITALITY AREA

ON WATER OFFICIAL TIME LAND SERVICES

SEA SERVICES

- E. RESCUE BOAT
- F. TOW BOAT
- G. FIRE BOAT
- H. COURSE PATROL BOATS
- I. FINISH BOAT
- J. COURSE CONTROL BOAT
- K. CONTROL BOAT

ACCESSORIES AND SPECIAL EQUIPMENT

SPECIAL INFORMATION TO BE PROVIDED BY THE LOC

RECOMMENDED SAFETY PROCEDURES

Minimum requirements to run of an XCAT Series Grand Prix

01. GENERAL REQUIREMENTS

- 01.01. the minimum depth of the water for the race course area is 4 meters in the turn mark; this depth can be reconsidered by the Safety Officer, in case of bad sea weather condition and or in case of particular geographical local condition;
- 01.02. the logistics point on land where safety boats and ambulances are located must not be further than 2 nautical miles from the furthest point of the race course;
- 01.03. the mooring post for the rescue boats has to allow for a parking area for ambulances;

02. TECHNICAL RECOMMENDATIONS FOR SAFETY BOATS

- 02.01. The rescue boats should be:
 - a minimum of 7.5 metres long with free bow in order to have enough space for stretchers;
 - sides and/or stern can allow an easy boarding of stretchers and/or must have a system for the easy loading of stretchers;
 - cannot transport on board persons who do not belong to the crew or to the rescue group;
- 02.02. Considering that race courses have a maximum lap length of six nautical miles, the safety plan, coordinated by the Local Safety Officer with the assistance of the Local Medical Coordinator, envisages a minimum number of six rescue boats. This plan is created according to the grid method in order to be able to patrol an area of 1 mile for each rescue boat.

Following this method, the rescue boats will be implemented if the race lap is longer than 6 miles.

The Local Safety Officer is responsible for the repositioning of the safety Plan, according to the interventions and in order to cover the whole race course.

The plan is not sufficient if 50% of the available rescue boats are used in one or more interventions at the same time.

In this eventuality, the Local Safety Officer must communicate the fact to the Race Director in order to stop the practice, pole position and/or race.

- 02.03. Each rescue boat is assigned a Tow boat and they form the safety group positioned in the grid plan. The rescue boat is the group leader.

The group has to take its position according to the safety plan and it has to maintain the position by maneuvering the engine (avoiding the creation of waves), at a minimum distance from the course line of 200 metres and, in any case, in a safe position.

Every group has to be constantly in contact and has to listen to Race Control on the assigned radio channel. The driver's assistant is assigned to carry out this function.

03. INTERVENTION PROCEDURE

- 03.01. Every safety group, composed of one rescue boat and one Tow boat, has to be in the assigned position at least 30 minutes before the official start time for practice, pole position & race.
- 03.02. Every group has to check how the practice is going, reporting every anomaly, accident, etc. to Race Control.
- 03.03. It cannot act unless authorised by Race Control.
- 03.04. Once the group has received the authorisation from Race Control, and if towing is not specifically requested - in which case, only the Tow boat would intervene - the group moves towards the point requiring intervention. While the rescue boat approaches the accident location, the Tow boat, with the yellow flag raised, places itself about 100 metres from the intervention point, on the course line, in a safe and visible position, signaling the danger to the other race boats, and protecting the intervention by the rescue boat.

If it is a medical intervention, the operation is controlled by the doctor and/or paramedic on board the Rescue boat, who will take care of rescuing the patient(s) and report to Race Control regarding what further action needs to be taken.

Once the rescue has been completed, the rescue boat will return to the established mooring post following the re-entry procedure, while the Tow boat will control the accident area and clear the water of any floating debris.

At the end of its intervention, with the consequent report having been made to Race Control, the group will take up its original position for normal service.

- 03.05. During the operation, Race Control will reposition the inactive groups in order to cover the whole race course.
- 03.06. If, during an intervention, the operation commander requires the aid of other rescue boats, Race Control will send the appropriate group(s), including the fire service.
- 03.07. If simultaneous interventions require the use of more than 50% of the active groups envisaged in the safety plan, it will be down to the Race Director, via Race Control, to stop the practice, pole position and/or race. In this case, the order will be given to raise the red flag, thus initiating the Race Stopped/Pace Boat procedure.

SAFETY GUIDELINES

SAFETY

The Safety and Rescue Plan includes both land and sea services.

All services will be carried out by Local Committee crews together with relevant class and Military crews.

The Safety and Rescue system will be co-ordinated by the Race Control.

LAND SERVICES

The Land Safety System includes the medical, fire prevention, helicopter and hospitality area services.

The Land area is divided into four separate zones:

- Dry Pit and Wet Pit area;
- Fuelling area;
- Helicopter area;
- Hospitality area

The areas indicated above must be set up and equipped as follows:

A. DRY PIT AND WET PIT AREAS

A.01. Medical Services

The medical services will be carried out under the control of the Head of the Medical Committee, with:

- A.01.01. N.1 Standard Ambulance (“B” type), with doctor and paramedical crew, normal equipment.

This ambulance will be positioned close to Dry Pit, free to move, from 08.00 to 19.00 from arrival to departure of all vehicles.

A.02. Fire Prevention Services

- A.02.01. 3 Fire points each containing 3 Extinguishers of 30 KG in separate locations in the pit area.
- A.02.02. N.2 fire extinguisher operators.
- A.02.03. N.2 water pumps, positioned in crane area (one at each crane).

B. FUELLING AREA

B.01. Medical Services

- B.01.01 N.1 Reanimation Ambulance (“A” type), fully equipped and manned by reanimation doctor and reanimation paramedical crew.

This ambulance will be positioned close to Fuelling area, free to move, one hour before and until one hour after the official fuelling times.

- B.01.02 N.1 Fire engine with foam and a minimum of 2m3 extinguishing capacity, fully equipped and manned.
- B.01.03 N.2 fire extinguisher operators.

During fuelling operations, the area will be closed and controlled by the security crews.

C. HELICOPTER AREA (IF REQUIRED)

C.01. Medical Services

According to the local Law.

C.02. Fire Prevention Services

According to the local Law.

D. HOSPITALITY AREA

D.01. Fire Prevention Services

- D.01.01. N.1 fire areas composed by 6 extinguishers of 10 Kg each, with locations clearly indicated.

ON WATER OFFICIAL TIME LAND SERVICES

- N.2 Reanimation Ambulance (“A” type), fully equipped and manned by reanimation doctor and reanimation paramedical crew.

These ambulances will be positioned free to move one hour before and until one hour after the official race, practice and pole position times.

Note: The LOC can also use the fuel ambulance during the official race times.

SEA SERVICES

Safety and Rescue Services on Racecourse.

E. RESCUE BOAT (Mandatory for Practice, Pole Position & Race)

- E.01. A minimum of n. 6 Rescue Boats min 7 m, sufficient space in order to accommodate stretcher, with crew, n. 1 reanimation doctor or specialist paramedic and n. 2 divers for each boat, fully equipped for reanimation at sea (see following details).

F. TOW BOAT

(Mandatory for Practice, Pole Position & Race)

- F.01. N.6 Boats with crew, equipped for towing in order to tow up to 5 tons (see following details).

G. FIRE BOAT

(Recommended for Practice, Pole Position & Race)

- G.01. N.2 Boats with crew, equipped with anti fire system with foams and water pump.

H. COURSE PATROL BOATS

(Mandatory for Practice, Pole Position & Race)

- H.01. N.6 / 7 Military boats for external course patrol.

I. FINISH BOAT

(Mandatory for the Race & Pole Position)

- I.01. N. 1 Boat Fly bridge or other suitable craft for finish race procedure/pole position run (minimum 9m), with crew and Jury/Course Officials.

J. COURSE CONTROL BOAT**(Mandatory for the Race & Pole Position)**

- J.01. N.1 Boat each turn point with crew and n. 2 Course Officials (minimum 7m).

K. PRESS BOAT**(Mandatory 1 day before the first practice and until the GP day)**

- K.01. N. 1 Boat for XCAT Press during whole period, available with driver at disposal of the Series Promoter (minimum 7m - high speed).

ACCESSORIES AND SPECIAL EQUIPMENT**• LOC DIVERS' EQUIPMENT**

The only special request for the divers is the supply of 5 litre oxygen cylinders.

• LOC TOW BOAT

- XCAT flag;
- Nautical radio system;
- 1 Yellow flag;
- 5 lengths of rope (15m each);
- 2 Blankets;
- 2 Marker buoys.

• LOC COURSE CONTROL BOAT

- XCAT flag;
- Nautical radio system;
- 1 Yellow flag;
- 1 Red flags;
- 1 Red flares.

• LOC FINISH BOAT

- XCAT flag;
- Nautical radio system;
- 1 Yellow flag;
- 1 Red flags;
- 1 Red flares;
- 1 Chequered flag.

• LOC RESCUE BOAT**CREW**

- Driver;

- Assistant;
- 1 Reanimation Doctor or Specialist Paramedic;
- 2 Paramedic Scuba Divers

LOC RESCUE BOAT EQUIPMENT

- XCAT flag;
- Nautical radio system;
- 1 Yellow flag;
- Oxygen
- 2 Collars
- C35
- Several Splints
- Pair of Scissors
- Floating Stretcher
- Thermal Blanket
- Emergency Medical Bag (see following details)

SUGGESTED BAG'S CONTENTS IN DETAIL (FOLLOWING THE NATIONAL LAWS)

Venflon 14 g	Ipnovel 15mg/3ml	Effortil vials 10mg/1ml
Venflon 16 g	Cortop vials 500mg/5ml vials	Ebrantil vials 50mg/10ml vials
Venflon 18 g	Gluc 33%/10ml	Sodium bicarbonate 8.4%/100ml
Butterfly 19 g	Mgso4 vials 1g/10ml vials	Emagel 500ml/flac
Butterfly 21 g	Normal saline solution 0.9%/10ml bottle	Sodium chloride 0.9%/500ml
Arterial compressors with 19g Needles	Calcium chloride fl 1g/10ml	Corrugated tube
Silk bandage 2.5	Esmeron vials	Endotracheal tube 7.5
Syringes 5 millilitre	Silk bandage 2.5	Endotracheal tube 7
Syringes 10 millilitre	Lubricant	Endotracheal tube 6.5
Administration set with flow regulator	Laryngoscope	Endotracheal tube 6
3-way taps 5.5	Blades for laryngoscope Mac 3-4	Endotracheal tube
Mini tracheal set	Magil forceps	Disposable gloves, size M
Adult laryngeal tubes	Mayo cannulas in several measures	Adrenaline 1mg/ vials
Adult Ambu bag	Intubation stylet	Midarine (succinylcholine) vials
Adult hematometer	Atropine sulphate 0,5mg/1ml vials	
Adult ventilation mask	Xilocaina 2% 50ml flac	
Ventolin spray flc	Cordarone 150mg/10ml	
Diprivan 10mg/millilitre bottle		Revivan vials 200mg/

SPECIAL INFORMATION TO BE PROVIDED BY THE LOC

- 1) NAME OF PREWARNED HOSPITAL(s) WITH CONTACT NAMES OF DOCTORS AND TELEPHONE NUMBERS;
- 2) NOTIFICATION OF LOCATION OF SPECIALISED UNITS AT HOSPITAL(s) (Emergency surgery; Radiology with TAC; Resuscitation; etc.);
- 3) TIME TO COVER DISTANCES FROM THE PITS TO THE DESIGNATED HOSPITAL(s) VIA ROAD;
- 4) TIME TO COVER DISTANCES FROM THE PITS TO THE DESIGNATED HOSPITAL(s) VIA HELICOPTER;
- 5) FURTHER EMERGENCY BERTHING POINTS (IF ANY);
- 6) POSSIBILITY OF SECURITY PRESENCE AT SUCH BERTHING POINTS;
- 7) POSSIBILITY OF RESUSCITATION AMBULANCE AT SUCH BERTHING POINTS.

APPENDIX 2

ON-WATER PROCEDURES AND GUIDELINES

01. POLE POSITION PROCEDURE
 - 01.01. GROUPING
 - 01.02. POLE POSITION PROCEDURE
02. PIT STOP PROCEDURES
03. START PROCEDURE
04. RACE STOPPED/PACE BOAT PROCEDURE
 - 04.01. RACE STOPPED
 - 04.02. PACE BOAT PROCEDURE
05. FINISHING PROCEDURE

01. POLE POSITION PROCEDURE

01.01. GROUPING

All Race boats must be positioned on the wet pit pontoon 1 hour before the scheduled start time for Pole Position, unless otherwise stated in Race instructions or race bulletin. Boats to be positioned in Championship order.

01.02. POLE POSITION PROCEDURE

XCAT Pole Position session is mandatory with no minimum or maximum number or laps. Boats qualify in order of fastest recorded lap times during this session. Any boat failing to compete in Pole Position session will be placed after all the qualifying boats from the session in their championship order.

As soon as the green flag is displayed and the course is open, race control will call in championship order, each boat to give permission to enter the course. After this first call any race boat is entitled to request permission from Race Control on the designated VHF channel to leave the Race wet pit pontoon and enter the race course (a white flag may also be shown 10 minutes prior to start of pole position).

There is no maximum number of race boats on the course.

Any boat entering or leaving the race course must request explicit permission from Race Control via radio on the designated VHF channel and follow the In & Out procedure as given in the Race Instructions and/or Bulletin and/or Pilots' Briefing.

The course will be closed at the Race Instruction specified time min 45 minutes to 90 minutes maximum, the red flag will be raised and all boats on the course must exit the course following the In & Out procedure.

As a consequence of the strict time limitation, all race boats must commence a timed lap before the allotted minutes have passed so as to be entitled to finish this timed lap. Each boat must then proceed to the wet pit area, respecting the In & Out procedure.

The final Pole Position results will determine the starting order for all participating boats in the Grand Prix Race.

It is at the discretion of the Race Committee to establish whether, when entering the race course, the departure point will be the Wet Pit, or whether it is necessary to designate a Pole Position Milling Area. This will be indicated in the Race Instructions and/or Bulletins and/or Pilots' Briefing.

After the start time of the Pole Position, priority use of the cranes is at the sole discretion of the Technical Director.

No refuelling will be allowed during the Pole Position.

Under no circumstance during the Pole Position may any boat run outside of the Pole Position course area. Failure to comply will result in a fine of Euro 1,000 and/or disqualification.

Lap timing commences after the first passing of the timing gate.

After Pole Position all boats must be available for scrutineering at discretion of the Technical Director.

The result of the Pole Position will be determined by the fastest timed lap.

No part of the Pole Position is subject to protest.

In addition to the penalties determined by the Rules, in the case of missing or destroying a buoy, the time from the relevant lap will not be counted towards qualification; but does count as one of the allowed number of race laps.

In the event of bad weather or curtailment

In the event that the Pole Position has run for a minimum of 50% of the published duration the results will stand, any boats that have not chosen to run in that time will be classified after the qualifying boats in their Series Championship order.

The session is cancelled if is not to completion of 50% of the published time, in this case the Race start order will follow the Championship order. In case of new Teams

Entry, their race start position will be taken in base at the Championship / Race date entry list.

02. PIT STOP PROCEDURES

- Boats which wish to make a Pit Stop must follow the In&Out procedure outlined in the Race Instructions, Drivers' Briefing and/or Bulletin
- Boats returning to the Wet Pit pontoon must turn on their strobe light.
- If several boats are returning at the same time, use of the cranes will be designated by the Technical Director based on the order of the boats entering the Wet Pit pontoon.
- Boats must be at very slow speed at all times while in the harbour (no wake). Penalty for infringement: Disqualification and/or fine.
- Refuelling is not permitted.
- Only the official crane areas may be used during pit stops unless agreed by the Technical Director.
- No boats will be recovered during the duration of Pole Position, except for damaged boats and under the responsibility of the Technical Director.
- Cranes may be used to recover boats during the Grand Prix race period, but priority will be given to pit stops, except for damaged boats, and under the responsibility of the Technical Director.
- Craning time is fixed at ten minutes per Pit Stop. A boat can enter the craning area only when it is ready to be hoisted by the crane. Craning time starts from when the boat is attached to the crane by its straps and ends when the straps are released from the crane.

See also Event Rules Section H 31Craning and 32 Pit Stop Rules.

03. START PROCEDURE

All boats will be positioned in Pole Position order by the wet pit pontoon.

15 minutes before the start, a white flag will be raised in the wet pits and the drivers will take their positions in their cockpits with their attention focused on race boat VHF channel.

10 minutes before the start, the green flag will be raised and the Race Director / or Start Marshall will call by radio the race boats in Pole Position order; all boats

will then leave the pontoon accordingly, following the Start Boat to proceed on parade to the start.

All boats will follow the Start Boat at very low speed (max 10 knots) in Pole Position qualifying order. Where possible a parade lap will take place straight in to a rolling start, with no Flare.

When the Start Boat turns in the direction of the Start Line, then all race boats have to take their position at the start.

All boats will line up in Pole Position qualifying order as described. The Pole Position winner must be adjacent to and minimum 30 metres astern of the Pace Boat. All other boats must line up alongside the Pole Position winner in PP order and maintain a distance approx 30 metres between them. When line up it's complete, a yellow flag will be raised. When all boats are in position the green flag will be raised as soon as possible in the vicinity of the start chute.

A second start boat may be used where available to form an imaginary line between the 2 start boat transoms. In this instance the 30 metre rule will apply from both start boats.

If any of the race boats are out of Pole Position order by the Start, a long lap penalty will be applied.

The Start Chute may be marked by two buoys. All boats must pass through this Gate.

The start of the race is denoted when the Start Boat has raised the green flag. The green flag will be raised at approx 45 Mph and confirmed by radio. The start boat will not advise boats to speed up or slow down, this is the sole responsibility of the pilots.

All race boats must line up and remain at a constant speed in starting order – no 'runs' through the line up will be permitted. Penalty – Long Lap.

If a race boat starts before the yellow flag is raised, the penalty is Disqualification.

Once the green flag is raised and the race is started all boats are under ALL RACE RULES and all overtaking and rounding marks rules will apply.

It is the Team Manager's responsibility to guarantee the good functioning of the Race Boat's radio communications.

Penalties for infraction of the Starting Procedure are:

- Interference with starting procedure – Long Lap Penalty and if the driver had caused danger, a Yellow Card.

- Should the bow of any boat be in front of the transom of the start boat when the green flag is raised - Long Lap Penalty or time penalty 30 seconds.
- Starting on the incorrect side of the start boat or in front of the start boat - Disqualification.
- Damaging the position of another boat - Yellow card.
- See relevant diagram issued with Race Instructions.
- Infringement of the 30 meters rule will be penalized with one long lap penalty or time penalty as describe in the Race Instructions.

See relevant diagram issued with Race Instructions.

NB - Refer to the race course location, bad weather condition, or others, different start procedure can be applied by Race Director.

The Start Can be as follow:

1. Standard one line;
2. Standard one line with multi gates on the first turn mark;
3. Multi line;
4. Single Row

See Appendix 5 – Start Procedures Drawing & Clarification

04. RACE STOPPED/PACE BOAT PROCEDURE

All Races – In the event of a race boat capsizing racing will be immediately Red Flagged until the incident is under control. Pace boat procedure will then commence as per rule and/or race instruction.

04.01. RACE STOPPED

In the event that a race has been stopped with a red flag, the race time will keep on running.

If the race is stopped during the first lap or, in any event, before the first timed lap has been completed by the leading boat, the Restart will be effected in accordance with the normal Start Procedure and starting order as given in the Race Instructions and/or Bulletin and/or Pilots' Briefing.

If the race is stopped before the first timed lap has been completed by the last boat, all boats must proceed to the Pace Boat Milling Area. If the Pace boat raise Nautical Flag Code “Y” means follow the Pace Boat and further instructions will be communicated via radio race channel or visual signals.

In any case, whichever procedure is to be followed (normal Restart and/or Pace Boat), all boats must observe the Pace Boat Milling Area procedure as described below.

- The stopping of the race will immediately be communicated via radio (VHF channel as announced in the Race Instructions) and via timing computer.
- It is the responsibility of the Team Manager to inform his pilots that the race has been stopped.
- All boats must immediately slow down. No overtaking will be permitted from the stopping of the race/red flag.
- Slowly, each boat must follow the race course to the designated Pace Boat Milling Area where the Pace Boat is located and waiting.
- As soon as all race boats are assembled in the Pace Boat Milling Area they must follow the instructions from the Pace Boat:
 - If the Pace Boat shows the red flag, the race is definitively stopped and all boats must follow the Pace Boat into the wet pits.
 - If the Pace Boat raised the yellow flag and/or turns on the orange strobe lights, the Pace Boat procedure comes into force.

Refer also to Event Rules 35 Finishing Procedure 01, 02, 03.

04.02. PACE BOAT PROCEDURE

The pace boat and the location of the Pace Boat Milling Area will be announced in the Race Instructions and/or Drivers' Briefing and/or Bulletin.

- Once all race boats have reached the Pace Boat Milling Area and are assembled, and the Pace Boat raised the yellow flag and/or turns on the orange strobe lights, the race boats must line up behind the pace boat in the order of the classification of the lap prior to the stopping of the race. From this moment on, each single overtaking will be penalised with 1 long lap per boat overtaken. (3 boats overtaken under Red Flag = 3 long laps as penalty).
- Race Control will announce the line-up via radio and timing screen.

- As soon as all race boats have taken their correct position behind the pace boat, they must closely follow the course of the pace boat.
- From the moment that the race course is cleared, the pace boat will follow the regular race course (race lap) as announced in the Race Instructions and/or Pilots' Briefing and/or Bulletin.
- It is the responsibility of the pace boat/Race Control to restart the race. The pace boat will accelerate to a high speed. The race boats must continue to follow the pace boat in the designated order.
- The restart of the race will be indicated by a green flag and communication via radio.
- The race will be considered restarted either as soon as the pace boat, at any time, when the Green flag is raised and called on the Raceboat channel.

The green flag will be displayed and the Team Managers will be informed via radio and timing screen

Overtaking is only permitted after the restart.

In the case of a boat having technical or other problems during the restart procedure, the strobe light must be switched on immediately and all following boats are entitled to pass this boat without penalisation. If the strobe lights are not switched on immediately and the restart procedure is affected, the concerning boat will be penalized with 1 long lap / or additional Time.

If the boat is not able to perform this penalty during the actual race, the penalty will be valid for the next race in which the boat takes part.

05. FINISHING PROCEDURE

- 05.01. No Patrol Boat or Jury Boat shall be released from his station until the official in charge at Race Control has given clearance to that specific boat.
- 05.02. Once all the race boats have passed the Finish/Timing line, they must proceed to a safe area as defined in the Race Instructions.
- 05.03. Once all the boats have passed the Finish/Timing line, the race boats are free to return to the wet pits, following the In & Out Procedure in the Race Instructions and/or Bulletins and/or Pilots' Briefing.
- 05.04. The Winner, 2nd- and 3rd-placed boats will be led to the Wet Pits or other designated area as announced in the Race Instructions by the Start/Pace Boat.

APPENDIX 3

ON-LAND PROCEDURES AND GUIDELINES

- 01. PITS
 - 01.01. DRY PIT
 - 01.02. CRANING AREA
 - 01.03. REFUELLING AREA
 - 01.04. WET PIT AREA
 - 01.05. HELIPAD
 - 01.06. TIMING/TEAM MANAGERS' AREA
- 02. HOSPITALITY AND VIP AREA
 - 02.01. PODIUM AREA
- 03. OFFICES
 - 03.01. PROMOTER OFFICES
 - 03.02. XCAT OFFICIALS
 - 03.03. PRESS OFFICES
 - 03.03.01. PROMOTER XCAT Press - PR
 - 03.03.02. LOC Press Office
 - 03.04. XCAT TV / VIRTUAL SYSTEM
 - 03.05. MEETING ROOM
 - 03.06. PILOTS' BRIEFING ROOM

01. PITS

The PIT area comprises the areas on land and the quayside where the activities connected to the event take place.

The Pit area is made up of: Dry Pit; Craning Area; Refuelling Area; Wet Pit; Helipad; Timing area;

01.01. DRY PIT

The Dry Pit is the area where the race boats and workshops are situated.

The usable area required is c.3,000m² and it is requested that this be available from 3 days before the start of the race weekend until the day after the Grand Prix.

It is compulsory that this area be closed off and the entrances protected 24 hours a day by security guards (the type of pass which allows access will be advised by Race Promoter via panels displayed in the area).

The area must have sufficient space for the movement of trucks to and from the craning area (point 1.2) and the refuelling area (point 1.3)

The Dry Pit must be equipped with the following:

- a) 8 multiple (minimum dual) 220V electricity connections with a maximum charge of 50 kW;
- b) 8 multiple (minimum dual) water supply points;
- c) minimum of 4 x 200 litre containers for collecting oil;
- d) 1 rubbish bin for each race boat (minimum of 10), 200 litres in size;
- e) 1 fire extinguisher for each race boat (minimum of 10) and a minimum of 2 fire prevention officers;
- f) 1 forklift truck, minimum 3 tons (no operator);
- g) 1 van for transporting materials (no driver);
- h) adequate electric lighting;
- i) adequate toilet facilities.

01.02. CRANING AREA

The craning area is where the cranes for launching and haulage operations are situated.

The usable area required is c.1,000m² and it is requested that this be available from 00.00 on the day 2 days before the start of the race weekend until 24.00 on the day of the Grand Prix.

It is compulsory that the entrances to this closed off area be protected 24 hours a day by security guards (the type of pass which allows access will be advised by Event Promoter/Organiser via panels displayed in the area).

The area must have sufficient space for the movement of trucks to and from the Dry Pit (point 1.1) and the refuelling area (point 1.3).

The Craning Area must be equipped as follows:

- a) 2 x minimum 30 ton cranes with 20-metre range available for use and in position from 08.00 of the day before the race weekend until 24.00 of the day of the GP, with operators present during the official hours. The operators must also be on call for other activities, if necessary, at other times.

- b) 2 multiple (minimum dual) 220/380V electricity connections
- c) 2 water pumps, one by each crane, to be used to pump out race boats in case of accident
- d) a minimum of one 200 litre rubbish bin;
- e) 2 fire extinguishers for each crane;

01.03. REFUELLING AREA

The refuelling area is where the race boats fill up with petrol.

The usable area required is a minimum of c.200 m² and it is requested that this be available from 00.00 on the first day of the race weekend until 24.00 on the second day of the race weekend.

It is compulsory that this area be closed off and the entrances protected 24 hours a day by security guards (the type of pass which allows access will be advised by Promoter/Organiser via panels displayed in the area).

The area must have sufficient space for the movement of trucks to and from the Dry Pit (point 1.1) and the craning area (point 1.2).

The Refuelling Area must be equipped as follows:

- a) 1 fire engine equipped for combustibles with personnel on call during all official refuelling times (the service must start 45 minutes before the official period and last until 45 minutes afterwards); The fire prevention group must also be on call outside these hours;
- b) 1 resuscitation ambulance with medical staff and paramedic in attendance during all official refuelling times (the service must start 45 minutes before the official period and last until 45 minutes afterwards);
- c) 1 multiple (minimum dual) 220/380V electricity connection;
- d) a minimum of one 200 litre container for collecting oil/fuel;
- e) 4 x 30 Kg fire extinguishers;
- f) sufficient antipollution material.

01.04. WET PIT AREA

The Wet Pit is the area for the launching and haulage of the boats.

The usable area must be adjacent to the craning area and have sufficient space for the positioning of c.20 XCAT racing boats measuring 10 metres each.

It is compulsory that the entrances to this closed off area be protected 24 hours a day by security guards (the type of official pass which allows access will be advised by Promoter/Organiser via panels displayed in the area).

The area must be equipped with gangplanks, ladders, etc. which enable the authorised personnel to reach the pontoons.

01.05. HELIPAD

The helipad is the area from where the helicopters used for the race take off, land and park.

The entrance to this closed off area must be protected by security.

The usable area must be suitable for the use by 3 helicopters (e.g. EC 130 or similar).

As an alternative to a permanent helipad, it is possible for the Local Organiser to provide an area which can be used for this purpose on a temporary basis and request the "OPENING OF TEMPORARY HELIPAD" authorisation from the relevant Civil Aviation Authority.

In this case, the Local Organiser must arrange for the temporary area to be set up in the following way or, in any event, in accordance with all legal requirements.

01.06. TIMING AREA

The Timing Area is where the timing of the event is done and from where the participating Teams are controlled.

It is situated close to the Finish Line.

The usable area required is a minimum of c.20 m x 4 m and it is requested that this area be available from 00.00 on the day 3 days before the start of the race weekend until 24.00 on the day of the Grand Prix.

It is compulsory that this area be closed off and the entrances protected 24 hours a day by security guards (the type of pass which allows access will be advised by Promoter/Organiser via panels displayed in the area).

The Timing Area must be equipped as follows:

- a) Use of Power Electricity: 10 multiple (minimum dual) 220V electricity connections; Min 10 KW;
- b) 4 MB upload Internet Connection;
- c) 3 rubbish bins;
- d) 2 fire extinguishers;

02. HOSPITALITY AND VIP AREA

The Local Organiser should reserve an area to be used as Hospitality and VIP Area (for approximately 100-150 persons (Teams – Organiser- LOC- Sponsors, plus Guests).

It is requested that this be available from the day before the race weekend until the end of the race weekend (opening from 09.00 am till 19.00 h).

This area has to be organized with catering service for Teams-Organiser- LOC-Sponsors and Guests.

02.01. PODIUM AREA

The Podium Area is where the prize-giving takes place.

This is normally situated inside the VIP Hospitality area and where VIP activities are run.

The usable area required is c.100 m² and it is requested that this be available from 00.00 on the day before the start of the race weekend until the end of the prize-giving ceremony which follows the Grand Prix.

This area must be protected by security guards for the duration of the prize-giving ceremony (the type of pass which allows access will be advised by the Promoter via panels displayed in the area).

The Podium area must be equipped as follows:

- a) 1 multiple (minimum dual) 220V electricity connection
- b) 1 stage, minimum 8m wide, 6m deep, on which backdrop is to be set up (provided by Promoter/Organiser) steps up to stage
- c) 1 PA system with radio-microphones (2)
- d) Table to display trophies

03. OFFICES

The Offices are a collection of areas requested for the execution of the technical and administrative work relating to the event.

They comprise: Promoter Operations Office; XCAT Officials; XCAT Press/PR; XCAT TV; XCAT Virtual System; Press Office; Meeting Room; Briefing Room;

Overall, they can be made up of offices and/or portacabins/stretched flexible structures set up for the purpose.

The Offices are normally located near the Pit Area.

They should be cleaned, with waste paper baskets to be emptied, on a daily basis.

03.01. PROMOTER OFFICES

Made up of 1/2 rooms totalling c.30m²:

- a minimum of 5 workstations, made up of desks and chairs with sufficient electrical sockets and internet connections (either via cable or wireless); one fax machine; one phone line; one colour photocopier with sorter; one colour and several b/w printers; also area to be used for private meetings.

03.02. XCAT OFFICIALS

Made up of 1 room totaling c.20m².

An office with a minimum of 8 workstations, made up of desks and chairs with sufficient electrical sockets and internet connections (either via cable or wireless); one fax machine; one phone line; one colour photocopier with sorter; one colour printer;

03.03. PRESS OFFICES

The Press Offices are divided between International (Promoter) and Local (LOC).

The Promoter Press/PR- and the local Press Office must be one large room, separated by a room divider.

03.03.01. Promoter XCAT Press - PR

An office c.25 m², to seat a minimum of 3 people, with workstations made up of desks and chairs with sufficient electrical sockets and fast ADSL Broadband

internet connections (either via cable or wireless); one fax machine; one telephone line;

03.03.02. LOC Press Office

An office c.60 m² (size, tables, chairs, etc. depend upon number of journalists expected) equipped with 5 telephones, one fax, ADSL Broadband internet connections, colour photocopier, a minimum of 4 computers, printers;

03.04. XCAT TV / VIRTUAL

A closed off office, minimum 40m², equipped with a minimum of 4 workstations, sufficient electrical sockets, internet connections (either via cable or wireless AT LEAST MINIMUM 4 Mbit UPLOAD); one fax machine; one telephone line; computers with CD/DVD writer;

03.05. MEETING ROOM

A room which can seat up to 30 people for private **XCAT** /Promoter meetings, furnished with tables and chairs.

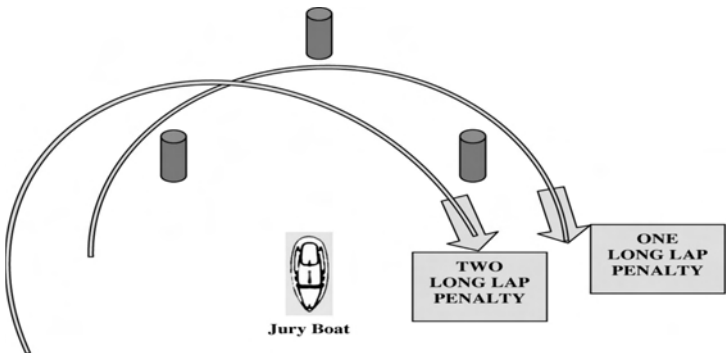
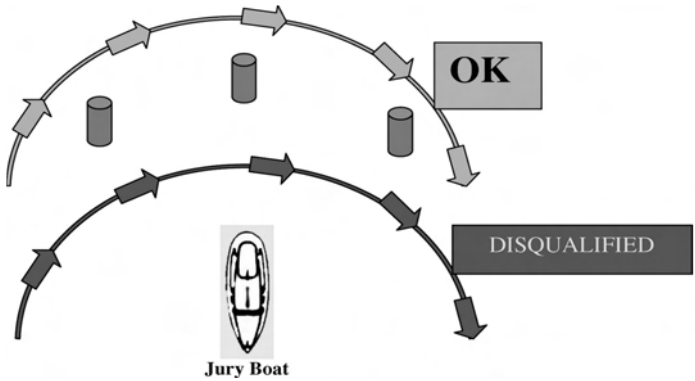
03.06. PILOTS' BRIEFING ROOM

A well-presented room which can seat around 50 people for the Pilots' Briefing. It is set up with a top table (for a minimum of 8 people) and seating in theatre-style.

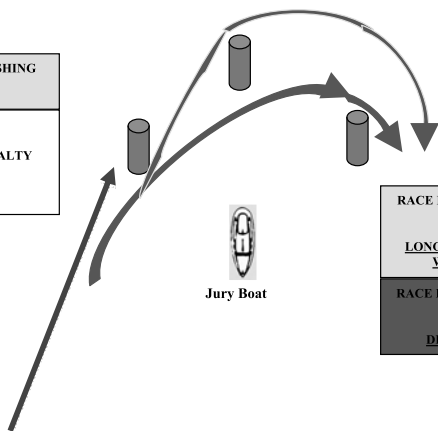
For use for two hours on the first day of the race weekend.

Equipped with PA system with radio-microphones (2), overhead projector and PC projector with projection screen.

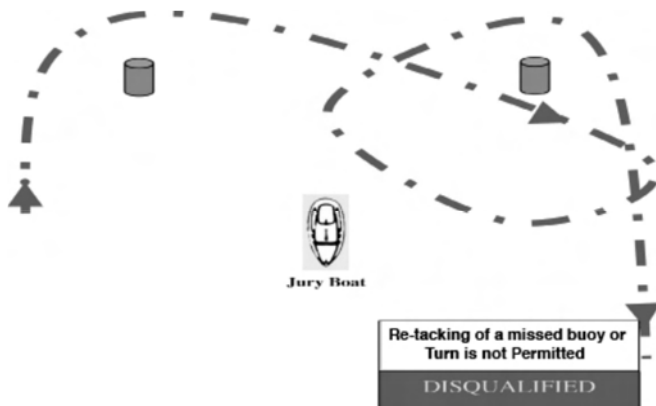
APPENDIX 4 TURN BUOYS CLARIFICATION DRAWINGS



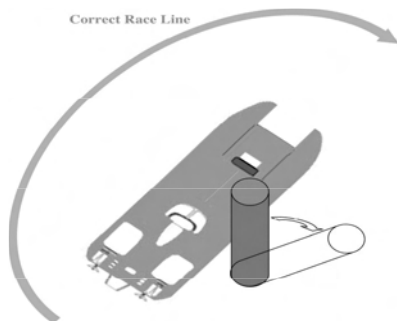
RACE BOAT PUSHING
ONE
RACE LAP PENALTY
PLUS



RACE BOAT MISS 1 OR MORE TURN BUOYS
<u>LONG LAP PENALTY RULE</u> <u>WILL BE APPLIED</u>
RACE BOAT MISS FULL TURN BUOYS
<u>DISQUALIFICATION</u>

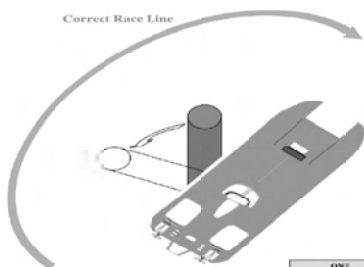


Re-tacking of a missed buoy or Turn is not Permitted
<u>DISQUALIFIED</u>

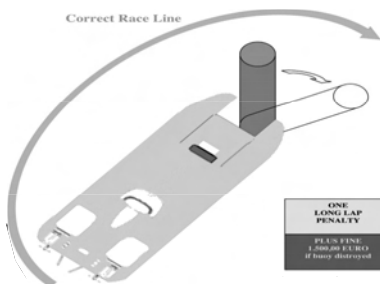


OK

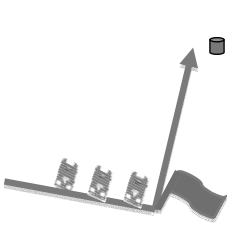
FINE
1.500,00 EURO
if buoy destroyed

ONE
LONG LAP
PENALTY

FINE
1.500,00 EURO
if buoy destroyed

ONE
LONG LAP
PENALTY
PLUS FINE
1.500,00 EURO
if buoy destroyed

APPENDIX 5 **START PROCEDURE DRAWING & CLARIFICATIONS**



Standard Start Procedure

ALL
Race
Overtaking

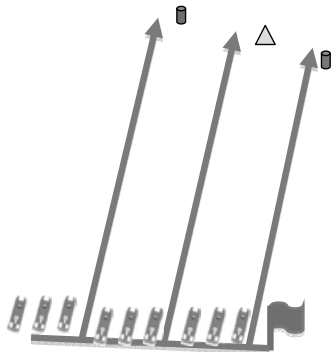
Standard Start Procedure

On line with multi gates on the first turn mark

**ALL RACE BOAT MUST MAINTAIN THEIR REFERENCE
BUOY TURN MARK**

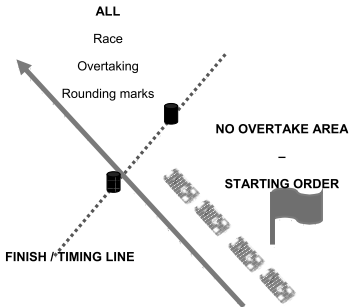
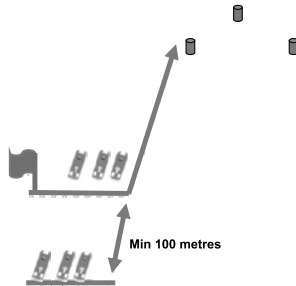
ALL
Race
Overtaking
Rounding marks

Rules will apply inside each "gate channel"



Start Multi Line Procedure

ALL
Race
Overtaking



Single Row Start Procedure

(Valid also for Pace Boat re start procedure)

**ALL RACE BOAT MUST MAINTAIN THEIR
REFERENCE BUOY TURN MARK**

Procedure

In the case of single row start, the race boats must be in line respect their starting order.

Boats must maintain their respective position behind the Pace Boat

It is the responsibility of the Pace Boat to raise the green flag as soon as the race course is cleared and all conditions are ready for re start.

Note: (This Schematic is only for clarification. Reference Appendix 2-04.02)

APPENDIX 6

ADVERTISING & BRANDING CODE

1. DEFINITIONS

1.1 The following definitions shall apply to this UIM XCAT Advertising Code only:

“Advertising” a name, logo, slogan, description, depiction, a variation or distortion thereof, or any other form of communication that promotes an organization, person, product, service, brand or idea so as to call attention to it or to persuade persons or organizations to buy, approve or otherwise support it

“Boat Number” an identifier assigned to a boat by the Series Promoter which shall be displayed on the bow of that boat.

“Competitor” a person on board a boat while Racing.

“Racing” the period of time defined in the Race Instructions.

1.2 Advertising on a boat, personal equipment, race overall, or any other object on board a boat shall be displayed in accordance with this Code, the applicable UIM XCAT rules or the rules of the relevant system. Advertising that is not specifically permitted under this Code is prohibited.

1.3 This Code applies to racing boats – Teams and Competitors during: Official Practice – Pole Position and Racing and at all other times when so prescribed herein.

1.4 Any Advertising and anything advertised shall meet generally accepted moral and ethical standards. Any Advertising which is political, religious, racial or propaganda shall not be displayed on a boat, personal equipment or any other object on board a boat while Racing. Attention is also drawn to the laws of individual nations which may restrict Advertising within their territory or territorial waters.

1.5 Advertising on the boat shall be clearly separated from hull/boat numbers and from XCAT – UIM/WPPA insignia and the Event Organizing Authority Advertising

2. TEAM/COMPETITOR’S ADVERTISING

2.1 Each Team or Competitor, with the agreement of the Person in Charge, may display Advertising on personal equipment.

2.2 Subject to any limitations on Advertising in the applicable UIM XCAT Rules or the rules of the relevant System, Advertising chosen by the Person in Charge may be displayed on a boat /race overall or part thereof, except on the areas detailed in Regulation 1.1 – 1.2

3. EVENT ADVERTISING

3.1 The Organizing Authority may require that boats to display the Event Advertising, provided that such requirement is stated in the Advance Notice or Race instructions. The Organizing Authority must provides the boats with the needed materials (stickers).

4. UIM LOGO AND BRANDING GUIDELINES

Teams / Competitors and event organizers are not allowed to use the UIM logo for any commercial activities or any use which is not specified by the Series Promoter. The usage of the UIM logo when authorize within this code should follow the UIM Branding Guidelines available on the UIM website: www.uimpowerboating.com

5. WPPA & XCAT LOGO AND BRANDING GUIDELINES

Teams / Competitors and event organizers are not allowed to use the WPPA and XCAT logo for any commercial activities or any use which is not specified by the Series Promoter. The usage of the WPPA logo when authorize within this code should follow the WPPA and XCAT Branding Guidelines available on the XCAT website: www.xcatracing.com

6. RACE NUMBER DIMENSIONS

The Race number has to be contained in a white square of 60 x 50cm for numbers 1 to 9, a white square of 70 x 50cm for numbers 10 to 19 and two digit numbers containing a 1, and a white square of 80 x 50cm for numbers 20 to 90 except two digit numbers containing a 1.

Race numbers must be black in colour, plain and upright and use the Arial font or similar. The number 1 in any number will not be Arial font but a plane upright single line white background must be surrounded by a 2cm thick black line with 12.5cm radius corners.

They shall conform to the following minimum dimensions: Height: 40cm Width: 30cm Thickness: 7.5cm Spacing:10cm (see diagrams). The numbers shall be

positioned as far forward as possible. The distance between the Race boat number area and any advertising will be 150mm.

Diagram numbers 1 to 9
(White cm 60x50)

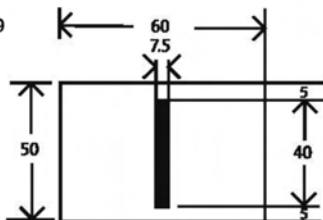


Diagram numbers 10 to 19
and 21 (White cm 70x50)

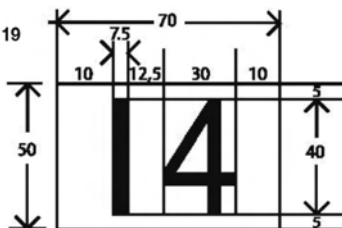
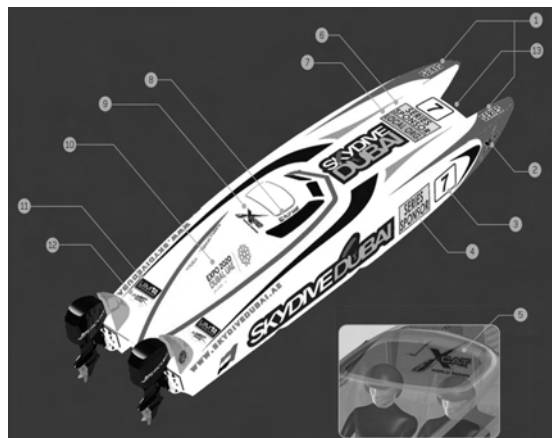


Diagram numbers 20 to 30
except 21 (White cm 80x50)



REGULATION 1.1

XCAT BOAT BRANDING

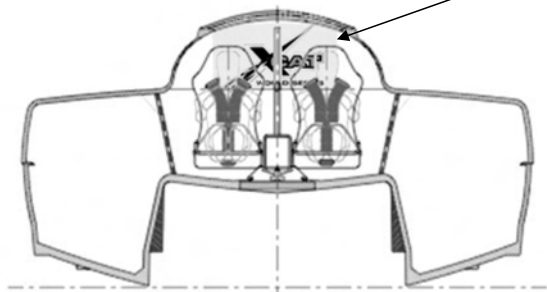


- 1 Series Sponsor
50 cm X 20 cm
- 2 XCAT Logo (x2)
40 cm X 40 cm
- 3 Boat Number (x4)
Size as per rules
- 4 Series Sponsor (x2)
95 cm X 60 cm
- 5 XCAT Logo Behind Pilots
40 cm X 40 cm
- 6 Series Sponsor
100 cm X 40 cm
- 7 Local Organiser
100 cm X 30 cm
- 8 Pilot's Name
& National Flag (x2)
- 9 XCAT Logo
40 cm X 40 cm
- 10 EXPO 2020 Logo (x2)
100 cm X 31.5 cm
- 11 UIM Logo (x2)
50 cm X 20 cm
- 12 WPPA Logo (x2)
50 cm X 20 cm
- 13 XCAT logo in front
of the Boat
90 cm X 60 cm

INSIDE THE COCKPIT

X-CAT 40 X 40 behind

Driver's seat

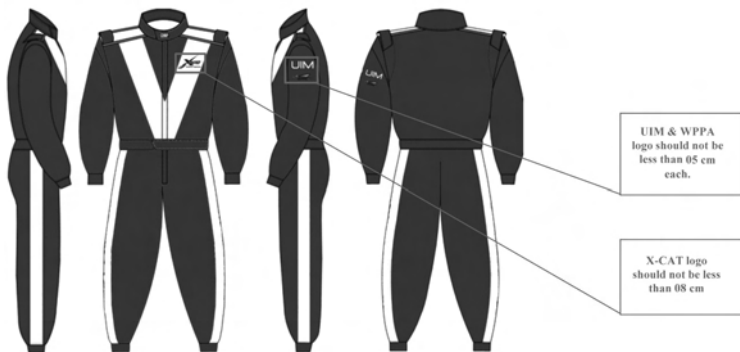


TEAMS/ PILOTS LIABILITY

- Teams/ Competitors are responsible to comply with the Advertising and branding code contained in the 2014 UIM XCAT Racing Rules and any and all regulations issued by the Series Promoter.
- Teams may submit a diagram of their Branding layout to the Series Promoter for approval.
- Branding shall be kept in a clean and tidy condition and stickers shall be properly applied and maintained. Teams shall maintain the branding material provided by the Series Promoter and the Event Organiser.
- Teams/Competitors attention is drawn to Appendix 6- Regulation 1-1 & 1-2: if the branding does not comply with the Rules:
- the Technical Director or the Race Director may apply a penalty to any non complying boat.
- the UIM & Series Promoter may penalize any non complying boat.

REGULATION 1.2

RACE OVERALL - ADVERTISING & BRANDING CODE



2014 CLASS 1 / XCAT SUPERLICENCE

205.03 - MEDICAL EVALUATION SHEET

Location of test _____ Date _____

Name _____ Surname _____

Born in _____ Date _____

Address _____

Status _____ Profession _____

Date of first involvement in the sport _____

Other sports practised _____

PRESENT STATE OF HEALTH AND FORMER DISEASES

Disease (former or current) _____

With special mention of _____

Cardiopathy Coronary Diabetes Type 1 / 2 (circle the type)

Epilepsy High blood pressure

Operations undergone previously _____

Dates of these operations _____

Accidents (when, where, under which circumstances) _____

Alcohol g/day _____ Tobacco pack/year _____

Allergies _____

Medicines usually/frequently taken _____

Please keep in mind the Anti Doping rules and the WADA – UIM list of banned substances and Therapeutic Use Exemption rules.

205.04 - GENERAL OBJECTIVE EXAMINATION

Height m _____ Weight kg _____

Body Mass Index (Weight (kg)/Height²(m) BMI): _____

No superlicence will be issued to a driver with a BMI over 30 kg/m²

These anthropometric data can be checked during competitions by the UIM Medical and/or AntiDoping Delegate who has the right to withdraw Superlicence from non compliant pilots.

Age _____ Sex _____

Congenital or acquired deformities _____

Respiratory system _____

Spirometry (mandatory) Please attach printed results and graphs _____

FVC (measured/predicted) _____

FEV₁ (measured/predicted) _____

Cardiovascular system : is there any evidence of abnormality of the heart or cardiovascular system ? Yes No

If "Yes", give details below.

Blood pressure at rest _____

Heart frequency at rest _____

Electrocardiogram at rest _____

Stress ECG and blood pressure profile (Mandatory, please attach printed results and graphs and final report in English language)

Ophthalmologic check-up (mandatory every other year)

Have you undergone this ophthalmologic check-up last year ?

If yes, please specify when : .. / .. /

If not, ask your ophthalmologist to fill the hereafter items

Faculty of vision (uncorrected) R _____ L _____

(with correction) R _____ L _____

Vision with both eyes open (wearing corrective lenses if necessary)

Field of vision _____

Vision of colours: is the applicant's colour vision normal?

Yes No

If "No", give details below.

Signature and practice stamp of the ophthalmologist

Hearing testing _____

Urine: Albumine _____ Sugar _____

Is the Urine analysis normal? Yes No

If "No", give details below.

Nervous system _____

Others _____

Does the applicant have any physical abnormality or restriction of movements in the arms or legs? Yes No

If "Yes", give details below.

Is there any evidence of a physical or mental condition (past or present) which could, in your opinion, prevent the applicant from holding a competition licence for motorsport ?

Yes No

If "Yes", give details below.

To the Doctor and the Driver :

Are you the applicant's usual doctor? Yes No

Your practice stamp (together with your name and qualifications):

Name in capital letters _____

Degree _____

Address _____

Tel _____

Fax _____

Aptitude assessment _____

Date _____

This is to certify that I have examined the applicant in line with this form.

Signature of the Doctor _____

Signature of the Driver _____

Maximal stress ECG and blood pressure profile: electrocardiogram have to be performed:

* on a cycloergometer starting at 60 or 70 watts and increasing by 30 or 35 watts every two minutes.

The driver must be capable of reaching a minimum power level of 2.5 watts (males) – 2.2 watts (females) per kilogram of body weight.

* or on a treadmill, using a Bruce protocol (preliminary warm-up recommended)

The driver must be capable to complete at least the entire stage 3 (i.e. 3 minutes at 5.5 km/h (3.4 mph) and 14% gradient).

This stress ECG aims to confirm both cardiovascular health and that the driver reaches the minimal fitness level to compete in motonautic sports. Therefore, we strongly encourage drivers to undergo regular physical training and to perform a maximal stress ECG test.

A written report from the physician (English language) must be enclosed.

Exhaustive ophthalmological check-up, tested and certified (English language) by a professional ophthalmologist to be renewed every two years and after skull trauma.

General Recommendation for Class 1 and XCAT superlicence holders concerning physical fitness

Cardiovascular training (endurance training) as jogging, cycling, rowing etc. twice a week - minimum 20 minutes without any interruption. Any sports activity which has “stand still” during the activity such as soccer, tennis or any ball sport is not suitable for the endurance training, but recommended as additional training.

Training to build up muscles for neck and back at least twice a week.

Achévé d'imprimer sur les presses de
MULTIPRINT - MONACO
en avril 2014
(82832)